

MASCOUTAH GREAT STREETS

Strategic Planning Report

Appendices



December 2020

Mascoutah Great Streets

Market Analysis & Strategy

Mascoutah, IL
November, 2020
WHITE PAPER

APPENDIX A



INTRODUCTION

Development Strategies is part of a multi-disciplinary team studying options to improve physical and economic conditions along the Main Street corridor in Mascoutah, IL. The main project area is a one-mile corridor on Main Street, stretching from 6th St. to Mine Rd and includes full blocks between Church St. on the north and State St. on the south.

The broader project area focuses on community connectivity to Main Street and the neighborhoods, schools, and parks of Mascoutah. While the market analysis and strategy focus on the Main Street corridor, the recommendations about zoning ordinances and property maintenance extend to the broader project area, focusing primarily on the City's Southside neighborhoods.

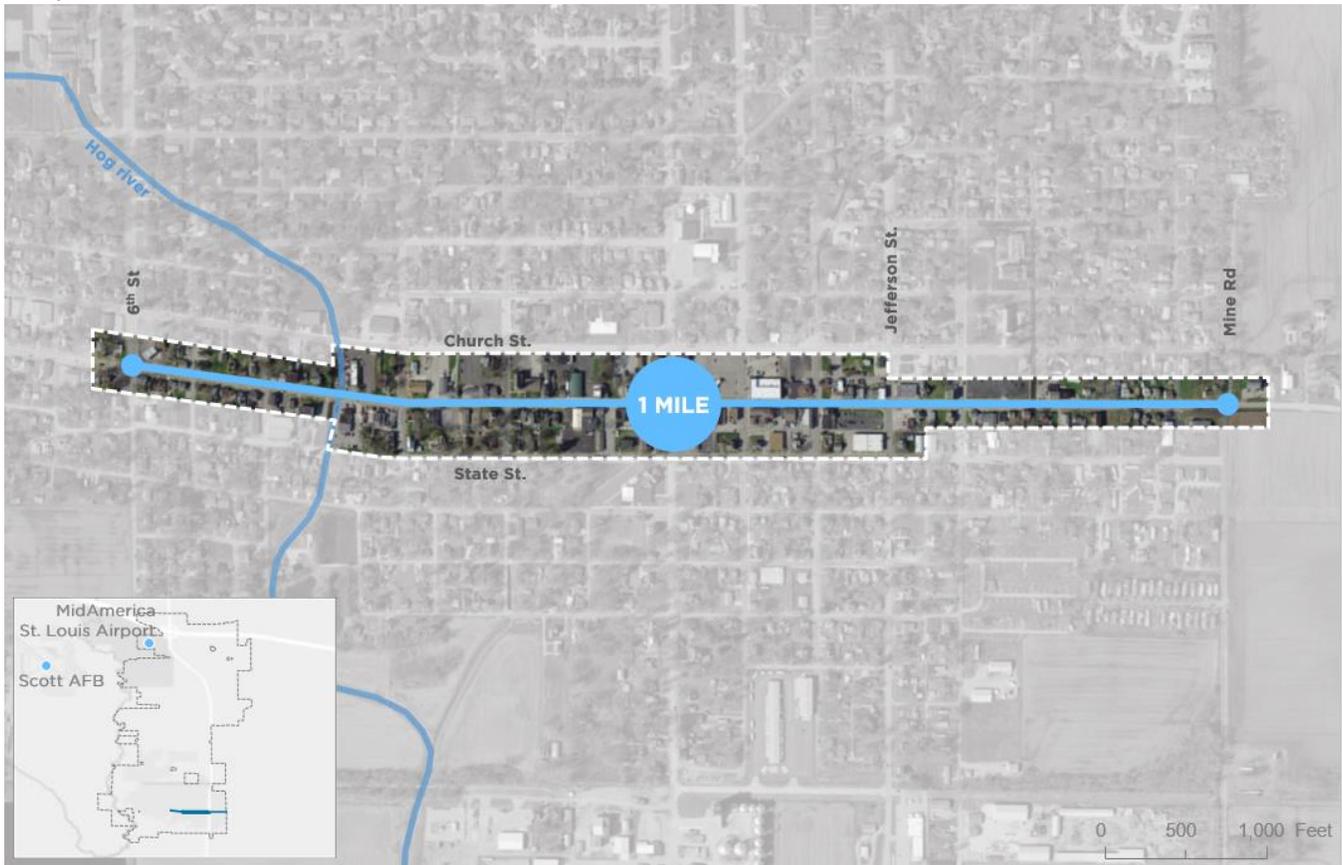
Mascoutah is a growing community. The City has been successful in maintaining incremental population growth while preserving its semi-rural setting. North-south state Route 4 passes through Mascoutah and links the City to Interstate 64 to the north and state Route 177 to the south. The community is served by MidAmerica St. Louis Airport, which is on the grounds of Scott AFB. A mix of old (mostly directly along the corridor) and new (mostly north and west) neighborhoods flank Main Street. An agreement between Scott Air Force Base and Mascoutah Schools results in a larger than typical student population for a city of this size, with well-funded facilities clustered near each other north of the main project area. The City has also recently installed a bicycle / pedestrian path which runs along a channelized creek, locally known as the "big ditch", parallel to Main Street, loosely connecting the schools to the nearby neighborhoods.

This plan offers strategies that will build on these successes and assets by attracting more visitors and residents to the City, improving recreational amenities, encouraging redevelopment, supporting local businesses, and enhancing the marketability of the neighborhoods around Main Street.

STUDY AREA MARKETABILITY

The Main Street corridor, stretching from 6th St. to Mine Rd., is the historic commercial heart of the City of Mascoutah. This corridor is truly dynamic—ranging from its historic commercial core, the transition from rural St. Clair County to the mix of residential and commercial uses. Each of these components are important to the community and provide a foundation for future economic development.

Study area: Main Street, Downtown Mascoutah



The Main Street corridor has many strengths that can be leveraged to improve its visibility, marketability, and economic conditions. The corridor has low vacancies for the most part. Its current access to Route 4 offers great connectivity and visibility for the corridor. Commercial properties along Main Street have good bones that, with some investment, can be leveraged for enhancing the corridor's overall visibility and attractiveness.

Current commercial rents in this part of town are relatively low (less than \$10/s.f.). While this indicates low demands for the current products being offered in the corridor, it certainly works in the favor of small businesses. Most buildings have parking lots to the rear. This is great for accommodating parking requirements without compromising the pedestrian experience of consumers visiting the businesses on Main Street. Additionally, the majority of businesses along Main Street are family-run, small businesses that have been in operation for a long time. City leaders and key stakeholders have expressed a desire to preserve and support these in-town entrepreneurs while attracting additional businesses and attractions to the corridor.

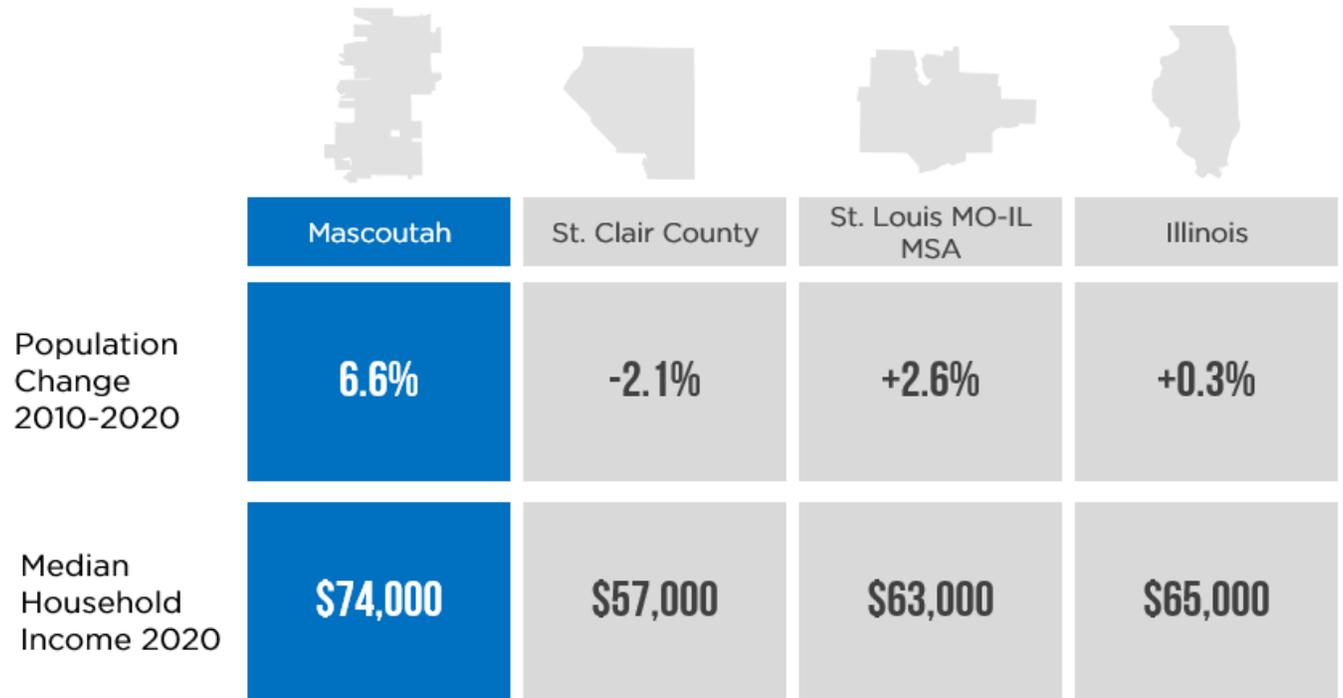
While the Main Street corridor has many strengths, this corridor faces challenges as well. The current easy automobile access has come at the expense of walkability and the corridor is in a major need of pedestrian-friendly investments. The condition of the sidewalks along the corridor is poor. Street trees and vegetation to improve the overall pedestrian experience is limited. Existing building stock offers opportunities for new tenants but needs significant cosmetic improvements and/or full scale redevelopment in order to attract desirable tenants.

Parking accommodations at the rear of the buildings are largely inconsistent, likely due to disparate ownership, and appear to be minimally maintained. Signage directing potential visitors to the area is essentially non-existent. This affects the overall appeal of the downtown area as many potential visitors perceive the area to lack parking and choose to visit businesses located outside of Mascoutah instead. There are occasional flooding issues in throughout the corridor that also need to be addressed by any redevelopment efforts that might take place. Please refer to the 'existing conditions' section in the white paper by DTLS for more details.

KEY METRICS

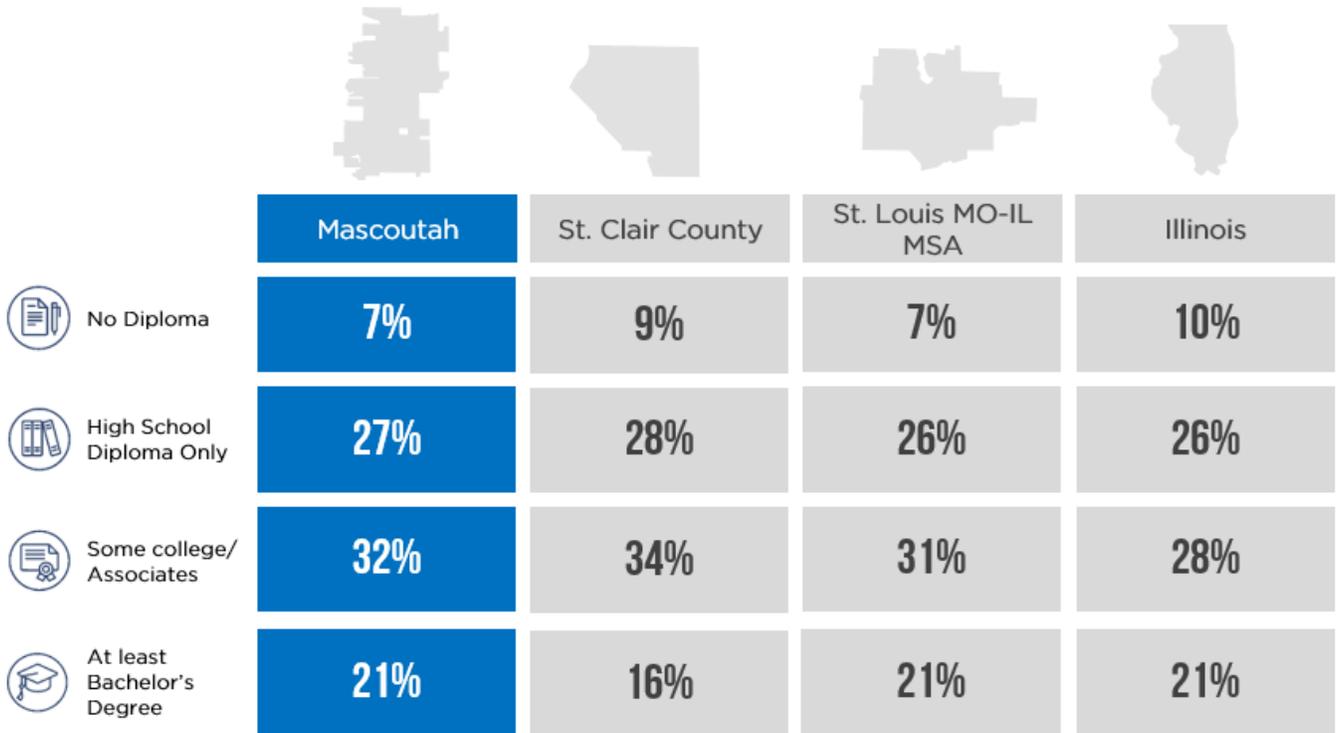
Demographic Information

According to the US Census, between 2000 and 2010, the City of Mascoutah increased its population by approximately 28% to 7,389. Recent estimates from the 2020 ESRI data indicate that the community has increased by another 6.6% since then, bringing the current population to 7,878 residents. These estimates demonstrate that Mascoutah's population is growing at a rapid pace. The City is also out-performing the state and the county in terms of population growth, and net in-migration indicates people's preference to move to Mascoutah.



Source: ESRI, US Census

The number of people in Mascoutah who do not have a high school diploma mirrors the region but is below the county and state amounts, indicating that most of the population has at least a basic level of job readiness. The City has a higher proportion of residents who have some additional training beyond high school (such as an associate's degree) and is keeping pace with the region at attracting/retaining those with a college degree.



Source: ESRI, US Census

From 2010 to 2020, ESRI estimates show that the City of Mascoutah grew most rapidly in the senior (65-74 years old), early workforce (25-35 years old), and empty nester (50-64 years old) age groups. The presence of Scott Air Force Base as well as the ongoing suburbanization, are both significant drivers for the growing senior and early workforce populations in Mascoutah. However, the City lost some residents in the college (18-24 years old) population. To ensure the long-term viability of the community, it is essential that the City actively work to attract young adults to Mascoutah. This data shows ample opportunity to retain the established older population while developing amenities to attract younger workers. City of Mascoutah can employ strategies discussed later in this white paper to attract younger workers while retaining the older population.

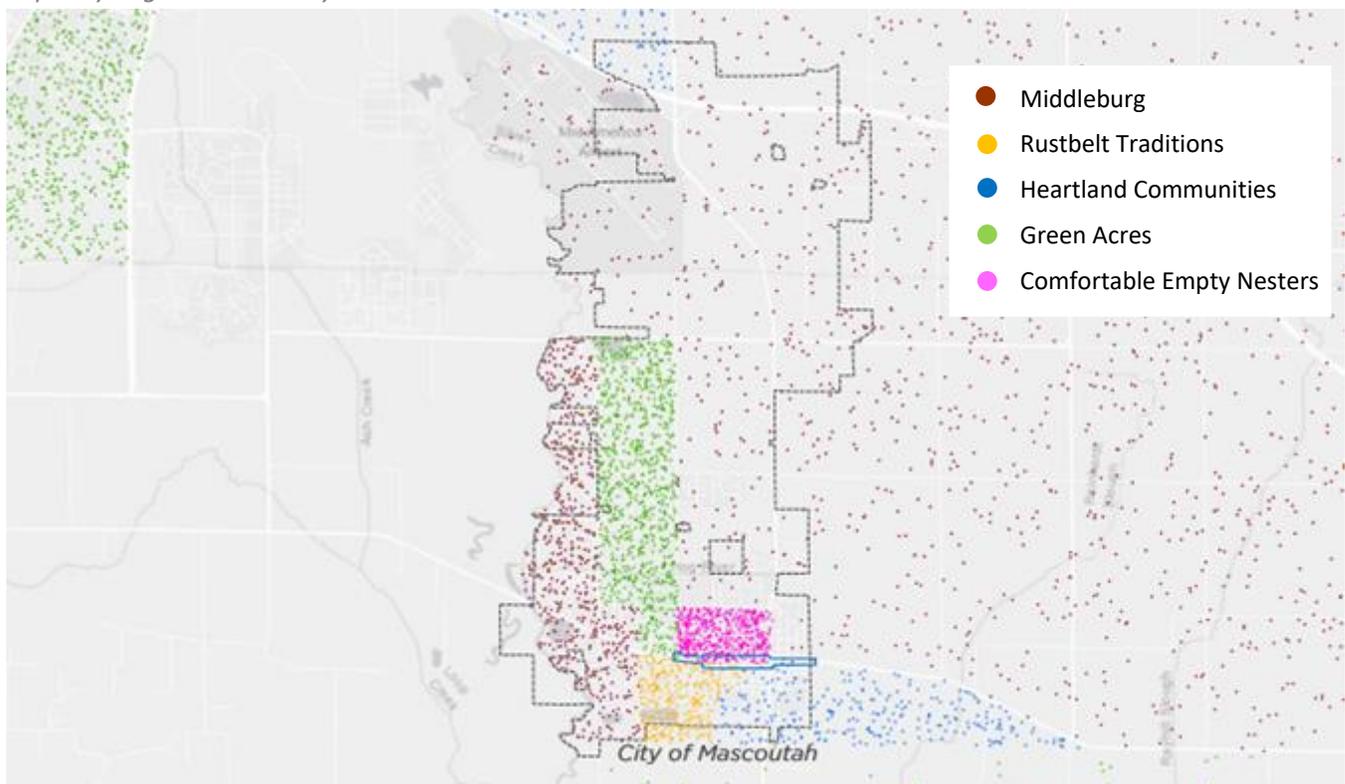


Tapestry Segmentation

Tapestry segmentation helps to gain a broader understanding of people that live in a particular area. It goes beyond basic demographics (such as age or income) to consider psychographics (such as preferences or aspirations) and classifies consumers based on common characteristics and lifestyle choices. For this study, we examine the dominant tapestry groups at the City and the county level to understand and identify demand for different housing and retail products in the City and the Study Corridor.

Five different segmentation groups are within the City of Mascoutah. As shown on the map below, the five groups include Middleburg, Rustbelt Traditions, Heartland Communities, Green Acres, and Comfortable Empty Nesters. All these groups exhibit homeownership rates that are higher than the national level. All these groups represent stable populations, with their median household incomes ranging from modest (Rustbelt Traditions and Heartland communities) to higher than the national average (Comfortable empty Nesters and Green Acres). The map below shows a concentration of Comfortable empty nesters group just north of the Study Corridor. This is a growing population segment in Mascoutah comprising semi-retired couples who might be interested in moving out of their current single-family houses and downsize without leaving the City. A summary of each group and some key data points are provided below.¹

Tapestry Segmentation-City Level



¹ <https://doc.arcgis.com/en/esri-demographics/data/tapestry-segmentation.htm>



| MIDDLEBURG | | |
|-------------|---------------|------------------|
| 36 years | 60K income | 73% homeowner |

Middleburg

Middleburg neighborhoods transformed from the leisurely pace of country living to semirural subdivisions in the last decade when the housing boom reached out. Residents are conservative, family-oriented consumers. Still more country than rock and roll, they are thrifty but willing to carry some debt and are already investing in their futures. They rely on their smartphones and mobile devices to stay in touch and pride themselves on their expertise. They prefer to buy American and travel in the US. This market is younger but growing in size and assets.



| RUSTBELT TRADITIONS | | |
|---------------------|---------------|------------------|
| 39 years | 52K income | 71% homeowner |

Rustbelt Traditions

The backbone of older industrial cities in states surrounding the Great Lakes, *Rustbelt Traditions* residents are a mix of married-couple families and singles living in older developments of single-family homes. While varied, the workforce is primarily white-collar, with a higher concentration of skilled workers in manufacturing, retail trade, and health care. Rustbelt Traditions represents a large market of stable, hard-working consumers with modest incomes but an average net worth of nearly \$400,000. Family-oriented, they value the time spent at home. Most have lived, worked, and played in the same area for years.



| HEARTLAND COMMUNITIES | | |
|-----------------------|---------------|------------------|
| 42 years | 42K income | 70% homeowner |

Heartland Communities

Well settled and close-knit, Heartland Communities are semirural and semiretired. These older householders are primarily homeowners, and many have paid off their mortgages. Their children have moved away, but they have no plans to leave their homes. Their hearts are with the country; they embrace the slower pace of life here but actively participate in outdoor activities and community events. Traditional and patriotic, these residents support their local businesses, always buy American, and favor domestic driving vacations over foreign plane trips.



GREEN ACRES

| | | |
|-------------|---------------|------------------|
| 44 years | 77K income | 86% homeowner |
|-------------|---------------|------------------|

Green Acres

The Green Acres lifestyle features country living and self-reliance. They are avid do-it-yourselfers, maintaining and remodeling their homes, with all the necessary power tools to accomplish the jobs. Gardening, especially growing vegetables, is also a priority, again with the right tools, tillers, tractors, and riding mowers. Outdoor living also features various sports: hunting and fishing, motorcycling, hiking and camping, and even golf. Self-described conservatives, residents of Green Acres remain pessimistic about the near future yet are heavily invested in it.



COMFORTABLE EMPTY NESTERS

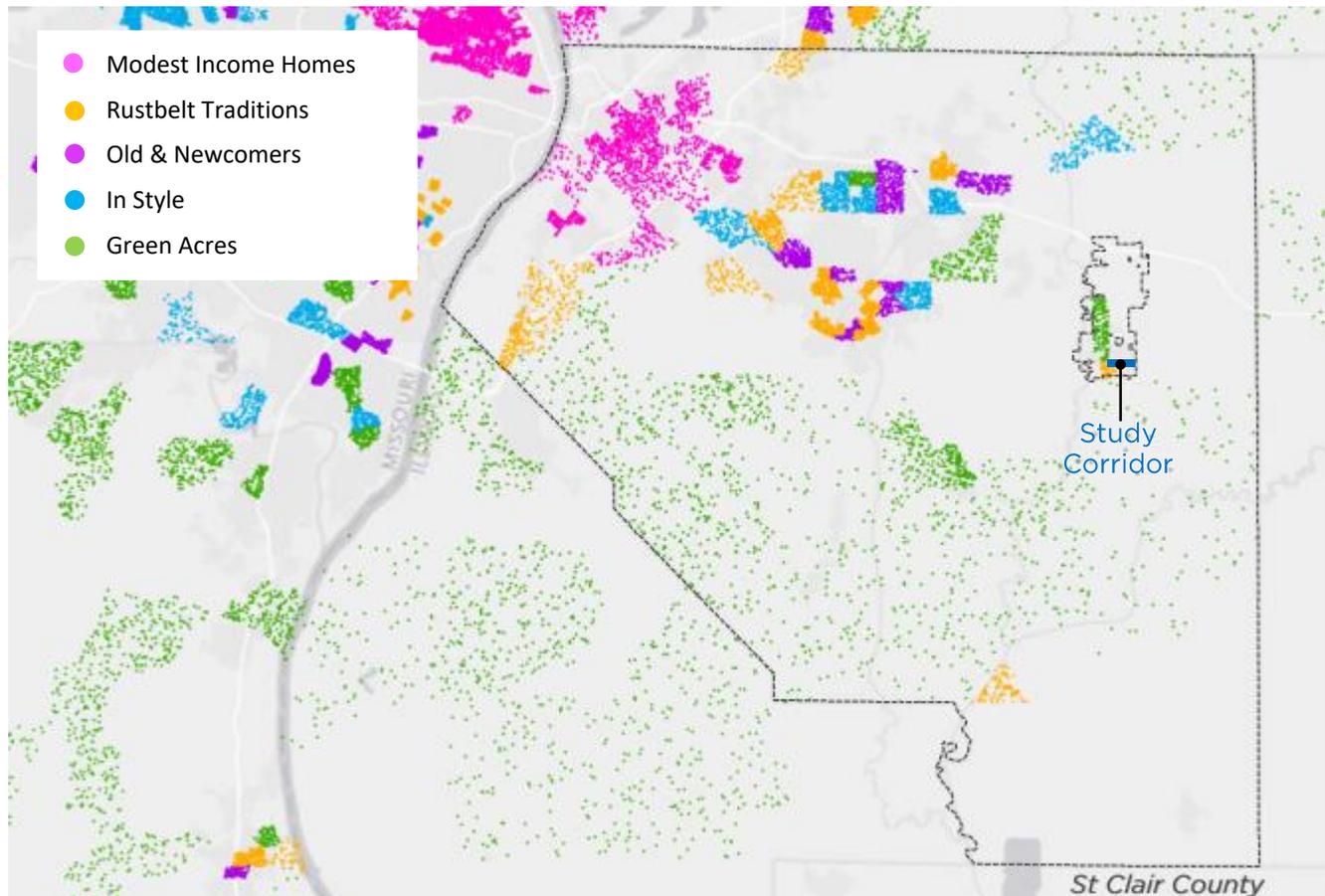
| | | |
|-------------|---------------|------------------|
| 48 years | 75K income | 87% homeowner |
|-------------|---------------|------------------|

Comfortable Empty Nesters

In this large, growing segment, residents are older, with nearly half of all householders aged 55 or older; many still live in the suburbs where they grew up. Most are professionals working in government, health care, or manufacturing. These Baby Boomers are earning a comfortable living and benefitting from years of prudent investing and saving. Their net worth is well above average. Many are enjoying the transition from child-rearing to retirement. They value their health and financial well-being.

In addition to Rustbelt Traditions and Green Acres groups (also present at the City level), three different segmentation groups are present within St. Clair County. As shown on the map below, these groups are Modest Income Homes, Old & Newcomers, and In Style. Even though these residents are typically located further away from the Study Corridor, they are an essential part of the customer base for any commercial or residential activity in the study area. Incomes in these tapestry segments are generally more modest and the homeownership rate is lower. The presence of a group of young adults such as Old & Newcomers as well as young families such as Modest Income Homes is a very positive sign for attracting new visitors and residents to downtown Mascoutah. Affordable rental housing products that act as 'start-up' housing could attract these groups to the City of Mascoutah. A brief summary of each group and some key data points are provided below.²

Tapestry Segmentation-County Level



² <https://doc.arcgis.com/en/esri-demographics/data/tapestry-segmentation.htm>



Modest Income Homes

Residents in this segment live primarily in low-density, settled neighborhoods in the Midwest. The households are a mix of married-couple families and singles. Many families encompass two generations who have lived and worked in the community; their children are likely to follow suit. The manufacturing, retail trade, and health care sectors are the primary sources of employment for these residents. This is a younger market—beginning householders who are juggling the responsibilities of living on their own or a new marriage while retaining their youthful interests in style and fun.



Old & Newcomers

This market features singles' lifestyles, on a budget. The focus is more on convenience than consumerism, economy over acquisition. *Old and Newcomers* is composed of neighborhoods in transition, populated by renters who are just beginning their careers or retiring. Some are still in college; some are taking adult education classes. They support charity causes and are environmentally conscious. Age is not always obvious from their choices.



In Style

In Style denizens embrace an urbane lifestyle that includes support of the arts, travel, and extensive reading. They are connected and make full use of the advantages of mobile devices. Professional couples or single households without children, they have the time to focus on their homes and their interests. The population is slightly older and already planning for their retirement.

MARKET ANALYSIS

Residential Supply

Development Strategies conducted a high-level scan of the housing supply in Mascoutah for both rental and for-sale properties.

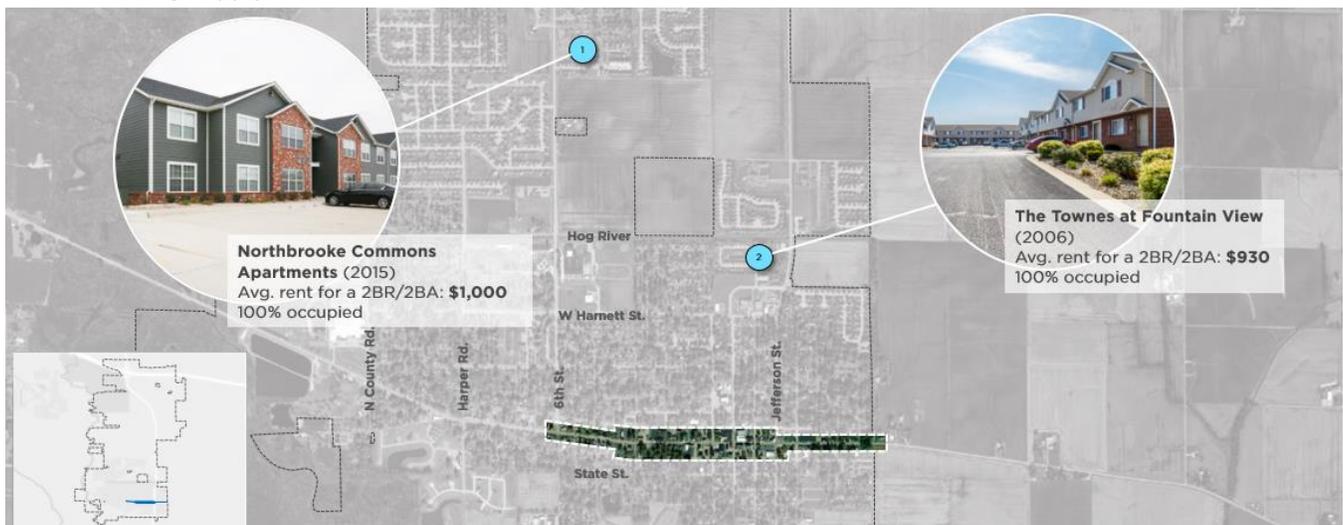
Rental

When looking for recently built apartments in the City, the only two rental properties that fit the criteria are north of the Study Corridor. The Northbrooke Commons Apartments, built in 2015, is the closest to an upscale rental option in the City. Average rents are \$1,000 per month. The Townes at Fountain View offers townhouse apartment living and is nearly 15 years old. On a per square foot basis, these units are renting at a higher price point (\$0.84) than anything else in the City, although the rate is still lower than expected for a true upscale product. Both of these properties are at 100% occupancy.

The Main Street corridor has a significant supply of lower-midscale rental properties. Most of these are at least 70 years old and located on the second and third floors of commercial properties and behind some retail businesses along the corridor. Local rental companies commercially manage all these properties, most of which are at 100% occupancy.

There is no commercially available data on residential rentals to the south of the City, most of which are single-family homes and mobile homes. Given the quality of the housing stock which is mostly below average, the rents being achieved here are expected to be much lower than the market rents for the broader area.

Rental Housing Supply



For-Sale

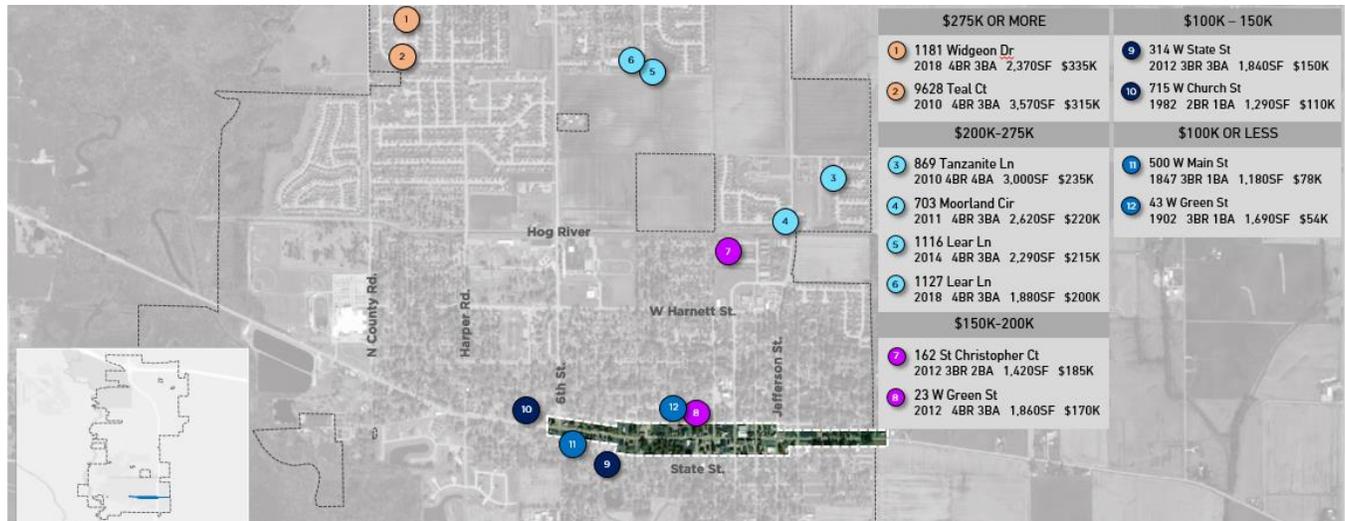
Mascoutah offers a decent variety of single-family homes. The subdivisions located to the north of downtown Mascoutah have the majority of homes built in the last ten years. In the Study Corridor, the homes are much older, with the median year of construction approximately 1910. The City boasts some beautiful historic homes south of the Study Corridor along Railway Road and South Street. Recent sale prices cover a somewhat limited range from about \$50,000 to \$330,000. The median sales price in Mascoutah in 2020 (YTD) was \$173,000, which is much higher than St. Clair county as a whole, but below the St. Louis MSA. Mascoutah's home sale prices have increased at a higher rate (20 percent) than that for the county (16 percent) in the past five years.

| Median Sales Price | Mascoutah | St Clair County | St. Louis MSA |
|---------------------------|------------------|------------------------|----------------------|
| 2020 YTD | \$173,000 | \$114,000 | \$183,000 |

Source: Zillow

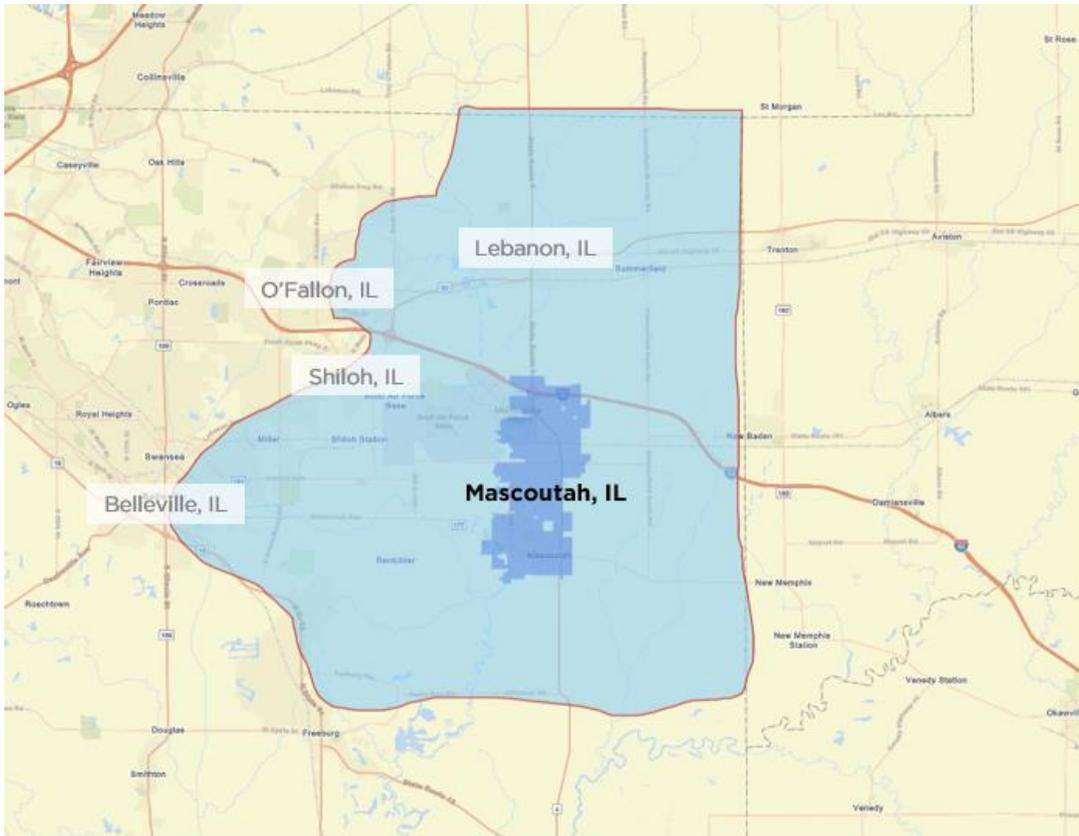
For our study's purposes, we focused our attention on the housing supply near the study area, along Main Street and one block to its north and south. The homes along these roads were developed in small clusters over a long period and are very inconsistent in appearance. Most of the supply along here is a mix of historic bungalows, inexpensive vinyl-wrapped cottages and some well-maintained smaller homes. Some of the older homes are in poor shape and require significant rehabilitation. These overall inconsistencies somewhat reduce the houses' market value but do provide a variety of products to suit many price points. Over the past year, sales prices in this part of town ranged from \$20,000 to \$160,000, and the 2020 median value of owner-occupied housing units is \$152,000.

For-Sale Housing Supply



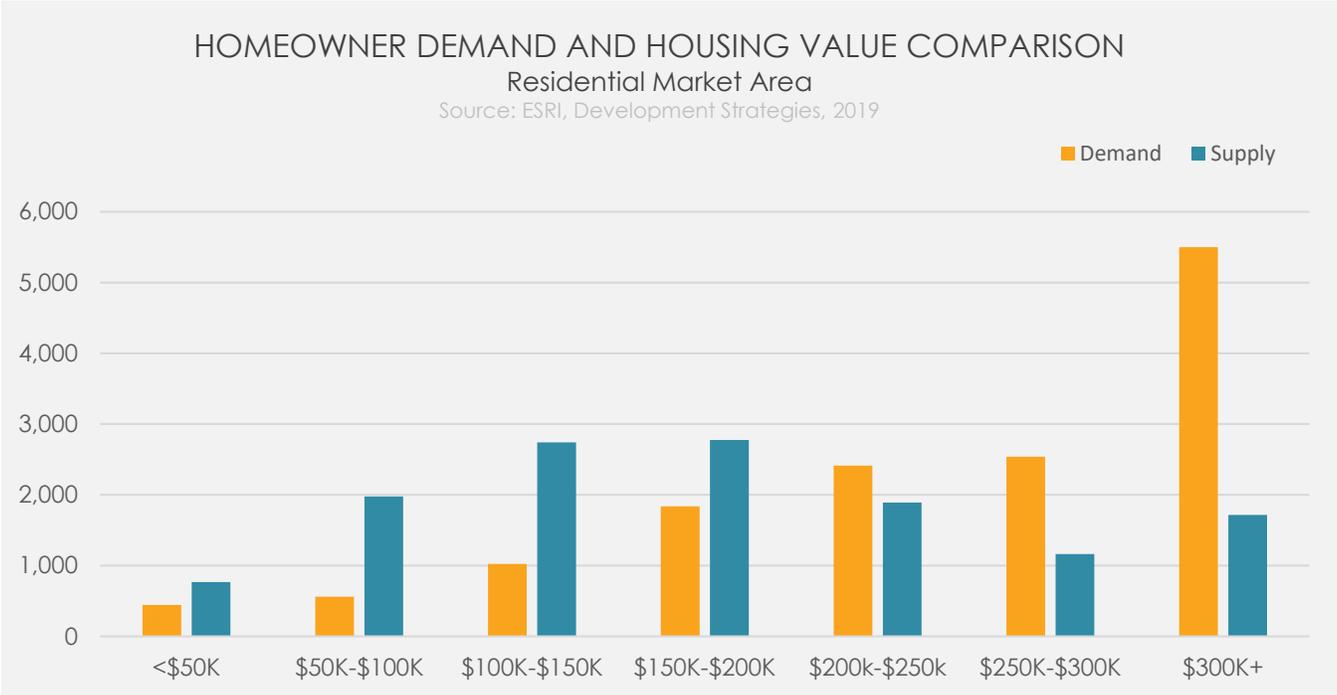
Residential Demand

For this high-level analysis, residential demand was evaluated using a methodology which considers income (and the ability to afford housing at various price points) and compares it to the existing supply to look for mismatches. To understand the housing demand for our study area, we looked at a market area stretching from eastern boundaries of the county to the west of Mascoutah, including Lebanon and parts of O'Fallon, Shiloh and Belleville. There are approximately 20,500 households in this market area, with a median income of \$65,600.

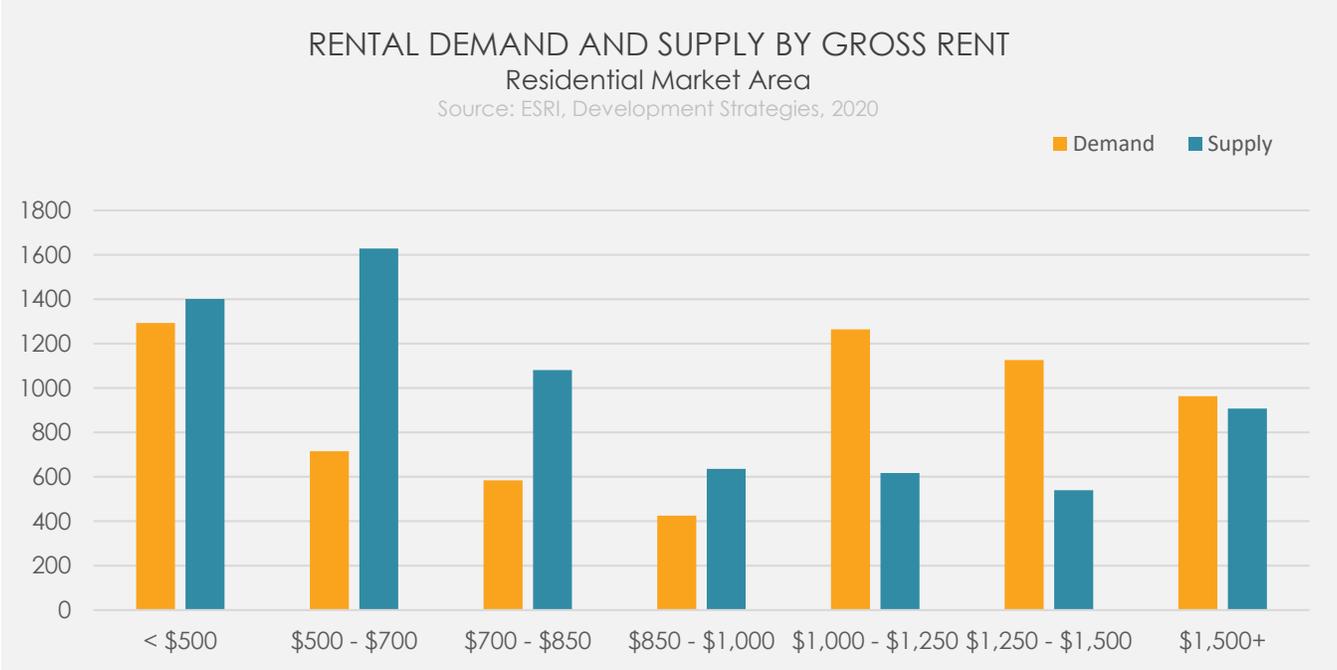


| |
|--------------------------------|
| POPULATION |
| 52,800 |
| HOUSEHOLDS |
| 20,600 |
| AVG HOUSEHOLD SIZE |
| 2.4 |
| MEDIAN HOUSEHOLD INCOME |
| \$65,600 |

In many communities, the demand for affordable housing exceeds the supply. However, in our market area, that dynamic is not present. The supply of homes \$150,000 or less far exceeds the demand for that product. In this case, most of this housing is likely "naturally occurring" affordable housing. If well-maintained, these homes can be perfect for modest income households, which are a significant group in the county level tapestry. If poorly maintained, this large stock of housing will slowly decay and present additional challenges for the community. In the \$150,000-\$200,000 range, supply and demand are more closely matched. Above that price point, the supply is deficient considering the large number of people who have the ability to afford these higher quality products. A possible explanation for this huge gap between supply and demand might be that the people who can afford more expensive homes are making the choice to buy homes that are less expensive.



Examination of the rental market shows that approximately 60% of the supply rents for \$850 per month or less. Approximately 1,400 households can afford \$1,000 or more per month but are renting more affordable products either due to personal preference or lack of an appropriate product. Since the supply analysis found very few apartments at these higher price points, most of the supply indicated below is likely larger units, such as single-family rental homes or duplexes.



Housing Conclusions

The multi-family market in Mascoutah has not been particularly active over the past decade. There are no products in the City achieving lease rates above \$1.00 a square foot. This level of activity helps inform estimates for the future. Within the primary market area, the pool of renters who can afford rates of at least \$1,000 a month (which would generally support new construction) is approximately 3,353 households. Applying a reasonable capture rate of fifteen percent yields the potential for 500 units of rental housing over the next five years in the City. Based on the current population in the study area and adjusting for a future scenario where the study area starts attracting more residents, we can estimate that the Main Street corridor could capture approximately 15% of the City's share of rental housing (75 units). Based on the lease rates of the highest quality rental products in Shiloh, and applying a premium for new construction, rates of approximately \$1.1-\$1.25 per square foot could be achievable with a high quality product at an optimal location. However, at that lease rate, a high quality product likely cannot be delivered without public subsidy in some form.

Even though existing homes can be purchased at a wide range of price points, new single family construction is generally only feasible at a minimum sale-price of \$200,000, unless otherwise subsidized. Within the market area, there are approximately 10,455 households with incomes that could afford homes at and above that price point. Applying a reasonable capture rate of fifteen percent yields the potential for 1,600 homes over the next five years in the City.

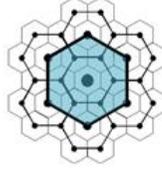
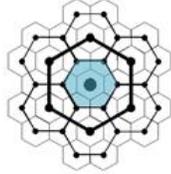
Assuming that the City continues in efforts to improve the marketability of the Study Area, neighborhoods around Main Street could capture a demand of approximately 240 homes over the next 5 years. However, since most of the neighborhood next to the Main Street corridor are fully developed a lot of these newer homes would be infills (smaller homes/ townhomes) or significant renovations/ replacement homes.

In addition to the new supply, rehab and renovation of existing housing is a lucrative strategy to stabilize the neighborhood and achieve higher home values and rents. There is a wide range of options for supporting rehabilitation and renovation. Rebating permitting fees for homeowners in targeted geographies, creating neighborhood tool-sharing programs, and providing lists of qualified contractors could all reduce barriers to reinvesting in the housing stock.

Retail Supply

Retail properties are classified into four broad types. At the highest level is the super-regional shopping center, which typically has three or more anchors and serves a population of 300,000+ people. They're generally enclosed with inward-facing stores connected by a common walkway. Regional level shopping centers have 1-2 anchors and are typically laid out in a straight line, U- or L-shape. Community centers have a smaller anchor tenant and reduced trade area, while neighborhood shopping centers usually rely on a grocery store to drive regular traffic to adjacent businesses.

General Characteristics of Shopping Centers



| NEIGHBORHOOD | COMMUNITY | REGIONAL | SUPER REGIONAL |
|--|---|---|---|
| | | | |
| TYPICAL ANCHOR Drugstore/Supermarket | TYPICAL ANCHOR Junior Dept. Store | TYPICAL ANCHOR 1 - 2 Full-line Dept. Stores | TYPICAL ANCHOR 3 - 4 Full-line Dept. Stores |
| GROSS LEASABLE AREA 30K - 100K SF | GROSS LEASABLE AREA 100K - 450K | GROSS LEASABLE AREA 300K - 900K SF | GROSS LEASABLE AREA 500K - 2M SF |
| POPULATION SUPPORT 3K - 40K | POPULATION SUPPORT 40K - 150K | POPULATION SUPPORT > 150K | POPULATION SUPPORT > 300K |
| TRADE AREA RADIUS 1.5 miles | TRADE AREA RADIUS 3 - 5 miles | TRADE AREA RADIUS 8 miles | TRADE AREA RADIUS 12 miles |
| DRIVE TIME 5 - 10 minutes | DRIVE TIME 10 - 20 minutes | DRIVE TIME 20 minutes | DRIVE TIME 30 minutes |

Mascoutah is served by the regional and super-regional shopping centers situated in Belleville, Shiloh, and Fairview Heights, located within a convenient 15-minute drive from downtown Mascoutah. The City itself has a limited supply of retail options; the bulk of retail available within Mascoutah is located downtown within our Study Corridor. These retail options are mostly small-scale, family-run businesses that have been in operation for many years. Most of this supply falls under the neighborhood retail category.

Lease Rate Implications



Main Street Supply

Currently there is 61,000 sf of retail space in the study area. The vacancy rates are low at approximately 5% (usually, vacancies above 10% should raise concerns). Having said that, while the spaces are generally occupied, some of that occupancy is essentially storage uses. Generally, the market rents for the corridor are on the lower end of the spectrum (\$8-\$10 per sf) which (as shown in the diagram above) does not provide enough income for landlords to invest in basic maintenance, much less major improvements. City intervention is needed to improve the marketability of this area to spur economic activity.

Main Street Retail Supply



Route 4 Supply

Route 4 in the City is another central corridor with some commercial supply. The majority of retail along this corridor is automobile-oriented commercial businesses. Currently, there are 154,000 sf of retail space along this corridor. The vacancy rates are much lower (1.6 percent) than those on Main Street. Generally, the market rents for the Route 4 corridor fall somewhere along the middle of the market rent spectrum (\$10-\$14 per sf). This indicates a need for necessary maintenance by landlords. Ample land is available along this route and is likely to be developed as car-dependent retail in the future.

Route 4 Retail Supply



Restaurant Supply

East West Gateway organized a series of public engagement and stakeholder interviews regarding the Main Street Corridor. These discussions highlighted community desire for having more dining options in the study area. Currently, the Main Street corridor has a handful of bar/restaurants and a few chain restaurants. All of these are reportedly running well even during the pandemic. Some interviewees reported that the restaurants that are in the corridor are not always their first choice when dining out. This could be due to a lack of visibility or lack of enough activity downtown to attract customers. Retail marketplace data from Esri, 2020 suggests that restaurants in Belleville/outside of Mascoutah are currently attracting potential customers from within the city of Mascoutah. Given the right options and atmosphere, these customers may choose to dine on Main Street.

We can ascertain that Main Street has a steady amount of traffic to support more restaurants than currently available in the corridor. The former Mascoutah Steakhouse building is one such opportunity to attract a new tenant as it is reportedly move-in ready. With enough investments in building façades, and physical improvements along the corridor, the Study Area has the potential to attract new restaurant tenants and enhance the downtown Main Street character.

Restaurant Supply



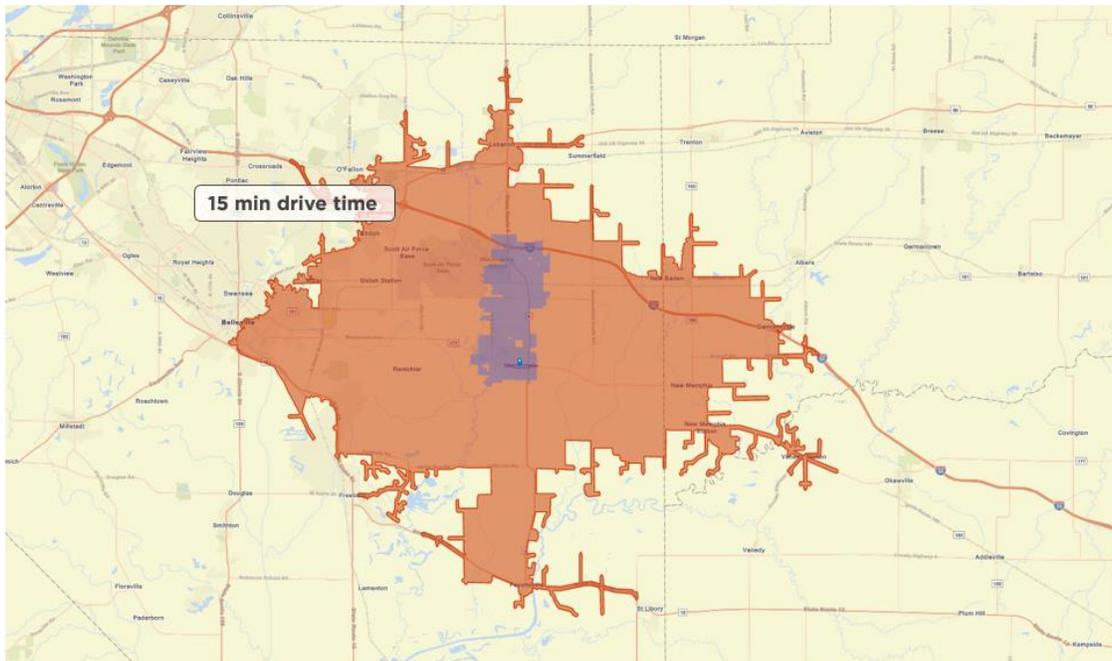
A healthy downtown is a point of pride for residents—the first place to bring out of town visitors to experience unique shops and restaurants. Our understanding of the current potential for restaurant supply in downtown Mascoutah reinforces the benefit of quality public space and place making investments that can help the tenants along this corridor in attracting more business. Infrastructure such as parking, walkability, lighting, wayfinding, ample sidewalks, etc. can all provide a much needed face-lift to the Main Street and position it to attract and maximize future investment opportunities.

Retail Demand

Retail gap analysis looks for mismatches between supply and demand for various types of stores (e.g. grocery, hardware, electronics). This analysis can identify opportunities to provide a product or service that isn't currently being supplied in the marketplace. For the purposes of this project, we examined a market area that constituted a 15-minute drive time from the center of the Study Corridor. We expect that this casts a wide enough net to capture any missing retail products near the Study Corridor and provides an opportunity to position Main Street Mascoutah as a destination for consumers looking for a niche downtown experience.

Market Area

Within this approximate 15-minute drive, there are about 19,395 households with a median household income of \$66,800. The retail gap analysis showed an overall gap of retail in the area. Sector-specific data showed opportunities primarily in sectors such as sporting goods, pharmacies, specialty food & liquor stores, clothing & accessory stores, and furniture stores. While some of these sectors suit the small-scale downtown environment that is in place in the Main Street corridor, others—such as pharmacies, sporting goods, and gas stations—are unlikely to be marketable within the study area due to the necessity for larger building types or higher parking ratios. These retail types would be best suited on larger parcels located on the Route 4 corridor instead.



Main Street Demand

Retail and commercial uses will continue to be focused along Main St as identified by the land use plan drawn by the urban design team for this project. The corridor has the opportunity to fulfill some of the retail and commercial gaps in the regional market area by providing niche stores that also work well in a downtown environment. Retail uses identified below can achieve that balance. The retail gap analysis identifies that the corridor could support approximately four specialty food and liquor stores, three clothing and accessory stores, one furniture store / workshop and three jewelry, luggage, or leather goods stores.

The retail gap analysis showed an overall surplus of grocery stores and restaurants in the area, partially due the presence of other major retail destinations in the market area within Shiloh, Belleville, and O'Fallon. This indicates that the competition will be tight for any new restaurant or grocery store on Main Street Mascoutah. Having said that, if the product is able to distinguish itself enough from the rest of its competitors in the market, the study area would be able to attract residents and visitors. Mascoutah may be able to play up its quaint downtown vibe to help these uses that might be cannibalizing others outside of the market area succeed.



Route 4 Demand

Some percentage of the remaining retail and commercial gaps in the regional market area can be provided by businesses that work well in auto-dependent retail environments. The uses identified below will be best suited for Mascoutah's Route 4 corridor. In addition to attracting sales revenue, these store will also provide a distinct identity to the Route 4 corridor, one that will be complementary but not competitive with the Main Street corridor. The gap analysis identifies that this corridor could support approximately two pharmacies / drug stores, two gas stations, and one sporting goods store.



Feasibility Indicators

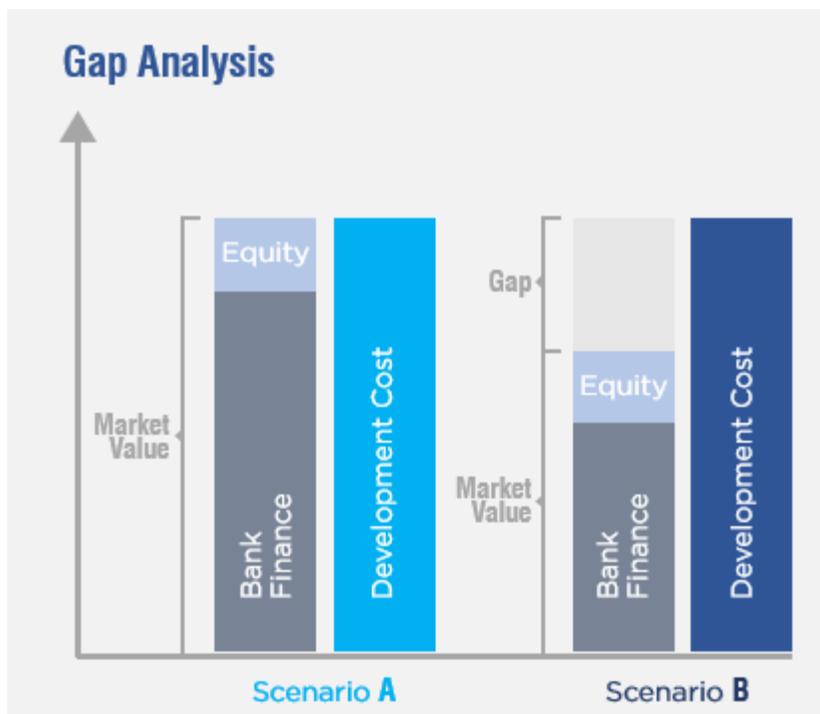
Market analysis provides information critical to understanding the feasibility of real estate developments. Economically feasible developments have a market value that is higher than their development and operational costs.

Key feasibility indicators that are derived from market analysis include:

- market rental rates
- sale prices
- occupancy rates
- operating expenses
- capitalization rates
- discount rates
- carrying costs

A project that is likely to be viable and attractive to private investment is one where value exceeds cost. Developments where cost exceed value can be made feasible through incentives and other tools that provide funds that fill the gap between cost and value. Incentives can be applied to projects in a strategic way, focusing on those that will catalyze a site, district, neighborhood, or corridor, or attract an anchor that can change the trajectory of the place.

Scenario A in the following graphic represents an economically feasible projects, while Scenario B represents a project where incentives are needed to make it feasible.



ENGAGEMENT SUMMARY

Overview

Due to the current pandemic, all of the citizen engagement and key stakeholder interviews for this plan were carried out online. The engagement was centered on a f-week community engagement comprising of three individual rounds lasting a little over a week each. During these rounds, a series of presentation segments was posted to the project web page on the City's website. These comprehensive presentation segments included content from all the consultant teams as well as a related survey of questions. These surveys were designed to help the project team to move in the right direction in the planning process.

The intent of the first round was to share technical data and analysis from the planning team and to clarify the community's goals and priorities with regards to the Study Corridor. The focus of the second round of outreach was to assess various possible options. Lastly, the third round focused on reviewing a draft plan, discussing the implementation strategy, and maintaining those improvements over time.

Key Observations by the Community

Results of the survey were very helpful in guiding our planning efforts. Survey results showed that:

- 76 percent of all survey takers visit downtown Mascoutah to either shop or dine.
- 50 percent of all trips to downtown are under half hour long.
- 43 percent of the survey takers want more cafes with outdoor seating on the Main Street.
- Nearly 70 percent indicated that they prefer apartments with ground-floor retail as a potential housing product for Main Street.
- When asked about their preferred experience on Main Street, the majority of respondents (69 percent) indicated that they wanted to see flexible public spaces that can be used for activities such as a farmer's market.
- 50 percent of the survey respondents prioritized the commercial rehab of the former Mascoutah Steakhouse building, followed by redevelopment of Fred's hardware store.

Additional comments indicated the preference to keep all retail and business in the corridor local and avoid chains. People also showed support and need for attracting more visitors, especially younger adults, to create an active and lively atmosphere downtown. Many comments in the survey highlighted the need for redeveloping the Mural Building into a mixed use program.

Stakeholder Interviews

A series of interviews with key representatives from the community were also conducted to inform our planning process. These included representatives from the City departments, Chamber of Commerce, the local Business community, and representatives from IDOT. Following are some of the key challenges that were highlighted by during our conversation in these interviews.

- Mascoutah is 12 minutes away from a major retail intersection in Shiloh and that has significant impacts on the demand for businesses in downtown Mascoutah. For example, reportedly 48 percent of the local residents go to Walmart for their grocery shopping.

- Residents are not necessarily aware of what downtown Mascoutah has to offer. This may partly be due to a lack of enough consistent activity but is also a result of poor signage throughout downtown and a lack of advertising.
- Residents want to see more retail and dining options along Main Street.
- Most customers visiting the retail in Downtown Mascoutah are older people. Younger crowd is not attracted to shop or visit downtown Mascoutah.
- Parking is available but difficult to find. This is due to a lack of proper maintenance as well as a lack of proper signage to provide directions to nearest parking.
- Mascoutah needs something to draw people in. Currently, Main Street has a little of everything but nothing unique enough to make people go out of their way to visit the community.
- The business community is motivated and willing to work together but needs guidance and some direction to take advantage of the available incentives.

MARKET STRATEGY

A market strategy pairs the goals and objectives of a community with market-based opportunities to develop a path forward. Community goals identified below take into consideration conversations with the City staff, key community members and the survey results from the engagement process.

- Attract new residents and visitors to Mascoutah to support its long-term viability.
- Revitalize businesses along Main Street and attract appropriate commercial tenants that preserve and build upon Mascoutah's quaint historic downtown character.
- Increase Main Street's walkability and attractiveness by investing in public right-of-way improvements, parking management, and sidewalk upgrades throughout the corridor
- Enhance the desirability of neighborhoods flanking the Study Corridor to better support downtown Mascoutah's economic and physical health.

These community goals, combined with the demographic and market analysis discussed in the initial segments of this white paper, help in laying out the following strategies for the Study Corridor and the City.

Attract new residents and visitors to Mascoutah

Data shows that the early workforce population group contributes to a portion of the growth experienced by Mascoutah in the last decade. While it is not as high as the senior and empty nester groups, it is definitely a trend that can be utilized to attract and retain more of the younger working population in the City. The Study Corridor provides an excellent opportunity to lure this demographic, but it will require continued improvements to the building stock and enhanced shopping, dining, and entertainment options.

Equally as important will be providing desirable rental units downtown. The vast majority of young adults are renters, not owners, and without the appropriate product, they will go elsewhere. While the survey during the engagement process showed adequate support for rental units, most prefer these to be located not directly but right behind the Main Street corridor. The stakeholder interviews also showed an underlying concern with the *quality* of rental products, which can be addressed through zoning and architectural controls during construction, and code enforcement during the product's lifetime. The City can also provide rehab incentives and landlord management assistance programs to encourage maintenance and upkeep of rental properties.

If Main Street and the areas around it can successfully attract people in their early workforce years and young families that are just starting out, it will ensure the long-term viability of the corridor and Mascoutah as a whole. Additionally, these families will be more likely to stay in Mascoutah as they grow older and become ready for a single-family home. In this way, the Main Street corridor can act as a conduit for surrounding neighborhoods that provide "step-up" housing while keeping people close to the amenities they love downtown.

Revitalize businesses along Main Street

The commercial properties along Main Street require varying levels of repair. Most of the existing businesses are either long-standing owner-occupied properties or establishments that are likely drawn to the location by the cheap rent. While low-rent can be an asset for the tenant, it also indicates low-demand--a scenario in which landlords are unlikely to improve their properties without City intervention.

Many properties along Main St are desperately in need of façade improvements. Through our discussions with the City officials and Stakeholders, we know that there is interest in extending the TIF district (reportedly set to expire in the next five years). TIF funds should be made available for a matching grant reimbursement program to help with these façade improvement efforts. The City should also consider providing additional assistance to ensure that professional design advice is utilized when making these improvements. This could range from providing a list of pre-screened architects who will take on these small projects to guaranteed reimbursement of a set amount when using approved architects.

A thriving local business scene is crucial for the economic success of Main Street. Local entrepreneurs and small-business can provide services that are unique to Mascoutah and set it apart from the rest of the competition. A thriving small business can also improve property values throughout a community, resulting in more property taxes for local governments. Local businesses can also be the backbone of special taxation districts focused on unique projects, such as lighting and sidewalk projects to improve historic shopping districts and attract additional customers.

Currently, prospective small business owners and aspiring entrepreneurs can face many barriers, such as access to capital, lack of assistance in site selection, antiquated building systems, etc. Efforts that encourage entrepreneurial activity would be beneficial for this corridor and the City as a whole. A small business incubator could offer a collaborative work environment with invaluable mentoring and networking opportunities, funding support and shared equipment. Interior renovations (particularly for adaptive reuse projects that involve the installation of commercial kitchen equipment) could be aided by low/no-interest loans and other tenant improvement programs. The City of Mascoutah can consider investing in a truly impactful business attraction and retention policy that can streamline processes such as permitting, networking, etc.

There are multiple commercial properties on Main Street with apartments on the upper floors. Most of these apartments require significant renovations and face-lift. These businesses can be encouraged to renovate these units by using incentives such as tax abatement, historic tax credits, and affordable housing incentives (might require ADA accommodations).

Increase Main Street's walkability and attractiveness

The Main Street corridor has the potential to be an incredibly diverse and walkable place. Improvements in private properties paired with public investments in streets, sidewalks, and parking can make the corridor much more appealing. Changes to the streetscape that improve aesthetics will encourage reinvestment in the study area. A vital aspect of this strategy is to enhance the street and sidewalk conditions of the Main Street corridor such that it benefits both the businesses and the consumer who happens to visit/shop in the corridor.

Parallel parking on Main Street currently takes up valuable real estate that could be producing tax dollars and economic benefits for Mascoutah. Removing parking in some segments of the corridor can actually free up space for wider sidewalks, more outdoor seating by restaurants, and a high-quality walking environment. It can also help the corridor in gaining more business as studies have shown that pedestrians and cyclists end up spending more on local businesses than those who drive. Better urbanism creates a place people want to linger, where they might discover business they hadn't intended to patronize.

The blocks behind Main Street already include large areas for parking that are inefficiently laid out. Reorganizing these parking lots into consolidated public parking spaces could accommodate the parking requirements for the corridor while maintaining easy access to all the businesses on Main Street. A successful partnership between the City and the private property owners could help realize this vision for downtown Mascoutah.

The zoning code could be amended to adjust parking requirements if a property is within a certain distance of these public parking lots. Many cities, such as neighboring O'Fallon, Illinois have provisions such as these in their zoning code for downtown environments.

CONSUMER SPENDING BEHAVIOR BASED ON TRAVEL MODE CHOICES

A study—Consumer Behavior and Travel Mode Choices—conducted for Oregon Transportation Research and Education Consortium, surveyed 1,884 people walking out of area convenience stores, restaurants and bars. At the bars, convenience stores and restaurants (all high-turnover sit-down pizza and Mexican spots), researchers talked to consumers on their way out and gave them a five to ten minute questionnaire about their demographics and travel behavior, how they had traveled that day, what they had spent, and how often they estimated they visited the business each month.

The results found that bikers actually out-consumed drivers over the course of a month. They often spent less per visit but cyclists and pedestrians in particular made more frequent trips (by their own estimation) to these restaurants, bars and convenience stores, and those receipts added up.

The study concluded that walkable and bikeable communities by definition facilitate a more frequent interaction between patrons and businesses. This means these bikers and pedestrians are also more regular customers.

Enhance the desirability of neighborhoods flanking the Study Corridor

The health of both Main Street and its adjacent neighborhoods are directly related. Most recent investment in Mascoutah's housing has been to the north. Most of this housing is on bigger parcels than what the older neighborhoods closer to downtown Mascoutah offer. However, recent market trends show that nearly two-thirds of home buyers now prefer smaller homes on small lots. Also, the typical buyers are both the empty nesters and young home buyers that are interested in less maintenance and prefer walkable neighborhoods. Most of these home buyers would fall under the Modest Income Home and Old and Newcomer tapestry groups, which as discussed earlier, are present in the St Clair county region. With expected steady population growth, Mascoutah's older neighborhoods can be better situated to attract such home buyers, and the neighborhood south of Main Street could take better advantage of its historic character and its proximity to downtown's Main Street.

Strategies such as a Rental Licensing program can improve and maintain quality of the housing stock in these neighborhoods. The City should also consider developing and implementing a code enforcement/property maintenance program to address problem properties and raise the expectations for housing quality in the neighborhood.

The City should also consider a review of the existing zoning codes to allow for a better, more diverse housing mix than what is currently permitted. It is equally essential that these neighborhoods to the south of Main Street are well connected with the rest of the City and have access to amenities such as public parks, trails, etc.

RENTAL LICENSING PROGRAMS

Many landlords in the city do an excellent job of maintaining their properties and serving their tenants; however, the landlords who do not proactively address maintenance, health, and safety issues create a number of challenges for their tenants and for the neighborhoods in which their units are located.

Rental licensing programs have been a powerful tool in many cities for addressing these problem properties by ensuring that all rental property businesses meet baseline standards for property maintenance. Owners of problem properties that routinely fail to comply with standards and put tenants at risk should be required to participate in various measures to ensure their compliance, such as property inspections prior to being granted an occupancy permit.

The Rental Licensing and Inspection Program in Lawrence, KS, which went into effect on January 1, 2015, requires all rental properties within the city to maintain a valid rental license. The program calls for interior and exterior inspections of dwelling units every three years to ensure minimum code standards are met to protect the life, health, safety and the general welfare of occupants.

The program is administered through an annual licensing fee of \$17 per dwelling unit (for a building having 1-50 units), varying as the number of units increases. Between inspections by the city, tenants are encouraged to work with their landlord to address maintenance issues that need to be corrected.

Opportunity Sites

As part of this market strategy, we identified multiple opportunity sites along the Study Corridor. These selections were based on the current vacancy conditions as well as suitability for future redevelopment. These key sites also tie in with the overall urban design vision for the Study Corridor. Based on the existing land-use, the urban design team is envisioning the Study Corridor in three segments; west, central, and east.

The west segment has a general residential character with significant commercial and office use. The Mascoutah Heritage Museum functions as the central anchor for this segment, lending it a historic charm. This segment currently has some commercial vacancies along with opportunities for mixed-use infill. Providing niche tenants that support and preserve the historical element of this segment.

The central segment is mainly commercial with some institutional uses such as the city hall. The most prominent intervention site in this segment is the site of the old Mill building (which has now been taken down) and the existing Mural building. These present opportunities for integrating active downtown uses, which will function as a central anchor to the Study Corridor. Although there aren't many sites for residential interventions in this segment, the immediate north and south blocks present some infill opportunities.

The east segment is primarily residential in character. Multiple vacant sites in this segment could be prime locations for providing medium-density residential infill products. We recognize this segment as an excellent opportunity to accommodate new residential growth to the south of the study area for consumers who would be interested in staying close to an active downtown. Below we provide segment-wise summaries for some of the identified opportunity sites and recommendations on potential uses based on the market analysis and overall market strategy.

Opportunity Site: Western Segment

220 West Main Street

Current status: Unoccupied Building

Potential Uses: Local ice-cream shop / Home furnishing store



The intersection of 2nd and Main Street is one of the most important sites in the western segment of the study area. The two buildings in this block were home to Bergheger Auto Network, a Chrysler car dealership that closed shop about two years ago. The buildings are currently unoccupied but have good bones and could become move-in ready with adequate reinvestment. With some minor façade improvements and landscaping, this site offers the potential to activate the western segment of the study area. It is essential to reuse these properties if possible, particularly since the market analysis revealed that achievable rents in this area are generally not high enough to support quality new construction. The City has an existing TIF in downtown, which currently has five years remaining before it expires. We know, through discussions with the City staff and stakeholders, that some revenue from this TIF was utilized for the renovation of the ACE hardware store on Main Street, which, since its renovation in 2018, has the highest market rent of any commercial building in the corridor. Approximately \$300,000 from the TIF have been committed to the ditch repair opposite City Hall, and some revenue would go towards matching funds for the Route 4-Main Street intersection project. It would also be extremely beneficial to use the existing TIF district in the downtown for renovation, as virtually all improvements made are considered eligible expenses.

Numerous examples of adaptive reuse of automobile-oriented facilities such as these are found all over the country. One of the best examples comparable in size is Olio in St. Louis, which involved the renovation of a dilapidated Standard Oil gas station. The building now houses an extremely successful small restaurant and wine bar with outdoor seating.

Olio in St. Louis



Opportunity Site: Central Segment

2 W Main Street

Current status: Unoccupied Building

Potential Uses: Mixed-use building (brewpub on the ground level and co-working space on the second and third floors)



The intersection of Jefferson and Main Street is one of the most important sites to revitalize the Main Street corridor. The sites located southwest of this intersection, the Mural building and the location of the former Grain Mill building (recently taken down by its owner) represent a significant opportunity to reinforce the quaint downtown experience in the central segment of the study area. Both these sites have the same ownership, and the owner has decided to donate the site of the Grain Mill building to the City. Once under the ownership of the City of Mascoutah, the site of the former Grain Mill building can be utilized to create a public open space for

outdoor events. This will function as a focal point to help draw more people in and encourage them to spend more time in the corridor.

This historic Mural building is a three-story brick building that is reportedly sound, has a well-maintained façade, and is already perceived as the town center by the residents of the City. This building is in a prime location to be developed as a rehabbed commercial use (e.g. a brewpub) on the ground floor with some type of office use on the top floors. A small business incubator / co-working spaces would be appropriate for the top floors of this property as these would require far less investment compared to a residential rehab. Renovation expenses will be considerable and the project may require multiple layers of incentives (façade grants, TIF redevelopment agreements, historic tax credits, etc.).

Development Strategies has only viewed the Mural Building from the exterior ground level. Based on our limited knowledge of the Mural Building and the fact that it has been unoccupied for many years, we assume that the structure will likely require extensive renovation/replacement of major systems (electrical, data, plumbing, fire suppression, HVAC), exterior envelope improvements, interior structural improvements, and significant investment in ADA accommodations. Based on similar projects in the region, our data shows that this type of renovation, excluding land costs and soft costs, is between \$100 and \$300 per square foot. As this range represents only an estimate of assumed similar properties in the region, we recommend that the property owner further investigate the renovation costs with a licensed architect, engineer, or cost estimator to establish the true renovation cost. Despite the efforts involved, bringing this property back into productive use should be a priority for Mascoutah. It will significantly improve the aesthetics of the corridor and provide a strong anchor for the downtown.

A local example of the renovation of a similar historic structure can be found in O'Fallon, Illinois. The city-owned building at 200 North Lincoln Avenue was vacant for many years and was in a state of disrepair. The City issued an RFP for redevelopment and was able to sell the building at a low cost to help subsidize the expensive renovations. The City's expectations and requirements were set forth in the sales contract and TIF redevelopment agreement, allowing for additional control of this key property. Since the City owned the property prior to the creation of the TIF district (giving it a \$0 base value), the incentives were particularly robust and significantly improved the feasibility of the project.

Opportunity Site: Eastern Segment

320 East Main Street

Current status: Unoccupied Building

Potential Uses: Bed and Breakfast establishment



Mascoutah has an excellent supply of historic, Victorian-style homes in and around downtown, especially along South Street, and Railway Street. While most of these are occupied for single-family residential uses, there are a few, such as 320 East Main Street, that are currently vacant. The traditional Bed and Breakfast model is appropriate for this type of market, given the building stock and preference for single-family homes or mansions (e.g. Victorian style). Bed and breakfasts are a great way to attract more people to the Main Street corridor, making it more active while supporting and sustaining small businesses on the corridor. Uses such as this can also help preserve and celebrate the Mascoutah's image as a historic town.

Other comparable historic downtowns, which include Hermann (MO), Washington (MO), and Paducah (KY), provide excellent examples of smaller-scale historic lodging. In some cases, a successful ground floor restaurant can improve economic feasibility and provide consistent revenue streams in slower visitation seasons.

ZONING REVIEW

The zoning recommendations are geared towards providing appropriate guidance for the zoning ordinance for the City of Mascoutah such that the City can support desirable development both in the Main Street corridor as well as the areas adjacent to this corridor and is able to fully realize the vision laid out in this great streets planning process. We recommend unifying Main Street corridor into one zoning district (that could be named Main Street District) such that the zoning along the whole corridor is consistent as opposed to the current condition which is a variation of three different districts (downtown commercial, general commercial, office). This new Main Street district can be designed to allow a mix of uses such as artist studios, live-work units, and custom manufacturing. This district shall also accommodate various types of residential uses ranging from mid to high-density developments with specific design standards. Also, this district can have regulations on maintaining pedestrian access throughout the corridor.

We also recommend developing design standards for conditional uses that might be permitted in the district upon review. To the right are design guidelines adopted by the city of Belleville, IL for buildings located within its historic districts.

DESIGN GUIDELINES BELLEVILLE HISTORIC DISTRICTS

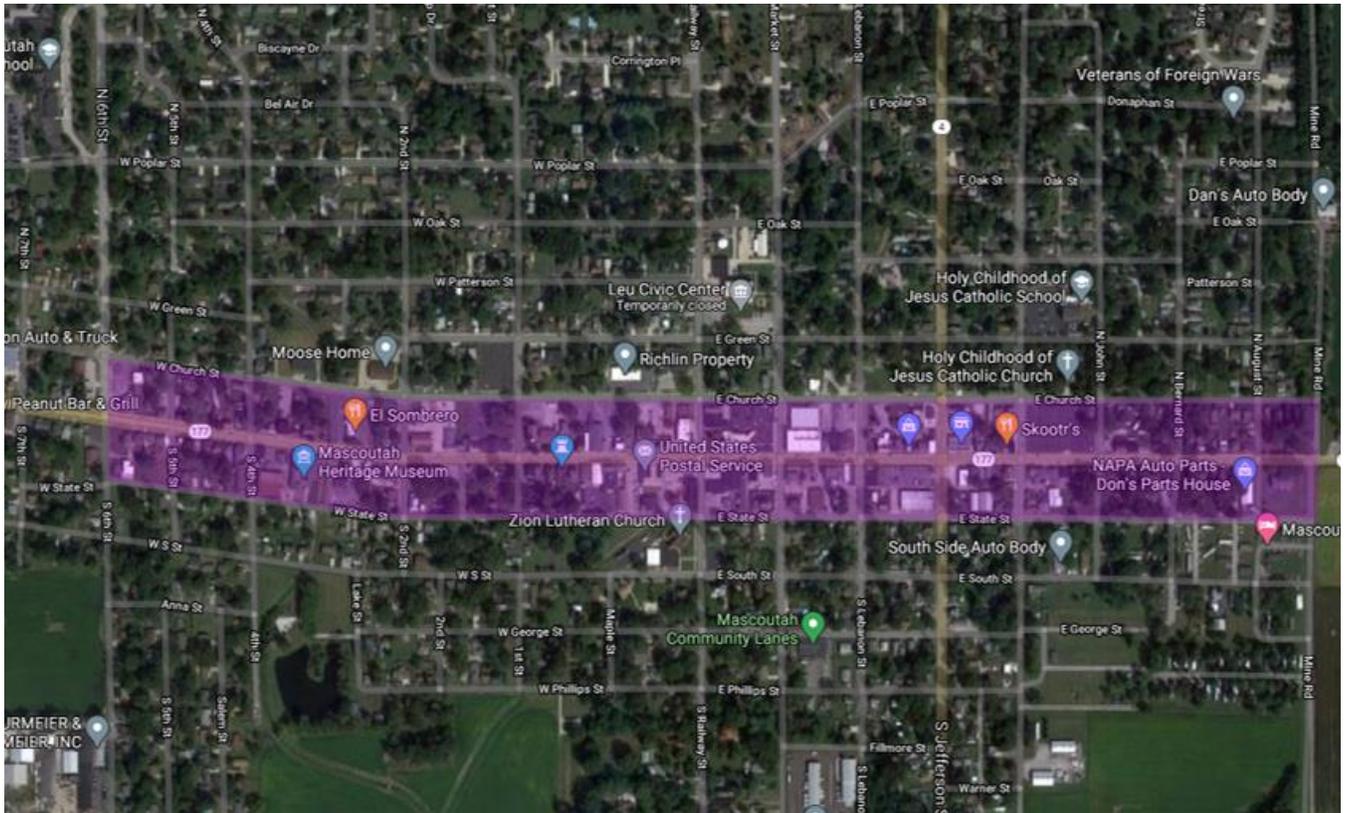
The design guidelines policy manual for historic districts in Belleville, IL is intended to inform property owners, architects and developers of the proper restoration techniques, development/restoration guidelines, approval and review procedures and to establish minimum standards for property re-development in the Belleville Historic Districts.

Belleville’s historic districts contain a collection of historic buildings from the 19th and early 20th centuries. Design review guidelines assist in the preservation and maintenance of the unique appearance of these districts.

When altering, original storefronts and facades are encouraged to be repaired instead of replacing. Architectural and decorative features original to a building are required to be preserved, maintained, and repaired. New buildings in Belleville’s commercial areas are required to be compatible with historic buildings in scale, height, materials, orientation, shape, placement, and rhythm and proportion of openings (see image below). It should be compatible with adjacent dwellings or should blend in with the district through replication.



Recommended Main Street District



Additionally, we recommend an Urban Family District to the south of the corridor. This district is expected to establish a more inclusive, middle-density zoning in the City. It is meant to be transitional between existing low-intensity residential use and the Main Street corridor and allow for products such as small-lot single family, attached duplexes, and townhomes. Also, we recommend that this district allows for accessory dwelling units that meet specified criteria so that existing as well as new families moving into the neighborhood have opportunities to account for their changing needs. Also, accessory dwelling units can help Mascoutah's senior population meet their needs in terms of support and finances and help them age in place.

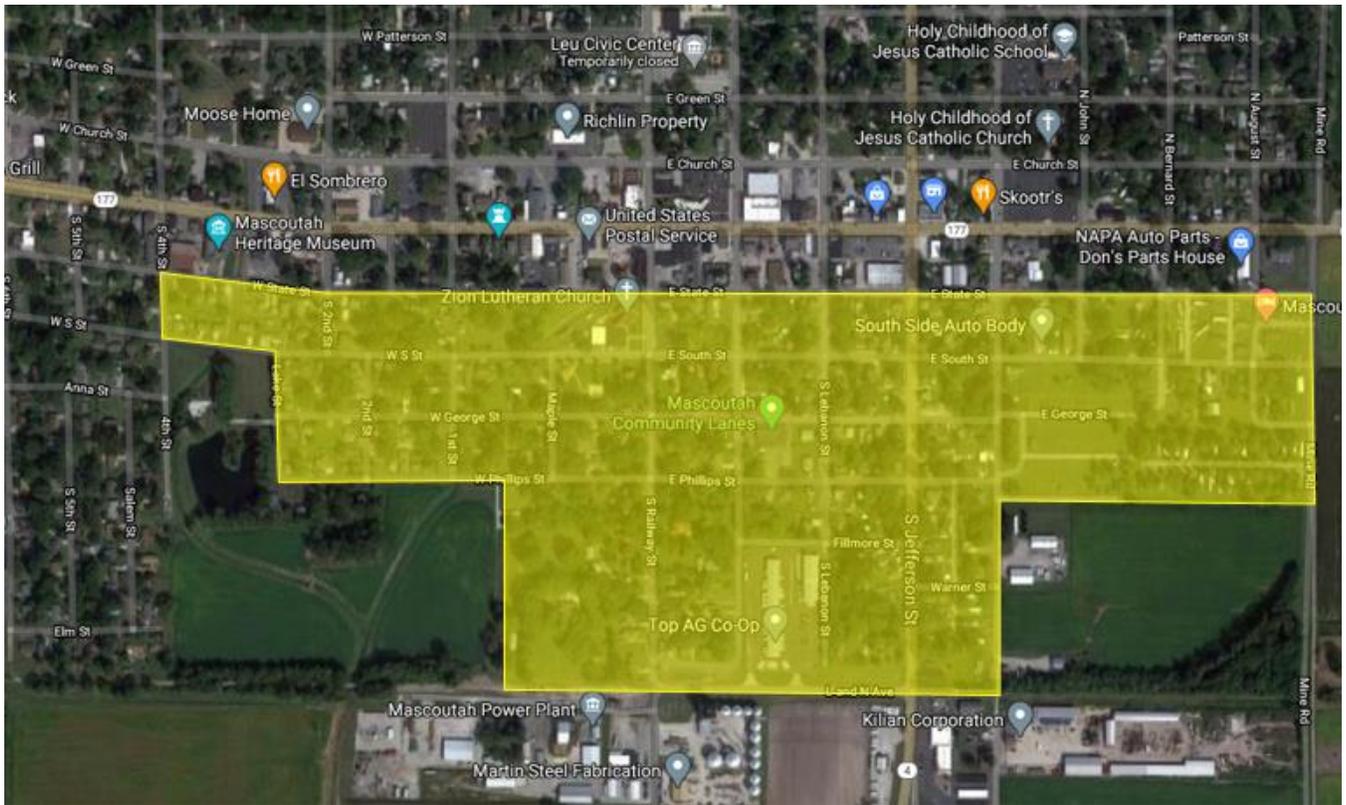
ACCESSORY DWELLING UNITS

An accessory dwelling unit (ADU) is a really simple and old idea: having a second small dwelling right on the same grounds (or attached to) your regular single-family house, such as an apartment over the garage or a tiny house (on a foundation) in the backyard. An ADU is not a separate property; it has the same owner as the primary dwelling.

ADUs can assist homeowners by offering a chance for extra income, can offset property taxes, and can offset the cost of home maintenance and repair.



Recommended Urban Family District



IMPLEMENTATION

The following matrix is a tool that the City and its partners can use to understand how various strategies fit together. Each strategy has a different timeframe for implementation and different partners involved. This matrix is expected to serve as a resource for aligning efforts, outlining next steps and costs involved.

IMPLEMENTATION MATRIX

| Recommendations | Implementation Timeframe | | | Key Partners | Subsequent Work Required | | |
|---|--------------------------|------------------------|------------------------|---|---|--|---|
| | Short-Term 1-5 Years | Mid-Term 5-10 Years | Long-Term 10+ Years | | Scope | Entity Responsible | Expected Costs |
| Attract new residents and visitors to Mascoutah | | | | | | | |
| Attract and retain early workforce population with continued improvements to the building stock, enhanced shopping, dining, and entertainment options. | ○ | | | City, Property owners, Small business groups, Scott Air Force Base | Creation of a detailed plan that analyses market capacity for community desired improvements and identifies critical investments that will serve to catalyze on-going workforce population growth | City | ~\$50k - \$100k |
| Encourage desirable rental units downtown by providing rehab incentives and landlord management assistance. | ○ | ○ | | City, Landlords, Chamber of Commerce, District Staff | Analysis of necessary investments for rental properties and creation of a program that administers those incentives. | City | \$250k - \$500k Plus annual administration costs |
| Revitalize businesses along Main Street | | | | | | | |
| Continue providing TIF funds for a matching grant reimbursement program to help with façade improvement efforts. Develop a stronger outreach program to attract property owners. | ○ | | | City, Property owners | Identify community liaison who conveys the benefits of the program to property owners and assists them through the application process | City | Staff time Plus TIF expenditures |
| Provide additional assistance to ensure that professional design advice is utilized when making facade improvements. | | ○ | | City, Property Owners | Develop detailed design recommendations | City | Staff administration time Plus grant allocations |
| Invest in a truly impactful business attraction and retention policy that can streamline processes such as permitting, networking, etc | | ○ | ○ | City, County, Utilities, Small business groups, Chamber of Commerce | Developing detailed strategy including targeted industries, financial incentives, permitting assistance etc. | City | \$100k - \$150 Plus annual administration costs |
| Invest in a small business incubator to offer collaborative work environment with mentoring, networking opportunities, funding support and shared equipment. | | | ○ | City, small business groups, regional institutions | Create partnership that identifies specific incubator types and raise capital to invest in physical space and on-going operations | Community development group/ Institutional partners | > \$500k |
| Encourage existing property owners to renovate units on their upper floors by using incentives such as tax abatement, historic tax credits, and affordable housing incentives. | ○ | ○ | | City, Property Owners | Identification of person/ group who acts as a liaison to help property owners | City/ Community development group | Staff time dedicated to this effort |
| Increase Main Street's walkability and attractiveness | | | | | | | |
| Reorganize parking lots in the blocks behind the Main Street into consolidated public parking spaces to accommodate the parking requirements for the corridor while maintaining easy access to all the businesses on Main Street. | ○ | | | City, Local Business Owners | Creation of agreements allowing shared use of the parking lots | City/ Legal counsel | ~ \$100k - \$150k for legal advice |
| Consider zoning code amendment to adjust parking requirements if a property is within a certain distance of the consolidated public parking lots. | | ○ | | City | Discussion and drafting of potential zoning codes. | City/ Zoning code specialist | \$25k - \$50k for zoning code specialist |
| Enhance the street and sidewalk conditions of the Main Street corridor to improve aesthetics and encourage reinvestment in the study area. | ○ | ○ | | Chamber of Commerce, District Staff, City | Create an overarching vision for street and sidewalk improvements and wayfinding throughout the corridor | City, Consultants, IDOT | Refer to cost estimations provided within the white paper by CBB |
| Enhance the desirability of neighborhoods flanking the Study Corridor | | | | | | | |
| Consider implementing a Rental Licensing Program to improve and maintain quality of the housing stock in these neighborhoods. | | ○ | | City | Developing detailed program that outlines rental property requirements and provides methods to implement the program | City | Staff time to develop program Ongoing admin. costs covered by program fees |
| Develop and implement a code enforcement/property maintenance program to address problem properties and raise the expectations for housing quality in the neighborhood. | | ○ | | City, Landlords & Property Owners | Developing detailed code enforcement policy and identify funding sources | City | Cost of a part-time/ full-time position, Ongoing program administration costs |
| Review existing zoning codes to allow for a better, more diverse housing mix than what is currently permitted. | ○ | ○ | | City, Property Owners, Developers | Review of existing zoning code and development of revised zoning code that better aligns with Mascoutah's vision for future development. | City/ Zoning code specialist | \$25k - \$50k for zoning code specialist |



MASCOUTAH GREAT STREETS
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APPENDIX B





| | |
|-------------------------------|----|
| Existing Conditions | 5 |
| Engagement Summary | 13 |
| Plan and Recommendations | 18 |
| Implementation & Partnerships | 42 |

How would you describe Mascoutah to someone Visiting?

“Small town vibe, with pride”

“Safe”

“Home”

“Great place to raise your kids”

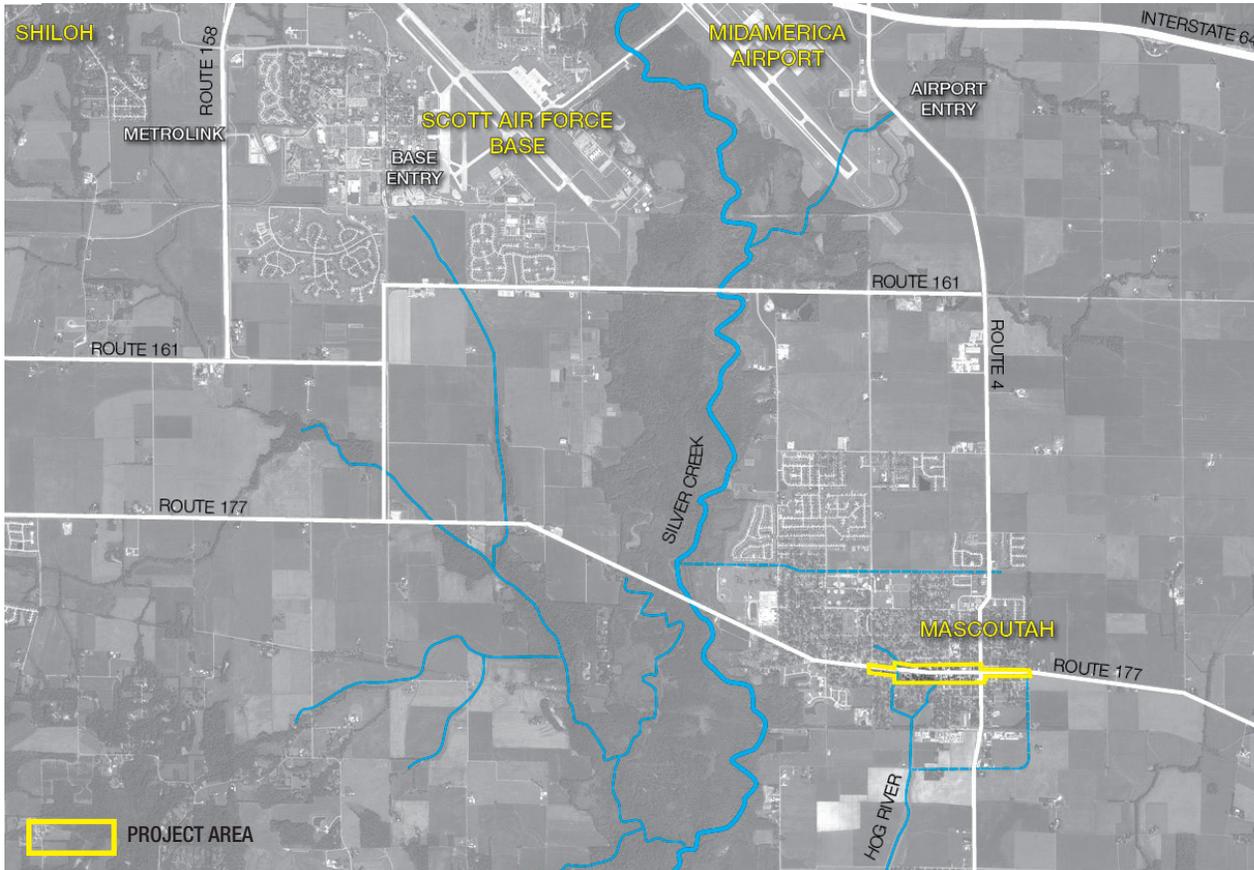
“Friendly residents”

“Quiet bedroom community”

“Great park (Scheve)”

“Quaint”

INTRODUCTION



This white paper represents the experience and recommendations of the DTLS Landscape Architecture Studio as part of a multi-disciplinary team of professionals led by East West Gateway and the City of Mascoutah. The project team included urban designers, market analysts and transportation engineers. DTLS' focus was on the environment.

For the purpose of this project, the term "environment" constitutes a broad view of outdoor natural and man-made conditions. These include stormwater management, streetscapes, street trees, trails, identity and wayfinding, etc.

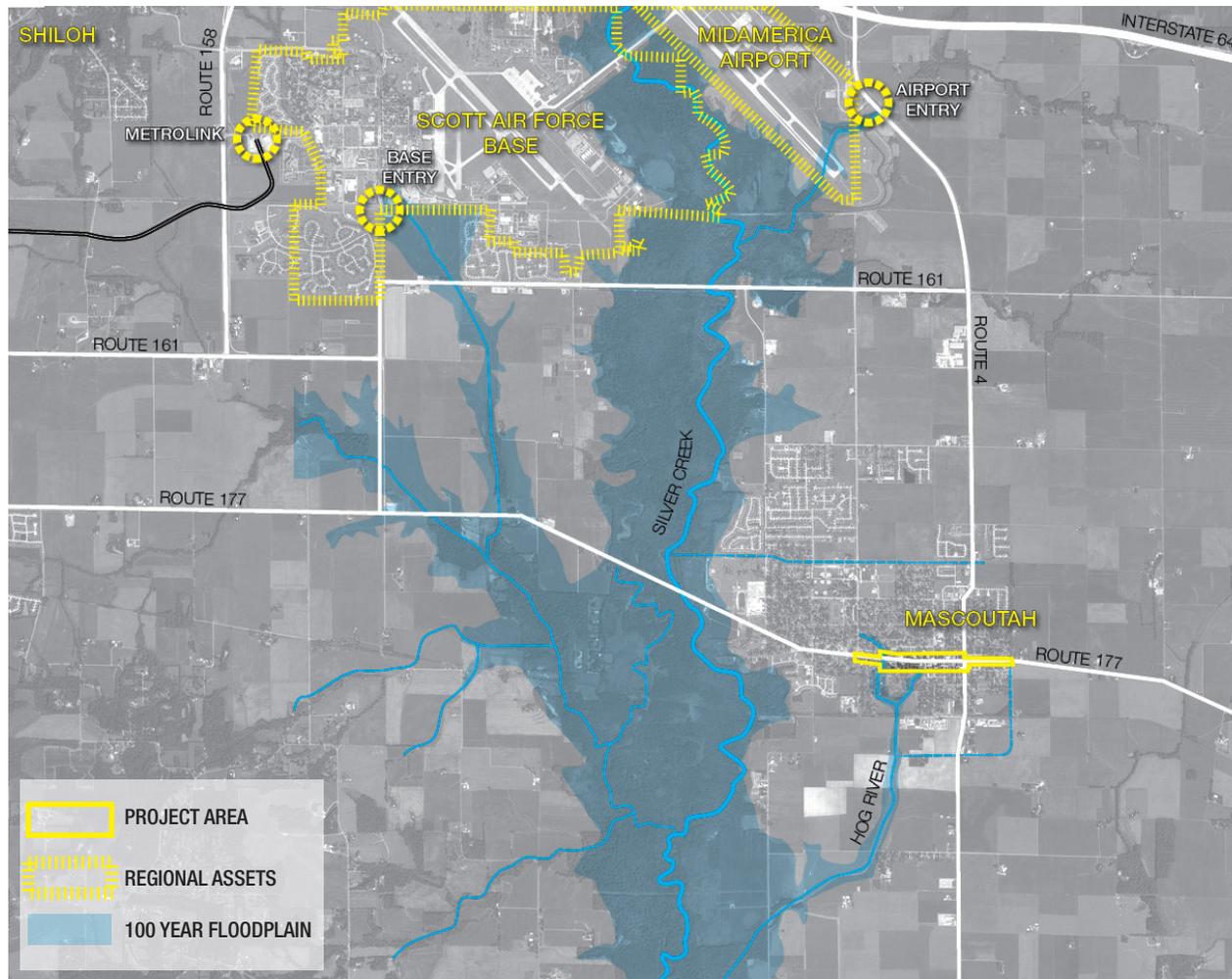
This document is divided into three sections. The first section details the existing conditions of Mascoutah from a regional and project specific perspective. Next is a summary of the public engagement process and the final portion recommends specific plan elements and implementation strategies related to environmental factors.



EXISTING CONDITIONS

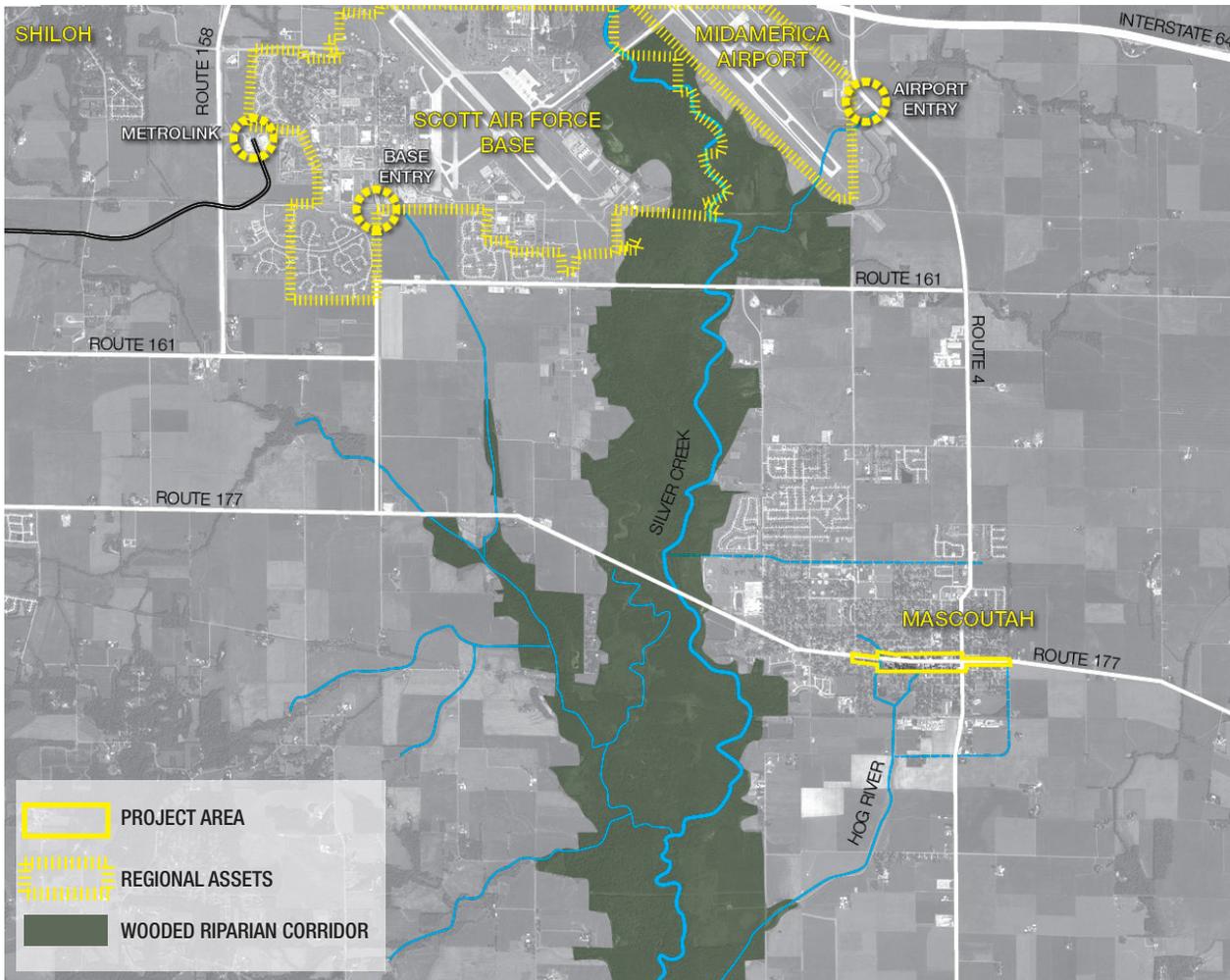
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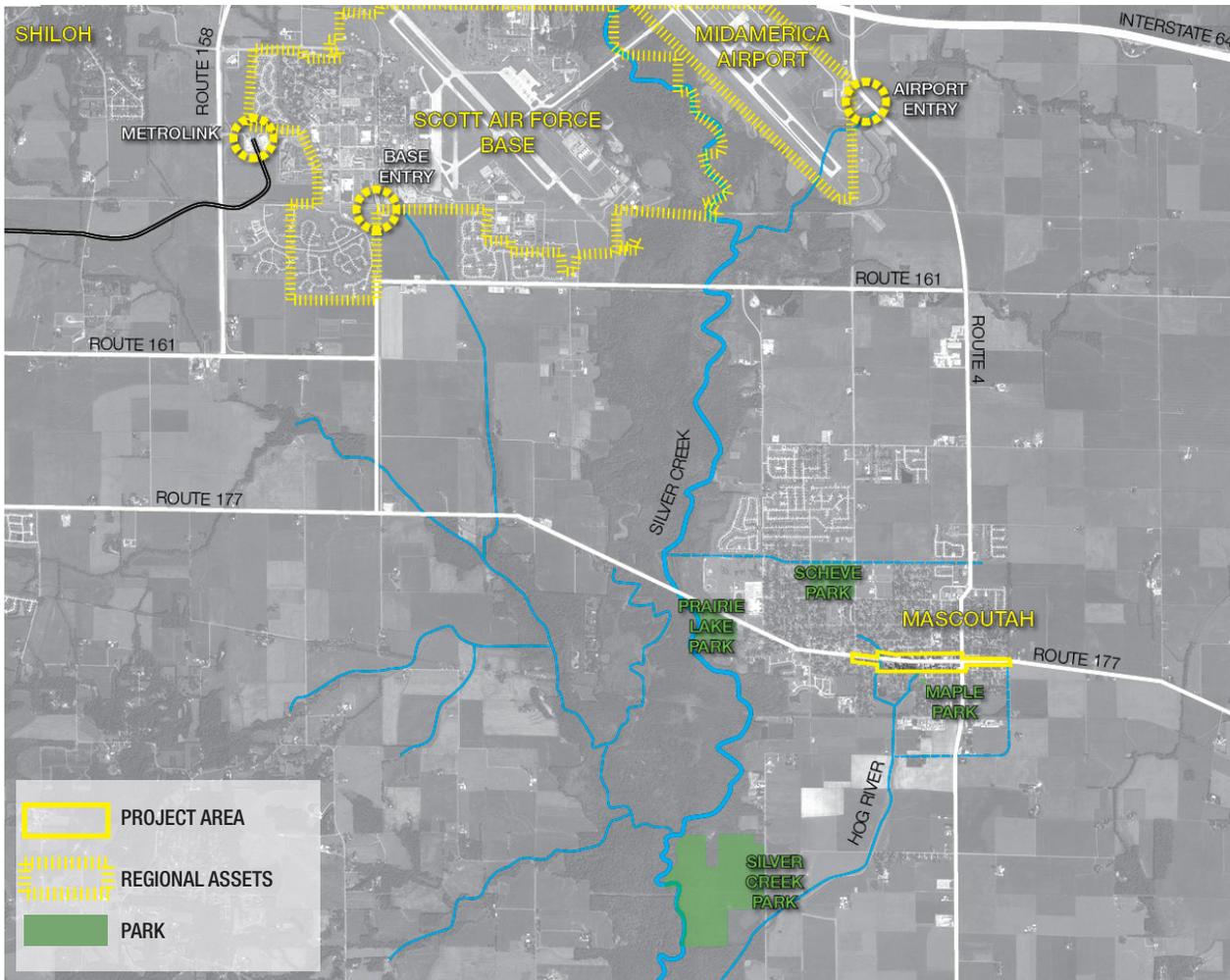
The City of Mascoutah is bordered on the west by the prominent Silver Creek drainageway and associated flood plain. The overall flat nature of local topography means that Silver Creek’s floodplain is extensive at an average of a mile wide, and is a defining natural feature of the city. Silver Creek and associated tributaries are a formidable boundary between Mascoutah and Scott Air Force Base, MidAmerica Airport, and the neighboring community of Shiloh. However, the waterways are also a common connection between each entity.

The areas shaded in blue represent the 100-year floodplain along Silver Creek. This floodway also extends into downtown Mascoutah through the Hog River and Pig Creek. Both of these waterways underwent extensive public works projects in the 1960s to channelize their streambanks. As a result, the steep edges must be maintained as lawn, and pools of water are treated regularly to keep mosquitoes and other pests at bay.



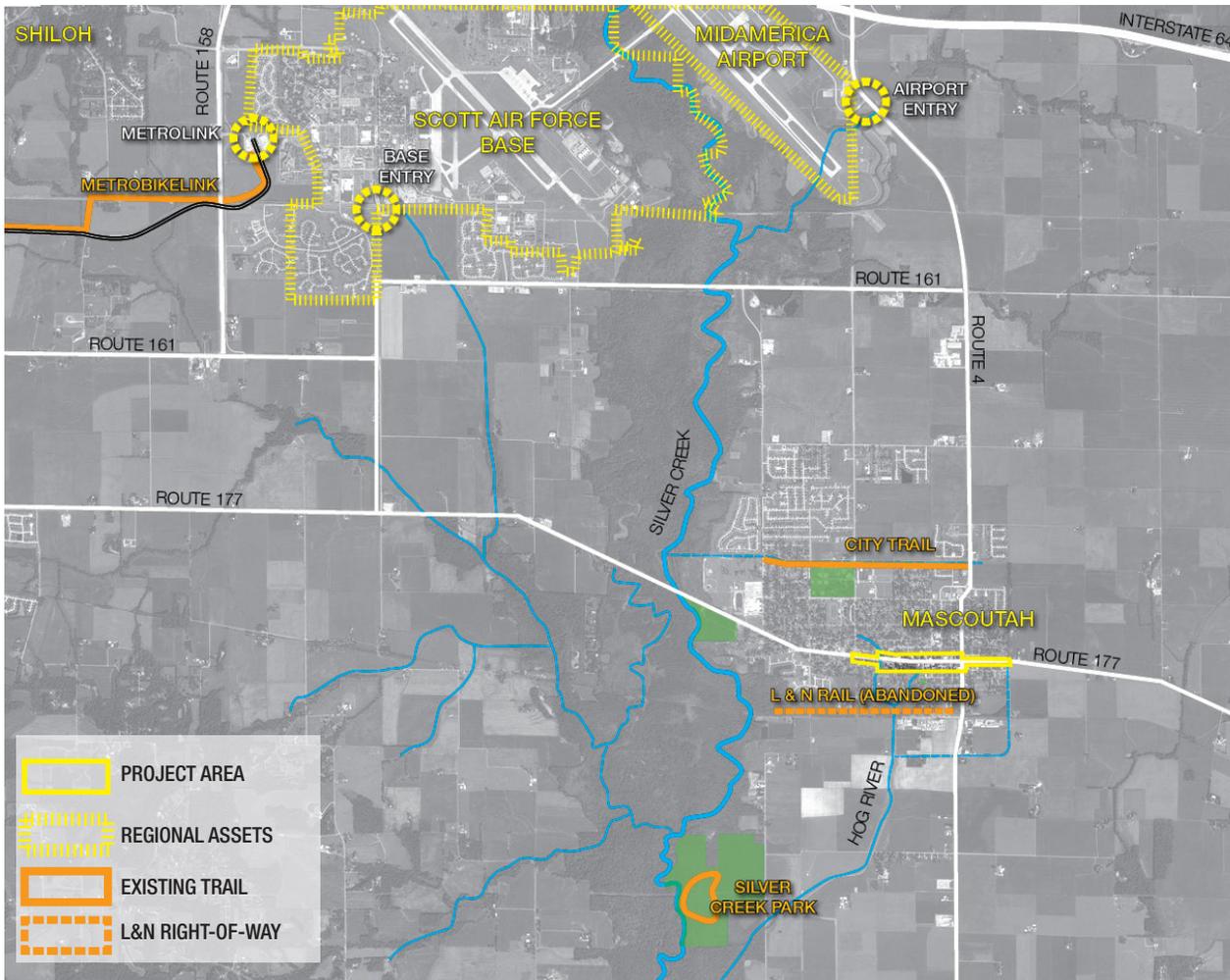
The land within the Silver Creek floodplain remains undeveloped due to frequent flooding. This expanse of bogs and forest is underscored by a tapestry of private property owners, including parcels with no access.

Unbroken riparian corridors are becoming increasingly rare as urban and agricultural areas encroach. The integrity of biological communities is dependent upon connectivity; these undeveloped areas are essentially islands, and are the last refuge for some native species. The mile-wide expanse of floodplain alongside Silver Creek is a benefit for wildlife.



The majority of public parks are located within downtown Mascoutah, including Prairie Lakes, Scheve, and Maple Parks. The one outlier is Saint Clair County’s Silver Creek Park to the south. City-owned parks include the following amenities:

- Maple Park: basketball, pavilion, playgrounds, paths
- Prairie Lakes Park: boating, fishing
- Scheve Park: baseball, BBQ, disc golf, football, pavilion(s), playgrounds, paths, volleyball, swimming pool, tennis, dog park
- Saint Clair County owns Silver Creek Park. Amenities include: hiking, pavilion(s), restroom(s), running/walking paths



The newly-completed city trail in northern Mascoutah is popular for walking, jogging and cycling. This trail is located along the south bank of the Hog River and connects multiple schools and Scheve Park. Mascoutah's other option is the loop trail at Silver Creek Park.

To the west, near Shiloh, the MetroBikeLink trail system connects the Scott Air Force Base Metrolink station to Belleville.

In most cases, the existing greenway systems have been implemented alongside infrastructure such as streams and transit. The abandoned L&N Railroad Right-of-Way to the south of downtown Mascoutah is a candidate for trail conversion.



Main Street is owned and maintained by the State of Illinois between the curbs. That means the City of Mascoutah is able to modify the sidewalks as needed to enhance the downtown experience for visitors and residents.

Currently the sidewalks along Main Street are wide enough for pedestrians to pass comfortably, but implementing outdoor dining or seating is currently a challenge.

Street trees are not present, but a lack of overhead utilities will not hinder their growth in the future, though IDOT limits tree size. The majority of overhead utilities are for street light power, and can be buried as the sidewalk is replaced or repaired.

There is not a consistent standard for lighting type, banners, signs, wayfinding, or other common streetscape elements at this time. A new lighting standard will be installed during the reconstruction of Main Street centered on the Jefferson intersection, but that was not completed at the time of our visit.





On a local level, water is an important factor to consider in regard to the safety of vehicles and pedestrians on Main Street. Current stormwater infrastructure is commonly present at intersections. The blocks are long between cross-streets in downtown Mascoutah; as a result, mid-block ponding is common after rain events. Ponding can be dangerous, especially in the winter.

Main Street is owned and maintained by the State of Illinois between the curbs. This means that the options for improving stormwater drainage along Main Street are limited. However, the City does have the ability to reduce contributing storm volume by treating it on the sidewalk and side-streets. Some side-streets are excessively wide for their adjacent land uses and are candidates for stormwater volume reduction best management practices.

Typical Existing Stormwater inlet frequency and IDOT ownership



Main Street curb deterioration at low spot



Typical inlet at intersection

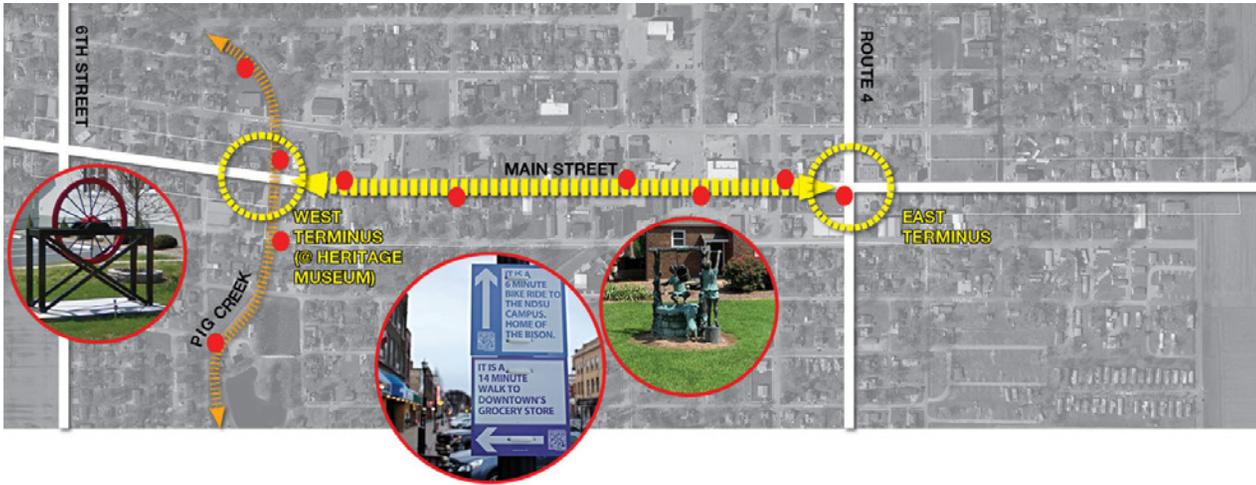


A walk down Main Street can tell you a lot about Mascoutah, but could tell you more of the history and narrative of the future. Civic pride is evident, and the large mural along Main Street at Railway Street celebrates the region’s long history. At the Mascoutah Historical Society, a dedicated group of volunteers has created an extensive history museum.

Outside of the museum’s walls, there are a few inconsistencies in how Mascoutah is portrayed. Entry signage is simple along 177, but bold and traditional on Route 4. Main Street benefits from small whimsical sculptures on the east end, and historic industrial relics on the west.

This project may be an opportunity to define what identity Mascoutah would like to promote moving forward, and to create spaces along Main Street which help the community share its collective vision.

Above: Existing Downtown Signage, Art, and Artifacts



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The emergence of Covid-19 in early spring 2020 had a profound impact on this project in its entirety, but mostly on the ability to engage directly with the public. The original plan for a four day in-person charrette was quickly rendered impossible, and so the project moved to an online format. We presented to the public three times via pre-recorded video and accompanied those engagements with online survey questions. The first presentation was focused on existing conditions, the second on plan options, and the third on the final plan categories.

The three public engagement efforts were not our only way to reach out to the community. The team also conducted a series of interviews with prominent local stakeholders, as well as engaging the Illinois Department of Transportation.

The following summaries are grouped into categories depending upon general topic.

Silver Creek

As mentioned previously in the existing conditions, Silver Creek is a prominent feature of Mascoutah's environmental context. It is both a barrier, and a potential means by which the city may connect to regional destinations and nature. The intent of our early questioning on the topic of Silver Creek was to understand the public's general perception of the riparian corridor and how it might be used as an asset.

In general, 64% of our respondents do not access Silver Creek for recreational activities. Of those that do, most engage with it at the St. Clair County Nature Preserve to the south of town. As the only public space along the corridor, this makes sense.

We heard during our stakeholder interviews that business owners are eager for ways to make Mascoutah a destination. Interestingly, 88% of our surveyed respondents travel away from Mascoutah to experience the outdoors. They travel away from Mascoutah to Missouri and southern Illinois to hike (40%), fish (20%), camp (15%) and walk (12%).

We also heard that Silver Creek is very prone to frequent and long lasting flood events. One of our interviews was held shortly after an intense downpour of 2-4 inches that highlighted the environmental challenge of utilizing Silver Creek. This challenge is

compounded by the tapestry of private property owners along its banks which would be difficult to consolidate into public property or get easements from.

Our overall impression is that Silver Creek is a truly substantial obstacle to surmount. If the challenges could be overcome, it would be an amenity for Mascoutah and surrounding communities, offering hiking, fishing and camping, but broad community support and partnership with larger governmental agencies would be necessary. For these reasons, improvements to this area seem to be long term, if not impossible visions, and our public engagement guided us to pursue other more feasible environmental opportunities.

Trail Systems

The intent behind our survey questions regarding trail systems was specifically tied to our contemplation of Silver Creek as a place for Mascoutah to connect with regional amenities, and were part of the second round of engagement. We asked respondents what regional destination they would like to connect to most, and if they would use those connections for their commute. Overall, the majority favored a connection directly west, to the MetroBikeWalk trail, followed by Scott Air Force Base and last with MidAmerica Airport. Only 31% said that any of these routes would allow them to commute and

ENGAGEMENT SUMMARY

65% said they would only use them for recreation.

This feedback, during the second round of engagement, was enough to act as additional confirmation that Silver Creek is best left as it is.

Main Street

Main Street Mascoutah already has many great features including wide sidewalks, historic architecture and small blocks, the hallmark of a great historic downtown area. Our initial survey questions were intended to get an understanding of how residents feel about downtown in order to provide us with challenges that need to be addressed.

To our surprise, the overall consensus is that Mascoutah is a comfortable place to be. Improved sidewalks was by and far the most popular choice for respondents, with 65% saying that this one change would make them feel most comfortable. Lighting was the second choice at 25%, with trees at 9%. This feedback was consistent with what we heard in our stakeholder interviews where improved sidewalks were favored over all other options for improvements on Main Street.

In addition, the majority of respondents visited downtown in the warmer months and during the business day. We heard that there wasn't much of a night life to keep people

downtown after dark and so this may be a reason why lighting was not more popular.

This feedback provided us with support for a simple streetscape improvement project focused mostly on pavement and lighting.

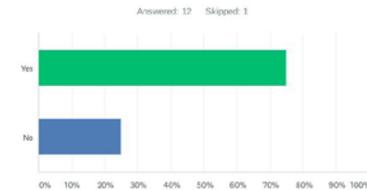
Parking

Main Street's moderate width makes it impossible to achieve parking on both sides of the street in the central segment of Main Street. The existing arrangement places parking on the south side, thereby favoring businesses on that side of the street. We wanted to gauge support for the removal of parking from Main Street during the third round of engagement. What we heard is that the majority of visitors downtown use side streets or traditional lots to park. 46% always park on side streets in traditional lots, and the remaining 53% do so sometimes. Parallel parking on Main Street is sometimes used by 61% of respondents and rarely by the other 38%.

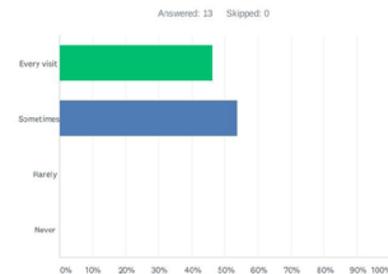
75% of our respondents favored additional sidewalk over the parallel parking spaces on Main Street. This is supported by the fact that these spaces appear to be underutilized and the primary improvement recommended in earlier engagement rounds was improved sidewalks.

These findings support our recommendations for the removal of parallel parking on the

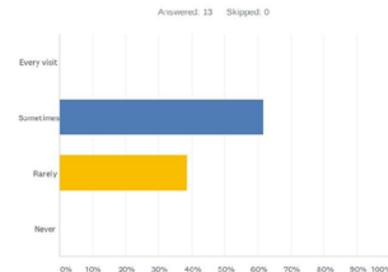
Q1 Do you place a greater value on the additional 4 feet of new pedestrian space, on both sides of Main Street (in the center segment of our Main Street study area), as a better use than the current one-sided parallel parking? The additional 4 feet of sidewalk increases the total from 8 to 12 feet for pedestrians and includes lighting, benches, signs, and trash receptacles.



Q5 How often do you find places to park on traditional lots or side streets when visiting downtown?

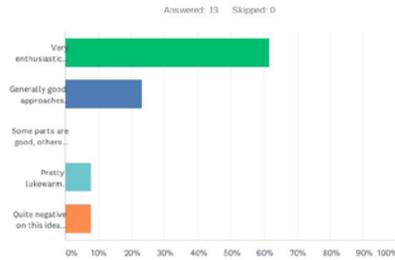


Q4 How often do you parallel park your vehicle downtown on Main Street?

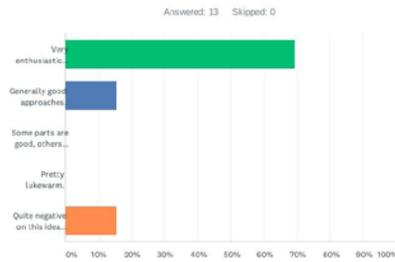


ENGAGEMENT SUMMARY

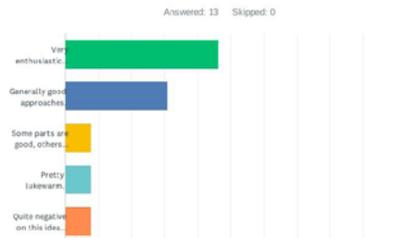
Q6 What is your reaction to the overall idea discussed of adding curb extensions to the western segment (6th Street - 2nd Street) of Main Street?



Q7 What is your reaction to the overall idea discussed for removing parallel parking and increasing the sidewalk width in the central segment of the corridor? (1st Street - Independence)



Q8 What is your reaction to the overall idea discussed for adding curb extensions and increasing sidewalk width to the eastern segment (6th Street - 2nd Street) of Main Street?



central segment of Main Street in favor of parking mid-block and off Church Street.

Curb Extensions

Curb extensions are proposed for a variety of reasons within the document, but during the third round of engagement we asked specifically about their use for art/sculpture and at intersections. 69% of respondents agreed that it would be worthwhile to reorganize the sculptures into the side-street curb extensions as a Main Street art walk.

For Main Street itself, we suggested that the east and west ends continue to have parallel parking on both sides, and the intersections be equipped with curb extensions to reduce pedestrian crossing distances and calm traffic. For the western end, 61% of respondents were very enthusiastic about adding curb extensions. 46% were very enthusiastic on the east end and another 31% felt that it was a good approach.

Trees

As the project developed, we learned that the Illinois Department of Transportation (IDOT) controls and owns Main Street, but that the city is responsible for maintaining the sidewalk. As a result, the state's transportation rules and regulations are applied to Main Street for traffic safety. This includes a limit on tree size. IDOT will

only allow trees that are below 4" caliper at maturity. This makes the street trees primarily aesthetic for automobile drivers, as the canopy will be too low for pedestrian shade and will block business signage.

We wanted to know what residents thought about this issue and if we should keep trees in the plan. 58% of respondents felt as though it would still be worthwhile to include street trees in the central corridor, even though small. We asked if those who own property along Main Street would be open to planting on their property, but the majority of the respondents did not own property on Main Street. Those that do are evenly split on their support to plant trees on their private property.

ENGAGEMENT SUMMARY

IDOT Interview & Comments

The Illinois Department of Transportation (IDOT) interview and subsequent comments have been timely and constructive for our project process and the City of Mascoutah. IDOT has not begun their design process, but will do so following this project and expect the work to be funded sometime between 2022 and 2026. Our involvement could not have come at a better time.

While the overall conversation with IDOT was enlightening, the following responses and requirements from the agency specifically related to the environmental aspects of this plan.

IDOT expressed interest in building what the community sees as the best option. They suggested that Mascoutah consider what has been completed recently on Route 159 in Collinsville, Illinois as an example, as well as the current resurfacing project already underway in downtown Mascoutah. They are open to moving curb locations, parking adjustments, decorative lighting, and pavement. However, the following are relevant rules and regulations from their 9/22/20 presentation response document:

- A WB-65 turning vehicle must be accommodated with encroachment on all public side roads, in addition to the 30' minimum width. This will impact the

design on any proposed bump-outs or stormwater public gardens and their location/orientation. Seating within bump-outs or stormwater gardens should be limited to the City's roads, and not along IL 177.

- All sidewalks along a state route must be on public ROW and maintained by the City. There are some locations in Mascoutah that the existing sidewalk may not be on public ROW.
- The current construction project has brick stamped pavement – this is an aesthetic design feature that if the City chooses to continue this on the IDOT project, the City will be required to pay the extra cost for that work and likely enter into a maintenance agreement with IDOT to maintain that surface. The Department would not be able to patch or maintain stamped concrete.
- If desired, the City will need to work with IDOT to secure a design exception for narrow lanes for the future reconstruction of Main Street between N County Road and Lebanon Street and between Independence Street and Mine Road in order to maintain the same lane widths as those recently constructed on Main Street between Lebanon Street and Independence Street.
- The City will need to work with IDOT to incorporate design features such as stamped concrete, wayfinding signing,

and decorative light fixtures for the future reconstruction of Main Street between N County Road and Lebanon Street and between Independence Street and Mine Road as were recently incorporated in construction on Main Street between Lebanon Street and Independence Street. Incorporation of these elements may include IDOT/City cost-share and/or maintenance agreements. Any special features will need to adhere to current IDOT Statutes and Department policies.

- Food trucks cannot be permitted to operate within the parking lanes.
- Other - IDOT encourages implementation of parking lots and bike access off of Main Street. However, removal of parking spaces for parkettes and outdoor dining is not a permitted use of State right-of-way.
- In addition, it was clear during our call that large canopy street trees will not be allowed. Smaller understory trees that will grow no larger than 4" caliper can be planted, but the species and location will have to be approved by IDOT.

IDOT's requirements for Main Street seem limiting. However, for Mascoutah they may be a net benefit because the State will pay to replace Main Street in a manner best suited to the residents, and as we described earlier, the residents aren't asking for a robust streetscape overhaul. The resident's requests for new pavement is perfectly in line with the offerings of the state.



PLAN AND RECOMMENDATIONS

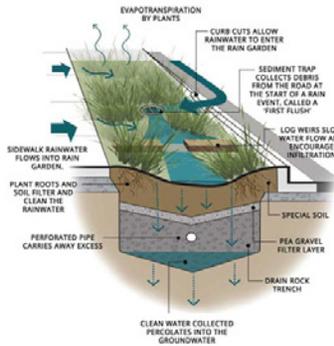
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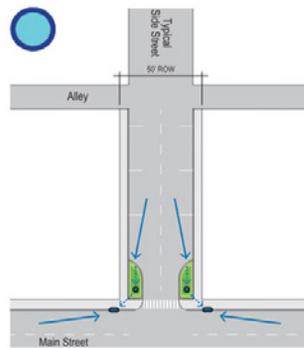


One challenge that we've heard is that stormwater drainage is an issue along Main Street. As an IDOT route, it may be a challenge to find ways to reduce stormwater runoff within the Street. However, the City of Mascoutah has complete control over what happens on the side streets. Runoff from the side streets contributes to the drainage issues on Main, so reducing volume from side streets will help Main Street drain more efficiently.

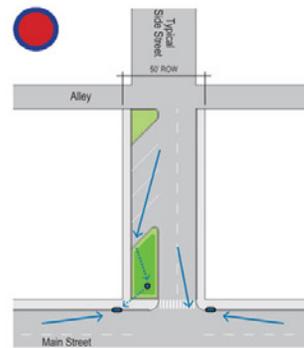
TYPICAL RAIN GARDEN



PARALLEL PARKING RAIN GARDENS



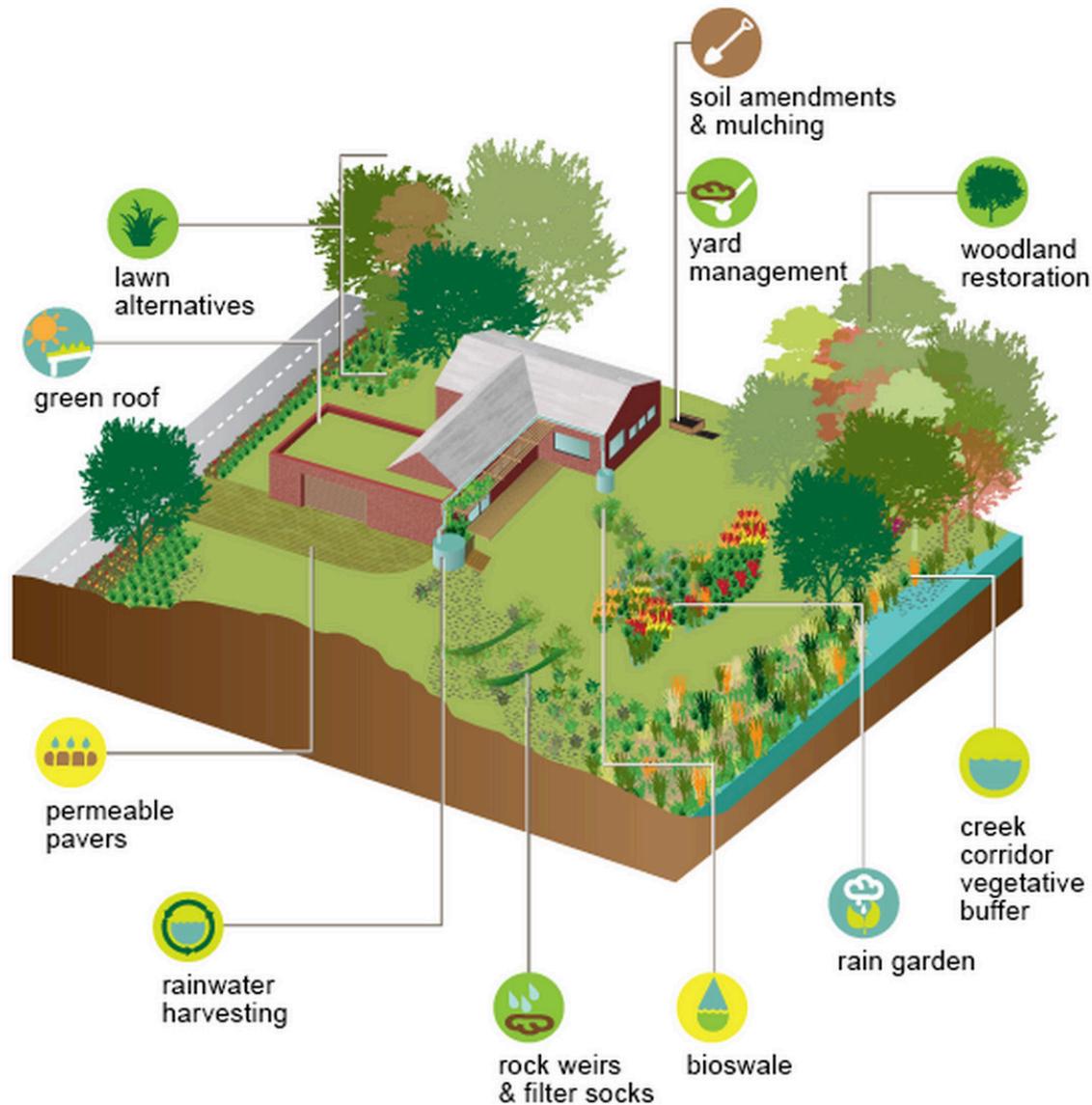
ANGLED OR 90 DEGREE PARKING RAIN GARDENS



Corner curb extensions are a practical way to implement rain gardens on side streets. These rain gardens will fit into the area set aside for parking, either parallel or angled. If the rain garden needs to increase volume, then some parking spaces may need to be reduced. Rain gardens will be planted with ornamental wetland plant species and large canopy trees to provide color and green space to the Main Street corridor.



Our plan recommends a further study, by a civil engineer, to determine the feasibility of rain gardens along Main Street.



In addition to private street bumpouts engaged as stormwater runoff reduction, there are many steps private landowners can take to mitigate stormwater issues along Main Street.

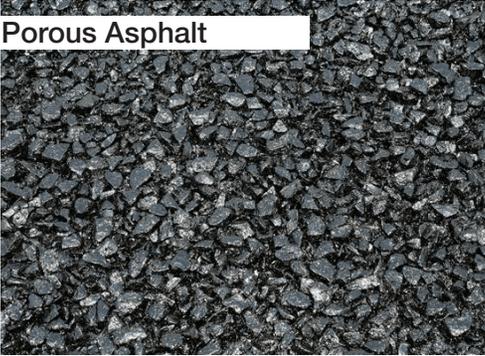
For City-Owned parcels, including public parking lots and pass-through parklets, the primary techniques that can be deployed is a reduction in impervious surface, storage, and buffers to filter and slow runoff.

On private-owned parcels, landowners may be incentivized to deploy raingardens, downspout disconnections, and lawn alternatives.

Collectively, these technologies are called Rainscaping and can be effective in both small- and large-scale implementations to make changes for the area as a whole.

Our plan recommends that City-Managed improvements deploy these Rainscaping technologies and that a grant program be considered to incentivize private landowners to do the same.

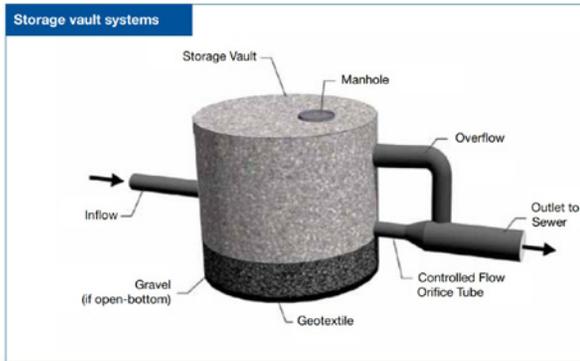
Source: Metropolitan Sewer District of St. Louis, Project Clear Rainscaping Technologies

| | Cost / Sq Ft | Aesthetic Considerations | Maintenance | PERVIOUS SURFACE REDUCTION |
|--|------------------|--|--|--|
|  <p>Pervious Concrete</p> | \$3.00 to \$4.00 | Range of colors and textures | Periodic Vacuuming Avoid Concentrated De-icers Avoid Winter Sanding | The most common pervious pavement surfaces are: <ul style="list-style-type: none"> • Pervious Concrete • Porous Asphalt • Permeable Interlocking Concrete Pavers Various considerations go into the selection of these technologies depending on design and use goals. These surfaces can also be combined with below-grade storage systems to collect and delay stormwater after a rain event. Depending on local authority and code, underdrains may be required to mitigate potential failure. All systems require some ongoing maintenance, and city snow and ice procedures should be considered, as well. |
|  <p>Porous Asphalt</p> | \$3.00 to \$4.00 | Black or Dark Grey Colors | Periodic Vacuuming Avoid Seal-Coating Avoid Winter Sanding | |
|  <p>Permeable Pavers</p> | \$3.00 to \$4.00 | Wide Range of Colors, Textures, and Patterns | Periodic Vacuuming Replace Jointing Stones as Necessary Avoid Winter Sanding | |

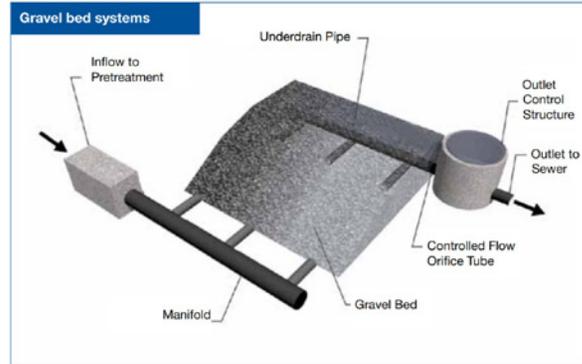
Our plan recommends porous pavements as a tool to reduce stormwater flows from private and public parcels toward Main Street.

Source: Metropolitan Sewer District of St. Louis, Project Clear Rainscaping Technologies

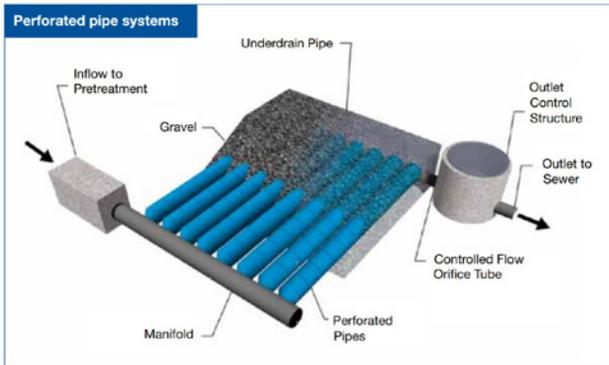
BELOW-GRADE STORAGE



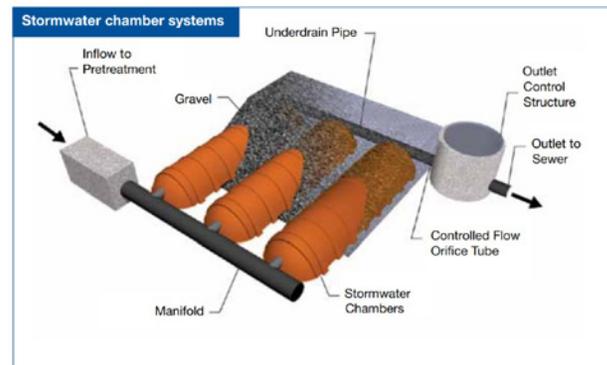
- Storage Vaults or Tanks: pre-cast structures, rings, culverts, pipes



- Gravel Beds: excavated areas filled with graded gravel, using the void space to detain water



- Perforated Pipes: Combine gravel and pipe storage to both detain and promote infiltration



- Stormwater Chambers: Structures available commercially specifically to detain and infiltrate stormwater with less skilled installation requirements

Supplemental technologies to be considered for private and public parcels include:

- Below-Grade Storage
- Downspout Disconnections
- Raingardens and Buffers
- Lawn alternatives

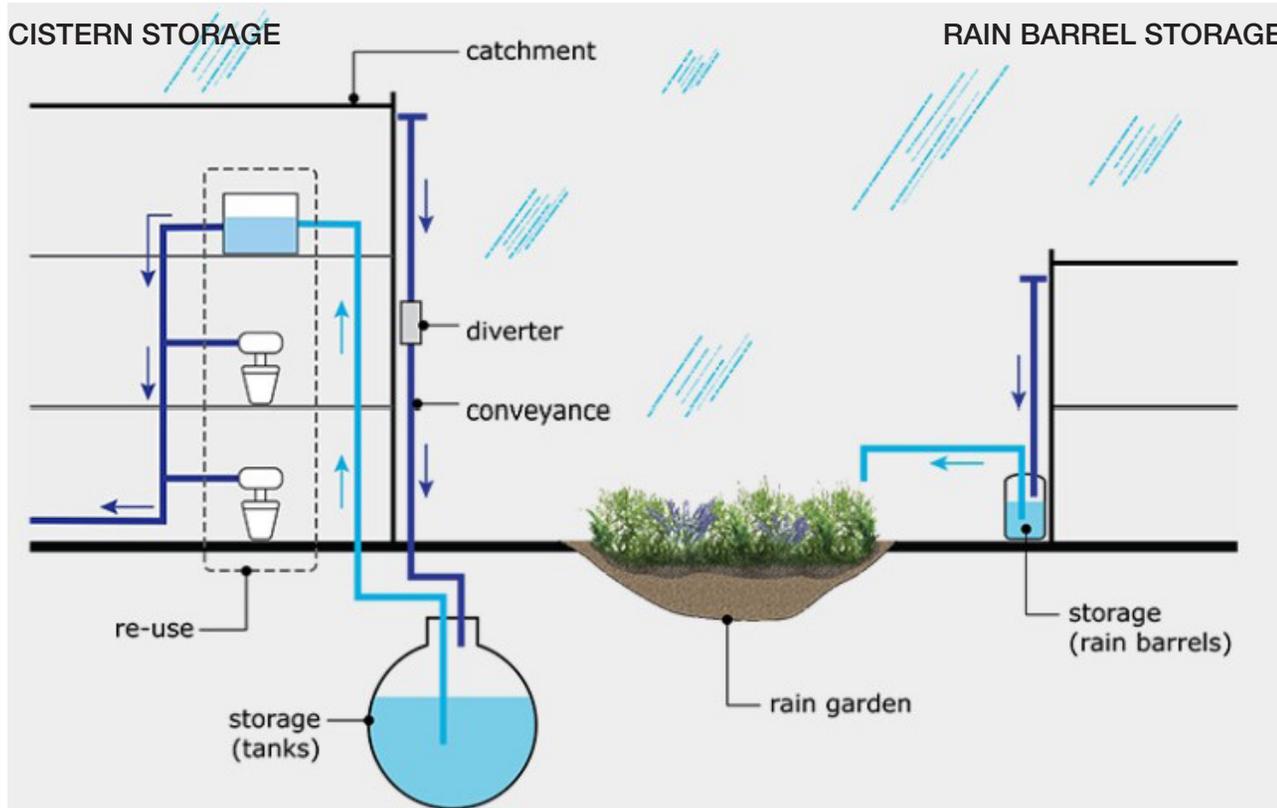
Through various additional methods, private and public parcels can have significant impact on the quality and quantity of stormwater that flows across sites following a rain event.

BELOW-GRADE STORAGE

Below-grade storage systems are often deployed in conjunction with Pervious Pavements. There are primarily four different types of systems, each with benefits or drawbacks dependent upon specific design considerations. Engineering calculations and pricing models can help inform desired system use. Below-grade storage can run between \$4.00 and \$22.00 per Cubic Foot of water stored depending on features and construction.

Where applicable, sub-surface storage systems should be considered to delay release of stormwater and reduce sheet flows.

Source: Guidelines for the Design and Construction of Stormwater Management Systems, New York City Government



Source: Next.CC

DOWNSPOUT DISCONNECTION

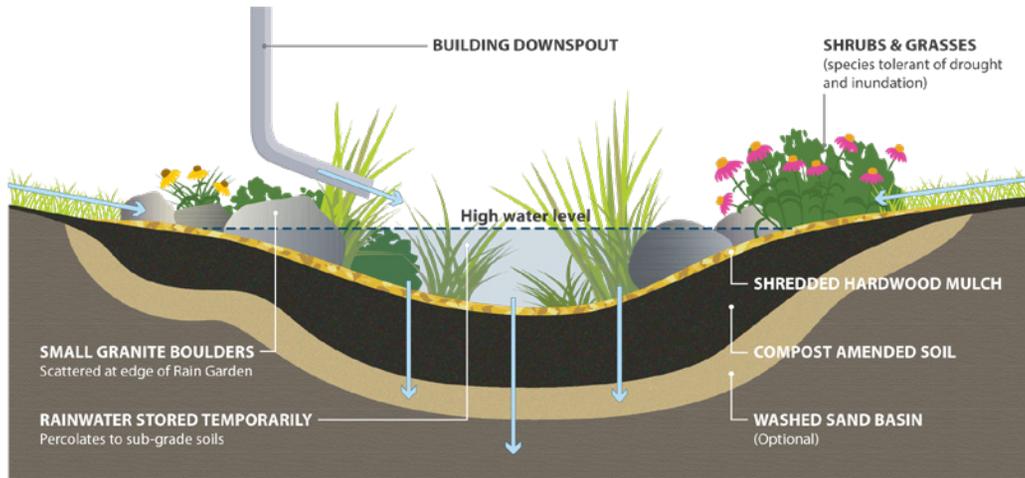
Removing roof stormwater from the sewer system and diverting it for productive use or dispersal into a rain garden is an ideal first step toward green infrastructure.

Downspouts can be disconnected from an entire structure, or a more targeted portion of a roof area. Depending on the general size of area requiring collection, Rain Barrels or the larger-sized Rainwater Cisterns may be appropriate approaches to collecting stormwater for use in landscape irrigation or other purposes.

Cisterns can be constructed of Galvanized Steel, Polyethylene, Fiberglass or other Composites, and can hold up to 5,000 gallons. For smaller-scale installations, a Rain Barrel of about 40 gallons can serve for intimate-scale storage. Disconnection and storage costs vary between \$0.60 to \$2.50 per gallon of water stored.

Downspout disconnection can lead to some small-scale changes that produce a larger impact. This would be an easy pilot project for the City or Landowners to implement initially to open the stormwater conversation.

RAIN GARDENS AND BUFFERS

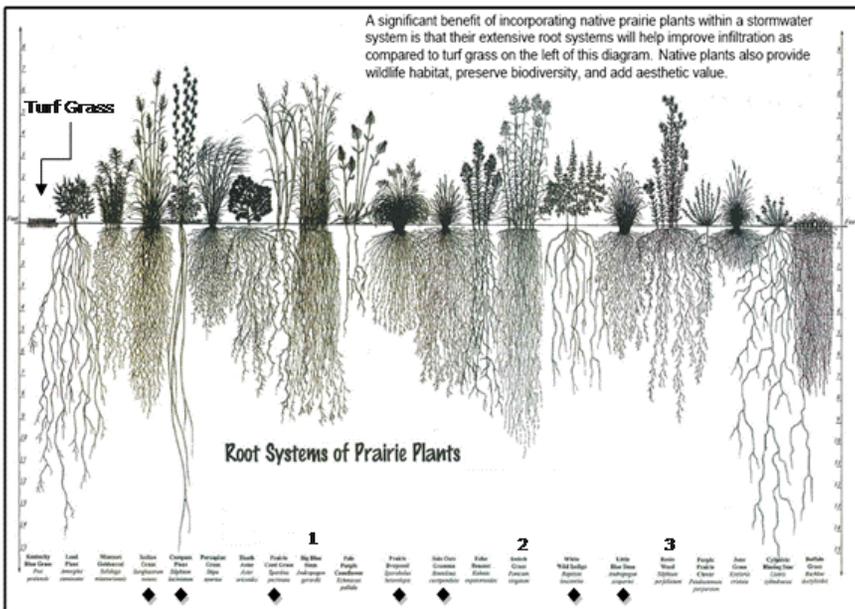


Source: Toronto and Region Conservation Authority

RAIN GARDENS AND BUFFERS

Rain gardens are areas of native shrubs, perennials, and flowers planted into a small depression designed to temporarily hold and infiltrate rain water runoff from various sources. The plants used filter any particulates from the runoff, effectively cleaning stormwater prior to reintroducing it to the groundwater stores. Rain gardens should be designed and engineered to infiltrate all water within 12-48 hours of a storm event. Small, residential rain gardens cost between \$4.00 and \$12.00 per square foot.

LAWN ALTERNATIVES



Source: LivingHabitats

LAWN ALTERNATIVES

Turf lawn has a shallow root system that is incapable of infiltrating stormwater at a rapid rate or high quantity so much of the rain water that lands on a turf lawn will run off. Alternatives to turf lawn that have robust root structures, such as trees, shrubs, perennials and groundcovers help to infiltrate and filter stormwater as it lands. Lawn alternatives cost between \$3.00 and \$10.00 per square foot.

Grading and planting schemes beyond the typical palette should be implemented as a portion of any new project the City or Private Landowners undertake. A requirement in City Ordinance of 10% of Open Area to be planted as Native Landscaping can kickstart this initiative.



INCENTIVE CASE STUDY

Following a Federal Consent Decree, the St Louis Metropolitan Sewer District (MSD) implemented an initiative to improve water quality and alleviate wastewater concerns by investing in a Rainscaping Grant program available to residents in their district boundary.

This program incentivizes landowners via a reimbursement program to implement any number of rainscaping projects. Supported with resources, education and outreach, this program has helped to make stormwater management a more approachable issue for landowners. To date, nearly 240 projects totaling over \$625,000 have been completed.

It is recommended that an incentive grant program be considered for Private Landowners that will help to make the leap to a thoughtful approach to green infrastructure. These grants could be administered by the City or Sewer Authority and program funding can be sought from universities, charities or other ecological sources.



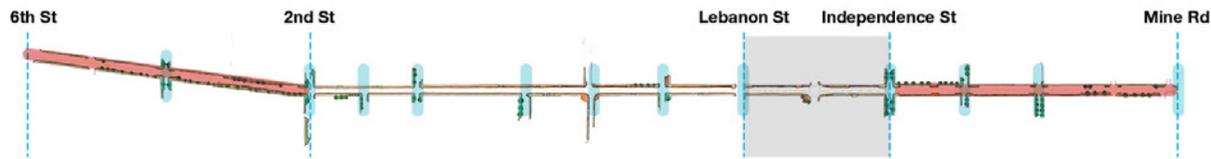
Source: Metropolitan Sewer District of St. Louis, Project Clear Small Grants Program

| Botanical Name | Missouri Botanical Garden Plant Finder | | | | | | | | | | | | MSD Landscape Guide for Stormwater BMPs | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|-------------------------------------|--|----------|-------|-------|-----|------|------|--------|-----------|---------|----------|----------|---|--------------------------|---------------|-----------------|-----------------|-----------|------------------|-------------|----------|----------|--------------------|---------|--------------|---------------|-----------------|---------------|-----------------------------|-----------|-------------------------------------|-----------------------------|---------------------------|---------------------------------|---------------------------------|----------------|--------------|----------------|---|--|--|
| | January | February | March | April | May | June | July | August | September | October | November | December | Plant Type | Sun | Water | Max Height (ft) | Max Spread (ft) | Clay Soil | Drought Tolerant | Heavy Shade | Wet Soil | Dry Soil | Shallow-Rocky Soil | Erosion | Black Walnut | Air Pollution | Rabbit Tolerant | Deer Tolerant | Pond Edge & Permanent Water | Over Sand | Lower Slopes and Biorientation Base | Upper Slopes (bioretention) | Flood Frequency Tolerance | Flood Height Tolerance (Inches) | Flood Duration Tolerance (Days) | Salt Tolerance | Agressivness | Silt Tolerance | | | |
| <i>Amsonia illustris</i> | | | | X | | | | | X | | | | Herbaceous Perennial | Full Sun to Part Shade | Medium | 3 | 1.5 | | | | | | | | | | | | Yes | Yes | Yes | | | H | 36 | 5 | | L | H | | |
| <i>Aronia melanocarpa</i> | | | | X | X | | | | X | X | | | Deciduous Shrub | Full Sun to Part Shade | Medium | 2 | 4 | | | | | | | | | | | | | | Yes | Yes | Yes | Yes | M | 12 | 2 | | L | | |
| <i>Asclepias tuberosa</i> | | | | | | X | X | | | | | | Herbaceous Perennial | Full Sun | Dry to Medium | 2.5 | 1.5 | | Yes | | | Yes | Yes | Yes | | | | Yes | | Yes | | | Yes | L | | | | V | M | | |
| <i>Baptisia sphaerocarpa</i> | | | | X | | | | | | | | | Herbaceous Perennial | Full Sun to Part Shade | Dry to Medium | 3 | 3 | | Yes | | | Yes | Yes | Yes | | | | | | | | Yes | L | | | | | | | | |
| <i>Bouteloua curtipendula</i> | | | | X | X | X | X | X | X | X | X | X | Grass | Full Sun | Dry to Medium | 2.5 | 2 | | Yes | | | Yes | Yes | Yes | Yes | Yes | Yes | Yes | | | Yes | L | | | | | | | | | |
| <i>Carex oravi</i> | | | | X | X | X | X | X | X | X | | | Sedge | Full Sun to Part Shade | Medium to Wet | 3 | 2 | | | | Yes | | | | | | | Yes | Yes | Yes | Yes | H | 24 | 3 | | L | | | | | |
| <i>Carex muskingumensis</i> | | | | X | X | X | X | X | X | X | | | Sedge | Full Sun to Part Shade | Medium to Wet | 3 | 3 | | Yes | | | | | | | | Yes | Yes | Yes | Yes | H | 24 | 3 | | M | M | | | | | |
| <i>Carex praeaculis</i> | | | | X | X | X | X | X | X | X | | | Sedge | Full Sun to Part Shade | Medium to Wet | 3 | 3 | | | | | | | | Yes | | | Yes | Yes | Yes | Yes | H | 12 | 2 | H | H | H | | | | |
| <i>Carex vulpinoidea</i> | | | | X | X | X | X | X | X | X | | | Sedge | Full Sun to Part Shade | Wet | 3 | 2 | | | | | | | | | | | Yes | Yes | Yes | Yes | H | 24 | 3 | L | L | | | | | |
| <i>Chasmanthium latifolium</i> | | | | X | X | X | X | X | X | | | | Grass | Full Sun to Part Shade | Medium to Wet | 3 | 2.5 | | | | | | | | | Yes | | | Yes | Yes | Yes | Yes | M | 12 | 1 | | H | | | | |
| <i>Conoclinium coelestinum</i> | | | | | | X | X | X | X | | | | Herbaceous Perennial | Full Sun to Part Shade | Medium | 3 | 3 | Yes | | | Yes | | | | | | | Yes | | Yes | Yes | Yes | Yes | M | 12 | 3 | M | M | H | | |
| <i>Coreopsis lanceolata</i> | | | | X | X | X | X | X | | | | | Herbaceous Perennial | Full Sun | Dry to Medium | 2 | 1.5 | | Yes | | | Yes | Yes | Yes | | | | Yes | Yes | Yes | Yes | L | | 1 | | L | | | | | |
| <i>Echinacea purpurea</i> | | | | | X | X | | | | | | | Herbaceous Perennial | Full Sun to Part Shade | Dry to Medium | 5 | 2 | Yes | Yes | | | Yes | Yes | Yes | | | | Yes | | | Yes | L | | | | | | | | | |
| <i>Heuchera parviflora</i> | | | | | X | X | | | | | | | Herbaceous Perennial | Full Sun to Part Shade | Medium | 1 | 1 | | | | | | | | | | | | | | Yes | L | | | | | | | | | |
| <i>Heuchera richardsonii</i> | | | | | X | X | | | | | | | Herbaceous Perennial | Full Sun to Part Shade | Dry to Medium | 2 | 1.5 | | Yes | | | | | | | | | | Yes | Yes | M | 12 | 1 | | L | | | | | | |
| <i>Iris cristata</i> | | | | X | X | | | | | | | | Herbaceous Perennial | Full Sun to Part Shade | Medium | 0.75 | 1 | | Yes | | | | | | | | | Yes | | Yes | L | | | | | | | | | | |
| <i>Iris fulva</i> | | | | | | X | X | | | | | | Herbaceous Perennial | Full Sun to Part Shade | Medium to Wet | 3 | 2 | Yes | | | Yes | | | | | | Yes | Yes | Yes | Yes | M | 12 | 1 | | M | | | | | | |
| <i>Iris virginica</i> | | | | | X | | | | | | | | Herbaceous Perennial | Full Sun | Medium to Wet | 3 | 3 | | | | Yes | | | | | | Yes | Yes | Yes | Yes | H | 36 | 4 | | M | M | | | | | |
| <i>Juncus effusus</i> | | | X | X | X | X | X | X | X | X | X | X | Rush | Full Sun | Wet | 4 | 4 | | | | Yes | | Yes | | Yes | | | Yes | Yes | Yes | H | 24 | 4 | L | M | M | | | | | |
| <i>Lobelia cardinalis</i> | | | | | | | X | X | X | | | | Herbaceous Perennial | Full Sun to Part Shade | Medium to Wet | 4 | 2 | | | | Yes | Yes | | | | | | Yes | Yes | Yes | H | 18 | 5 | L | M | U | | | | | |
| <i>Lobelia siphilitica</i> | | | | | | | X | X | X | | | | Herbaceous Perennial | Full Sun to Part Shade | Medium to Wet | 3 | 1.5 | | | Yes | Yes | | | | | | | Yes | Yes | Yes | H | 24 | 3 | | L | H | | | | | |
| <i>Packera obovata</i> | | | X | X | X | X | X | X | X | X | X | | Herbaceous Perennial | Full Sun to Part Shade | Medium to Wet | 1.5 | 1 | | | | | | | | | | | | | Yes | Yes | Yes | Yes | M | 12 | 2 | M | M | M | | |
| <i>Panicum virgatum</i> | X | | | X | X | X | X | X | X | X | X | X | Grass | Full Sun to Part Shade | Medium to Wet | 4 | 4 | Yes | Yes | | Yes | Yes | Yes | Yes | Yes | Yes | Yes | | | Yes | Yes | Yes | M | 12 | 2 | M | M | M | | | |
| <i>Pycnanthemum tenuifolium</i> | | | | | | X | X | X | | | | | Herbaceous Perennial | Full Sun to Part Shade | Dry to Medium | 3 | 3 | Yes | Yes | | Yes | Yes | Yes | Yes | | | | | Yes | Yes | L | | 1 | | | | | | | | |
| <i>Rudbeckia fulgida</i> | | | | | | X | X | | | | | | Herbaceous Perennial | Full Sun | Dry to Medium | 3 | 2.5 | Yes | Yes | | | | | | | | | Yes | | Yes | L | | | | | | | | | | |
| <i>Schizachyrium scoparium</i> | | | | | | X | X | X | X | X | X | | Grass | Full Sun | Dry to Medium | 4 | 2 | | Yes | | | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | L | | | | | M | L | L | | | |
| <i>Scutellaria incana</i> | | | | | | X | X | X | | | | | Herbaceous Perennial | Full Sun to Part Shade | Dry to Medium | 3 | 2 | Yes | Yes | | | Yes | | | | | | | Yes | L | | | | | | | | | | | |
| <i>Solidago flexicaulis</i> | | | | | | | X | X | | X | X | | Herbaceous Perennial | Full Sun to Part Shade | Medium | 3 | 3 | Yes | Yes | | Yes | | | | | | Yes | | Yes | Yes | L | | | | | | | | | | |
| <i>Solidago speciosa</i> | | | | | | | X | X | | X | X | | Herbaceous Perennial | Full Sun | Dry to Medium | 3 | 3 | Yes | Yes | | | | | | | | Yes | | Yes | Yes | L | | | | M | H | L | | | | |
| <i>Sociella marilandica</i> | | | | | | X | X | X | | | | | Herbaceous Perennial | Part Shade to Full Shade | Medium | 2 | 1.5 | | Yes | | | Yes | | | | | | | Yes | Yes | L | | | | | | | | | | |
| <i>Sporobolus heterolepis</i> | X | X | | | | | X | X | X | X | X | | Grass | Full Sun | Dry to Medium | 3 | 3 | | Yes | | | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | Yes | L | | | | | | | | | | |
| <i>Symphoricarpon oblongifolium</i> | | | | | | | X | X | | | | | Herbaceous Perennial | Full Sun | Dry to Medium | 2 | 2 | Yes | Yes | | | Yes | Yes | Yes | Yes | Yes | Yes | Yes | | | | | | | | | | | | | |
| <i>Zizia aurea</i> | | | | X | X | | | | | | | | Herbaceous Perennial | Full Sun to Part Shade | Medium | 3 | 2 | | | | | | | | | | | | | Yes | L | | | | | M | H | | | | |

Above: Recommended rain garden plant list

Note:

- It is recommended to include a diverse mix of plant types in your design, approximately 1 grass, sedge, or rush species for every two herbaceous perennial.
- Diverse landscape designs should consist of between 5% and 15% of the total plant count for each rain garden.
- Source(s) DTLs Landscape Architecture, Missouri Botanical Garden, and “The Landscape Guide for Stormwater Best Management Practice Design” May 2012.



MAIN STREET: UNDERSTORY TREES BELOW 4" CALIPER AT MATURITY

SIDE STREETS: NO SIZE RESTRICTIONS



Main Street is operated by IDOT. IDOT will only allow street tree species to be planted that will grow no larger than four (4) inches in trunk diameter at maturity. This includes species such as redbuds, dogwood, and serviceberry. Unfortunately, this results in lower and smaller tree canopy which in turn provides little pedestrian shade and often conflicts with large vehicles such as delivery trucks and emergency vehicles.

As a result, our recommendation is to only plant small street trees on the eastern and western segments of Main Street which are proposed to have some additional room for a tree lawn. This planting scheme will create a welcoming green corridor to bookend the historic commercial Main Street.

Some residents may be interested in planting larger canopy trees in their yards or other frontages. This is an opportunity for a public/private partnership between those property owners and the City of Mascoutah which would benefit both parties.

The tree canopy should be expanded wherever possible throughout downtown Mascoutah to the greatest extent feasible. Trees have enormous benefits, as outlined on the following page, but for a cost of approximately \$250-600 each, they return over \$90,000 of direct benefits over the lifetime of the tree.

Source: Urban Street Trees: 22 Benefits. Dan Burden, 2006

22 BENEFITS OF URBAN STREET TREES

- 1. **Reduce urban traffic** of up to 15 mph.
- 2. **Create safer walking environments** by forming and framing distinct edges to sidewalks.
- 3. **Trees call for placemaking planting strips and medians** which further separate motorists from one another, pedestrians, buildings and other urban fabric. Medians reduce crashes by 50% or more.
- 4. **Increased Security.** Trees create more pleasant walking environments bringing more ownership and eyes on the street.
- 5. **Improved businesses.** 12% higher income streams on treescaped streets.
- 6. **Less drainage infrastructure.** Trees absorb the first 30% of most precipitation through their leaf system and up to another 30% through their roots.
- 7. **Rain, sun, heat and skin protection.** Temperature differentials of 5-15 degrees under canopy shade.
- 8. **Reduced harm from tailpipe emissions.**
- 9. **Gas transformation efficiency.** Trees in street proximity absorb 9 times more pollutants than more distant trees.
- 10. **Lower urban air temperatures.** Reduce energy bills for a household from 15-35%
- 11. **Lower Ozone**
- 12. **Convert streets, parking and walls into more aesthetically-pleasing environments.**
- 13. **Soften and screen necessary street features** such as utility poles.
- 14. **Reduced blood pressure, improved overall emotional and psychological health.**
- 15. **Time in travel perception** a treeless environment trip is perceived to be longer than one that is treed.
- 16. **Reduced Road Rage**
- 17. **Improved operations potential.** Trees are an excellent backdrop to help regulatory signage and signals to stand out.
- 18. **Added value to adjacent homes, businesses and tax base** by an increase of \$15-\$25,000.
- 19. **Tree lawns are useful space** for splash zones, storage of snow, driveway elevation transition and more.
- 20. **Filtering and screening agent** for typical urban visual pollution.
- 21. **Longer pavement life.** 40-60% increase in longevity of asphalt.

Source: Urban Street Trees: 22 Benefits. Dan Burden, 2006

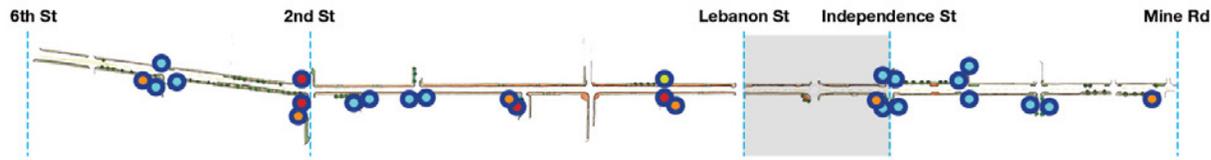
| Missouri Botanical Garden Plant Finder | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|---------------------|---------|----------|-------|-------|-----|------|------|--------|-----------|---------|----------|----------|------------------------|---------------|-----------------|-----------------|----------------|----------------|-----------|------------------|-------------|----------|----------|--------------------|---------|--------------|---------------|-----------------|---------------|
| Botanical Name | Cultivar | January | February | March | April | May | June | July | August | September | October | November | December | Sun | Water | Max Height (ft) | Max Spread (ft) | MO Native | Plant of Merit | Clay Soil | Drought Tolerant | Heavy Shade | Wet Soil | Dry Soil | Shallow-Rocky Soil | Erosion | Black Walnut | Air Pollution | Rabbit Tolerant | Deer Tolerant |
| <i>Amelanchier arborea</i> | | | | X | | | X | | | X | X | | | Full Sun to Part Shade | Medium | 25 | 25 | Yes | Yes | Yes | | | | | | | | | | Yes |
| <i>Amelanchier x grandiflora</i> | 'Autumn Brilliance' | | | | X | | X | | | X | X | | | Full Sun to Part Shade | Medium | 25 | 25 | Yes - Cultivar | Yes | | | | | | | | | | | |
| <i>Cercis canadensis</i> | 'Appalachian Red' | | | X | X | | | | | | | | | Full Sun to Part Shade | Medium | 25 | 25 | Yes - Cultivar | | Yes | | | | | | | Yes | | | |
| <i>Cercis canadensis</i> | 'Forest Pansy' | | | X | X | | | | | | | | | Full Sun to Part Shade | Medium | 30 | 35 | Yes - Cultivar | | Yes | | | | | | | Yes | | | |
| <i>Cornus florida</i> | | | | X | X | | | | | | | | | Full Sun to Part Shade | Medium | 30 | 30 | Yes | | Yes | | | | | | | Yes | | | |
| <i>Crataegus viridis</i> | 'Winter King' | | | | | | X | X | | | | X | X | Full Sun | Dry to Medium | 35 | 35 | Yes - Cultivar | | Yes | Yes | | | | | | | | | |
| <i>Ostrya virginiana</i> | | | | | | | | | | | X | X | | Full Sun to Part Shade | Medium | 40 | 30 | Yes | | | Yes | Yes | | | | | | | | Yes |
| <i>Syringa reticulata</i> subsp. <i>pekinensis</i> | | | | | | X | X | | | | | | | Full Sun | Medium | 20 | 15 | | | Yes | Yes | | | | | | | | | |

Above: Recommended understory street trees for Main Street

| Missouri Botanical Garden Plant Finder | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|---------|----------|-------|-------|-----|------|------|--------|-----------|---------|----------|----------|------------------------|---------------|-----------------|-----------------|----------------|-----------|------------------|-------------|----------|----------|--------------------|---------|--------------|---------------|-----------------|---------------|-----|--|
| Botanical Name | January | February | March | April | May | June | July | August | September | October | November | December | Sun | Water | Max Height (ft) | Max Spread (ft) | Plant of Merit | Clay Soil | Drought Tolerant | Heavy Shade | Wet Soil | Dry Soil | Shallow-Rocky Soil | Erosion | Black Walnut | Air Pollution | Rabbit Tolerant | Deer Tolerant | | |
| <i>Betula nigra</i> | | | | | | | | | X | X | | | Full Sun to Part Shade | Medium to Wet | 70 | 60 | | Yes | Yes | | Yes | | | | | | Yes | | Yes | |
| <i>Nyssa sylvatica</i> | | | | | | | | | X | X | X | | Full Sun to Part Shade | Medium to Wet | 50 | 30 | Yes | Yes | | | Yes | | | | | | | | | |
| <i>Quercus alba</i> | | | | | | | | | | | X | X | Full Sun | Dry to Medium | 80 | 80 | | Yes | Yes | | | Yes | Yes | | Yes | | | | | |
| <i>Quercus bicolor</i> | | | | | | | | | | | X | X | Full Sun | Medium to Wet | 60 | 60 | | | | | Yes | | | | | | | | | |
| <i>Quercus macrocarpa</i> | | | | | | | | | | | X | X | Full Sun | Dry to Medium | 80 | 80 | | Yes | Yes | | | Yes | | | | | | | | |
| <i>Quercus muehlenbergii</i> | | | | | | | | | | | X | X | Full Sun | Dry to Medium | 60 | 70 | | | Yes | | | | | | | | | | | |
| <i>Quercus phellos</i> | | | | | | | | | | | X | X | Full Sun | Medium to Wet | 75 | 50 | | Yes | | | Yes | | | | | Yes | | | | |
| <i>Quercus shumardii</i> | | | | | | | | | | | X | X | Full Sun | Dry to Medium | 60 | 40 | | | Yes | | | Yes | | | | Yes | | | | |
| <i>Taxodium distichum</i> var. <i>distichum</i> | | | | | | | | X | X | X | X | | Full Sun | Medium to Wet | 70 | 45 | | Yes | | | Yes | | | | | Yes | | | Yes | |
| <i>Tilia americana</i> | | | X | X | | | | | | | | | Full Sun to Part Shade | Medium | 80 | 50 | | Yes | Yes | | | | | | | | | | | |

Above: Recommended large canopy trees for side streets and rain gardens.

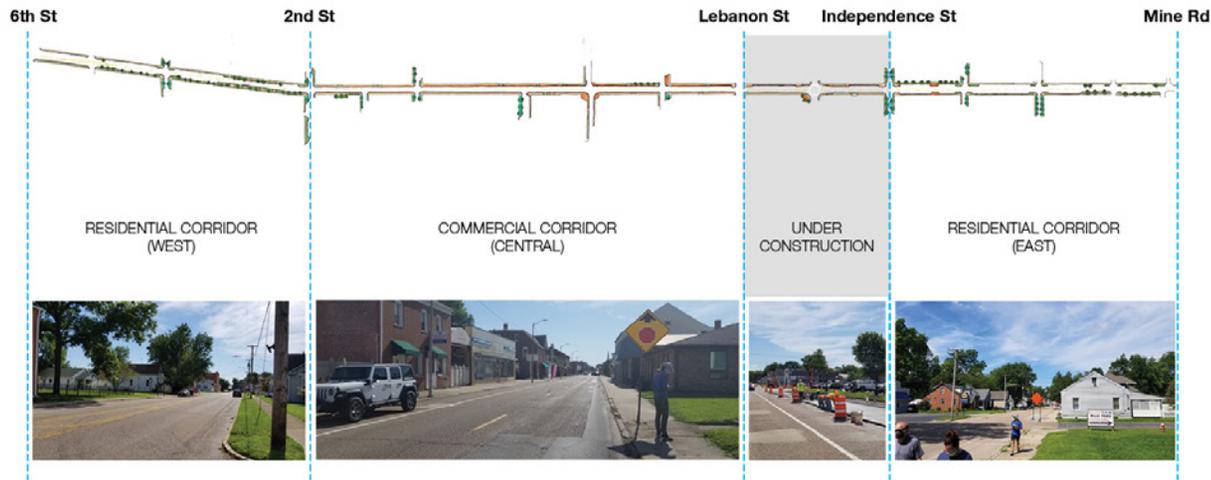
Source(s) DTLs Landscape Architecture, Missouri Botanical Garden, and "The Landscape Guide for Stormwater Best Management Practice Design" May 2012.



The proposed stormwater curb extensions along Main Street are intended to be multi-functional. These public spaces can also serve as an amenity for pedestrians along Main Street. Each stormwater curb extension is a public garden, and the ideal place to rest along Main Street. Seating can easily be implemented alongside these curb extensions, or in the larger basins the seating may be included as a parklet.

Our previous community poll expressed favor for all four corners of every intersection to have curb extensions. Where stormwater improvements aren't needed, simple plantings or pavement can fill these spaces. Pavement may be ideal next to restaurants or other businesses that would benefit from the additional seating or storage area on side streets.

Finally, these spaces can house some of the art or sculpture already located on the corridor, which might help draw visitors to walk Main Street's new art walk or aid in wayfinding.



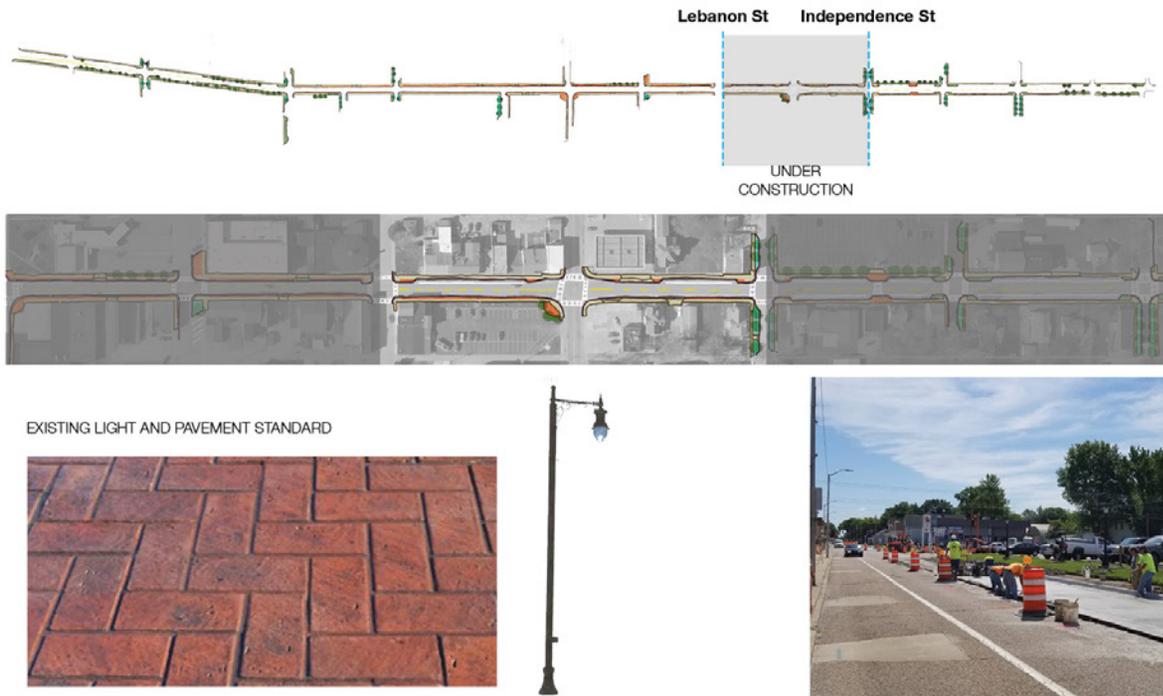
The State of Illinois Department of Transportation (IDOT) owns and operates Main Street. IDOT is planning to complete road resurfacing improvements on Main Street sometime between 2022 and 2026. They have not yet begun their early planning efforts, but will do so soon. Our Great Streets project is perfectly timed to provide initial community feedback to IDOT to assist them in understanding how changes to Main Street can be directed to make a positive impact. The Great Streets team has collaborated with IDOT to understand what safety regulations and design requirements will be applied to Main Street so that a reasonable community plan is put forth for review.

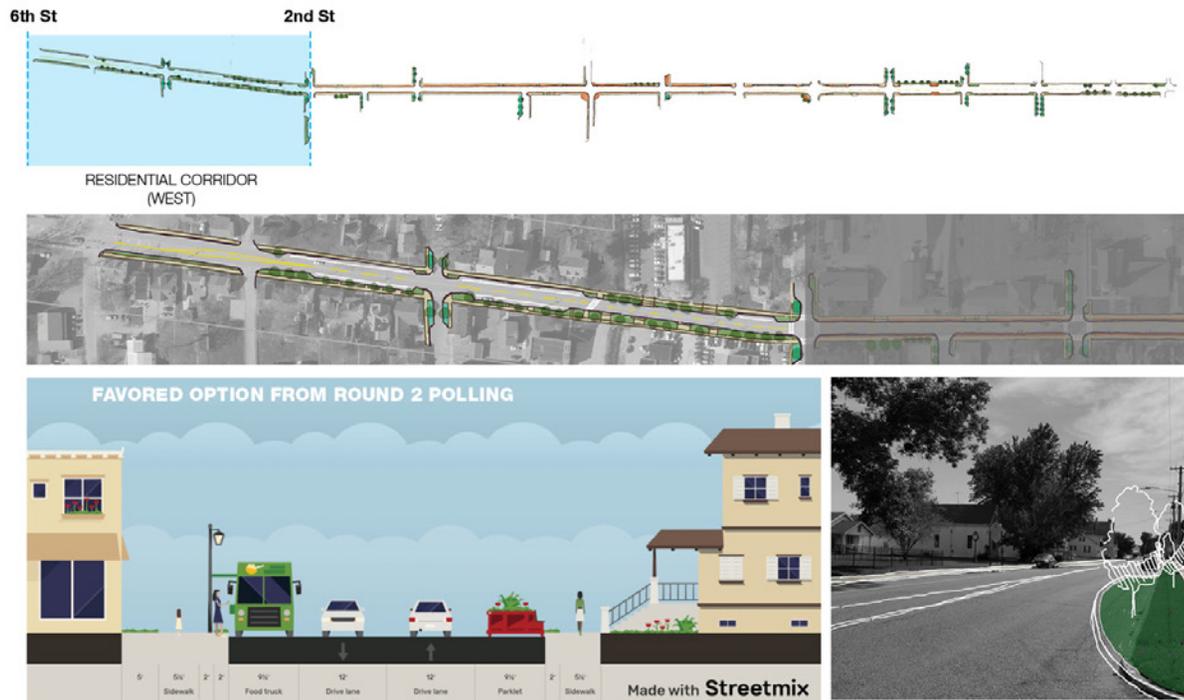
The following pages include a detailed look at our concepts for Main Street in four character segments; West, Central, under construction, and East and shown in the graphic to the left.

Under Construction

One small, two block section of Main Street is currently under construction between Lebanon and Independence. This segment sets a precedent for future work on the corridor. The improvements here include new red brick stamped concrete and decorative lights. As Main Street is rebuilt to the east and west, these design elements should be carried through to create unity.

As you will see in the graphic to the left, the red brick pattern is suggested for the traditional commercial section of Main Street between 2nd and Independence whereas the lighting can be implemented for the full length of the corridor between 6th Street and Mine Road.



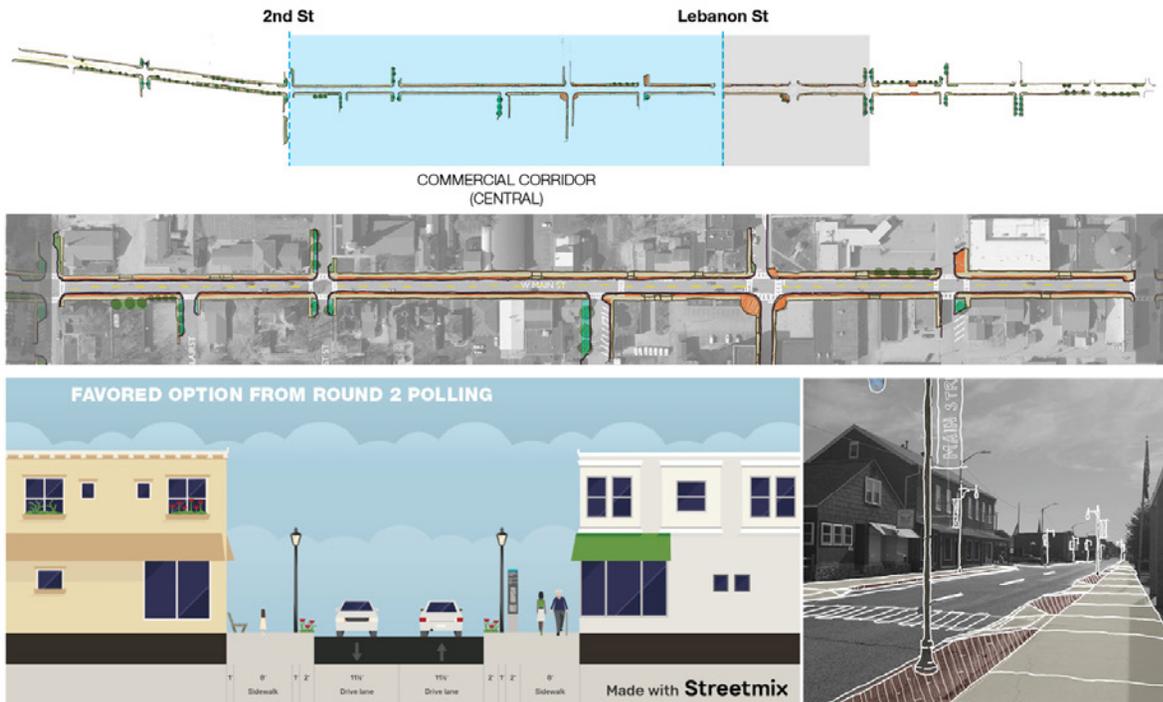


West Segment

The western segment of Main Street begins at 6th and ends at 2nd street. The primary adjacent land use is residential, with buildings set back from the street. During our last presentation, we asked participants to vote for their favorite cross-section, and as shown, 55% selected the option with spaces for parklets and food trucks next to the driving lanes. We have since verified with IDOT that these improvements will not be allowed, but spatially they are the same as allowing the existing parallel parking to remain.

The primary change is to extend curbs to capture unused space along Main Street for tree lawns. The additional space between sidewalk and street is enough to plant small understory street trees, such as redbuds, dogwood, or serviceberry without the negative impacts to vehicles or pedestrians caused by the lower canopy height. The benefit is that the new tree lawns will add a green welcome to downtown Main Street while preserving the existing parallel parking spaces throughout this segment.





Central Segment

The central segment of Main Street begins at 2nd Street and ends at Lebanon. The primary adjacent land use is commercial, with traditional store frontage at the back of the sidewalk. During our last presentation, we asked participants to vote for their favorite cross-section. The majority favored moving the curb to increase sidewalk width evenly on both sides as shown in the cross section to the left. As a result, the pedestrian space on each side grows from 8 to 12 feet, with a wide sidewalk and room for lighting, signage, planters, and benches. Twelve feet is also enough for restaurants to add outdoor café dining while still having room for pedestrians to walk freely. The cost of this increase is the removal of on-street parallel parking, which is currently located on one side of the street. Red brick accent pavement can be added to this section to extend what is currently being constructed east of Lebanon.

As mentioned previously, IDOT will only allow small understory trees along Main Street. The overall narrow width of Main Street in this section does not favor small trees as the canopy will conflict with pedestrians and vehicles.

The loss of parallel parking spaces can be accommodated in reorganized parking behind Main Street, between Church and State as you will see further in this presentation

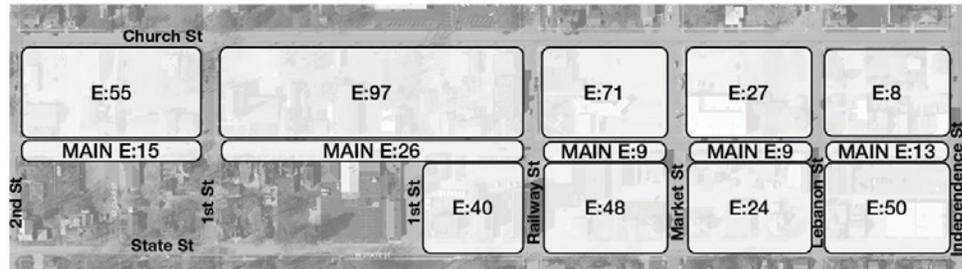


Central Segment Parking

Main Street between 2nd and Independence is proposed to be free of on-street parallel parking. The blocks behind Main Street already include large areas for parking, but are inefficiently laid out. Reorganization of these parking lots into public parking spaces easily accommodates the losses from the new Main Street streetscape, while boosting the overall number of parking spaces in the district by over 60 spaces.

The success of this scheme will be dependent upon a partnership between the city and private property owners to develop improved lots which have adequate wayfinding for pedestrians and vehicles, as well as improved mid-block connections to Main Street.

EXISTING PARKING ALONG MAIN AND BETWEEN CHURCH AND STATE STREETS



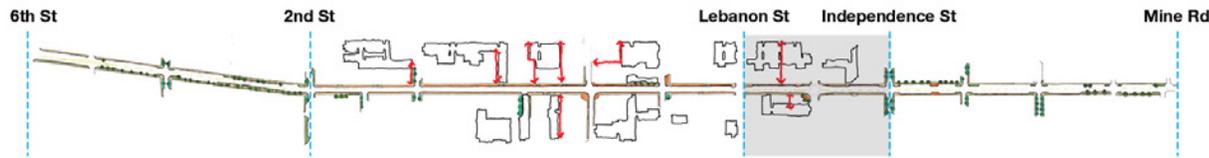
TOTAL
492

PROPOSED PARKING ON MAIN BETWEEN CHURCH AND STATE STREETS



TOTAL
557

See Urban Design appendix document
for more information



Central Segment Pass-Through

The reorganized parking behind Main Street will accommodate the loss of on-street parallel parking, but can only be successful if pedestrians are comfortable accessing Main Street. Many opportunities exist, as shown in red arrows in the plan above, for pedestrian pass through. Pedestrian comfort can be achieved in these spaces with adequate signage and safe lighting levels.

Larger spaces are opportunities for temporary pocket parks, which can be open spaces for informal use, or programmed for adjacent buildings to use for seating, dining, or outdoor shopping.

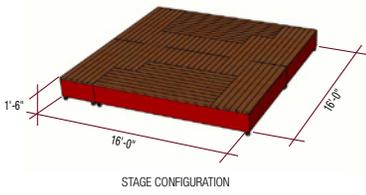
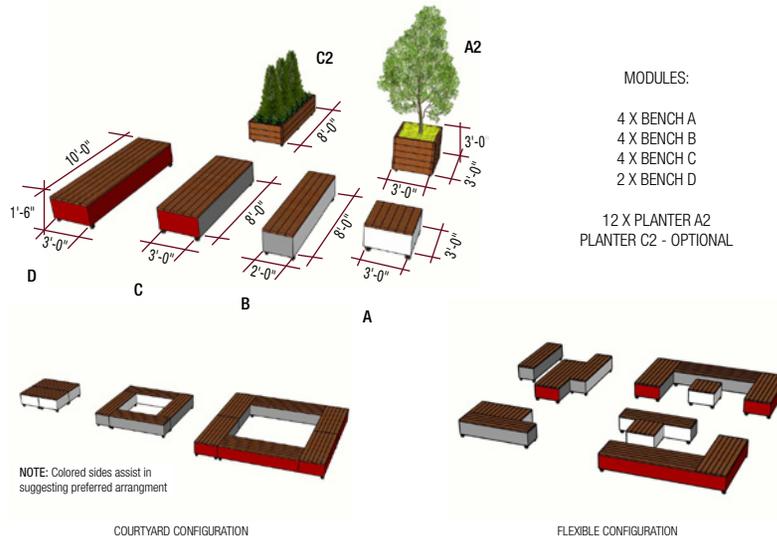
SMALL EXISTING PASS-THROUGH ALONG MAIN STREET



LARGER PEDESTRIAN SPACES CAN BECOME POCKET PARKS



See Urban Design appendix document for more information, including a detailed plan of proposed pedestrian connections



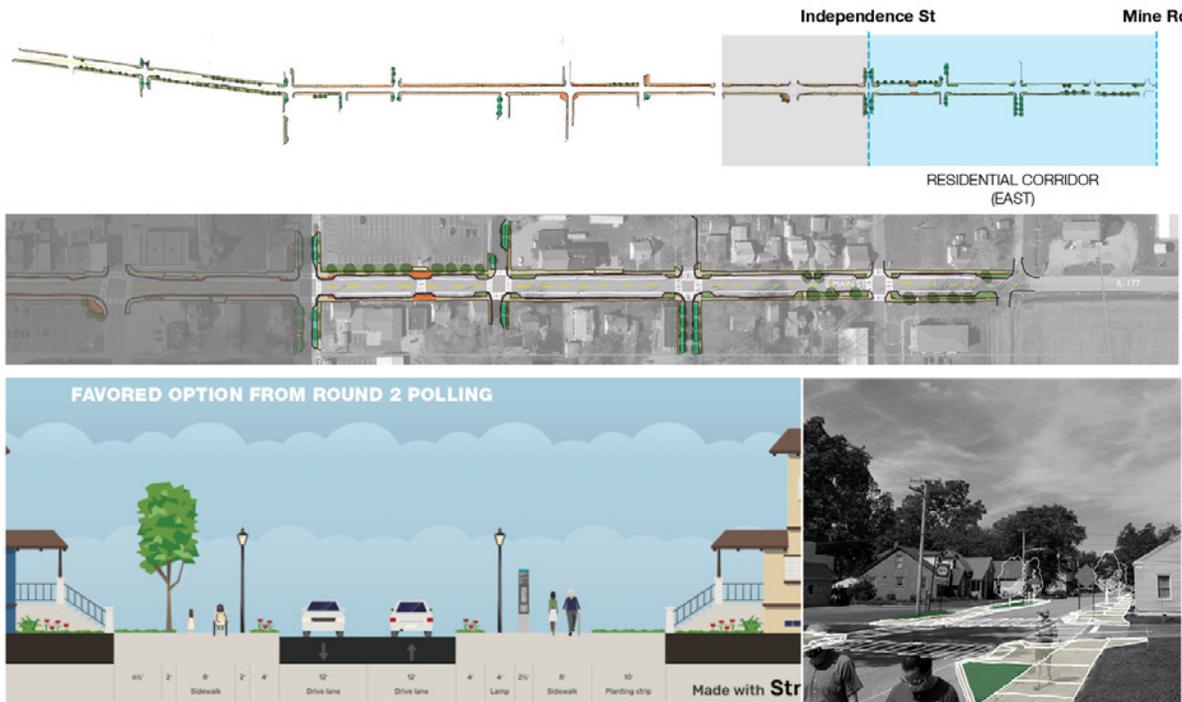
Central Segment Pass-through

As mentioned on the previous page, larger spaces are opportunities for temporary pocket parks. The diagrams to the left are examples of relatively easy to construct, modular, and temporary seating elements.

Constructed of wood on rolling casters, the separate pieces can be rearranged for a variety of user groups, individuals, or collected into a stage.

The key design consideration is a consistency in height and width. A coordinated color palette may assist in suggesting arrangements by painting sides while brightening the space.





East Segment

The East segment of Main Street begins at Independence Street and ends at Mine Road. The primary adjacent land use is residential, with buildings set back from the street. During our last presentation, we asked participants to vote for their favorite cross-section, and as shown, 70% favored fully expanded sidewalks with complete removal of parallel parking.

The proposed design is intended to match the western segment, to create a bookend arrival sequence for Main Street in both directions. Some parallel parking remains, but street intersections are supported with curb extensions and street trees. The additional space between sidewalk and street is enough to plant small understory street trees, such as redbuds, dogwood, or serviceberry (see plant list page 27) without the negative impacts to vehicles or pedestrians caused by the lower canopy height. The benefit is that the new tree lawns will add a green welcome to downtown main street while preserving the existing parallel parking spaces throughout this segment.





Benches perpendicular to the sidewalk



Backless benches along curb



Bike racks along curb



Planters and seasonal color



Variety of planter sizes with same material and finish

Additional Recommended Streetscape Elements

A streetscape is more than decorative placement, lighting, and street trees. It needs to include places to rest, park a bike, and add seasonal interest.

As Main Street develops further, a consistency in site furnishing type, color, and placement will be key to unifying Main Street as a cohesive space. These furnishings include:

- **Benches:** Consider backless benches for placement parallel along the curb, or otherwise with backs against buildings or perpendicular to the sidewalk. Backless benches along the curb will be a benefit during festivals and parades.
- **Trash receptacles:** A minimum of one trash receptacle per block, typically at the corner or where people gather most.
- **Bike racks:** Place near stores, popular restaurants or parks. Can be located along the curb if room allows.
- **Planters:** An effective way to add seasonal color and foliage along the corridor. Consider a variety of sizes and shapes, but keep the material and finish consistent. Large planters may accommodate small trees.

Planters can also be positioned together to create a safe space for pedestrians to gather and act as traffic calming.

IMPLEMENTATION AND PARTNERSHIPS

Timeline/Phasing

The proposed Main Street improvements are on a somewhat fixed schedule, with IDOT planning to fund improvements between 2022 and 2026. IDOT will have its own procedure for the design, engineering and engagement for Main Street prior to the start of construction. There is little that the City of Mascoutah can do to impact those series of events, but this Great Streets project is a good first step to begin the process and be forthcoming with the City's preferences.

The stormwater volume reduction improvements proposed on the side-streets along Main Street is a project that will need to begin very quickly in order to be successful. This is because IDOT will want to know exactly what the City is planning in order to coordinate. If the stormwater improvements remain "future projects" through construction of the Main Street project, they are unlikely to ever happen due to the cost of future IDOT coordination, demolition and replacement of new improvements on Main Street.

Construction of the stormwater improvements does not have to happen in conjunction with Main Street, but the design of the improvements needs to be finalized, or close to final.

Costs & Potential Funding Sources

The City's costs associated with Main Street improvement are likely to be related to aesthetic improvements only, as the street reconstruction will be funded by IDOT. This arrangement will be similar to the section of Main Street currently under construction between Lebanon and Independence Streets, with the city covering the cost for decorative lighting and pavement.

- Lighting: Approximately 1 light per 70 feet between 6th Street and Mine Road, or 76 total. Each light can be around \$6,000 each, or \$456,000 total.
- Decorative Pavement: Approximately 20,000 square feet between 2nd Street and Lebanon at \$25 a square foot, or \$500,000
- Trash Receptacles: 10 at \$500 each, or \$5,000
- Trees: Assuming two trees per light at \$300 each, or \$45,600
- Contingency, mobilization, and design fees of 27%, or \$271,782
- Total Streetscape Budget: \$1.3 Million

Curbside rain gardens are typically around \$30 a square foot. However this number is difficult to budget for accurately due to infrastructure unknowns and will be easy to refine following the initial study by a civil engineer.

This plan proposes a total of 12,000 square feet of rain gardens, which will cost approximately \$360,000.

IMPLEMENTATION AND PARTNERSHIPS

Action Steps

Immediately engage a civil engineer to study stormwater capacity on Main Street and all side streets downtown. This process will yield preliminary recommendations for stormwater improvements, their size, function and connection points to the existing system. A detailed cost estimate for the proposed improvements as a final deliverable will allow the City to decide if the benefits are worth the costs. Below is an outline of recommended project scope items for the civil engineer to complete:

- Background data collection, review & analysis
- Drainage area analysis
- Stormwater system capacity analysis
- Existing system recommendations
- Concept level (30%) design drawings for recommended green infrastructure
- Public engagement.
- Revised concept drawings.
- Conceptual cost estimate.

Total costs for engineering study and preliminary plans will likely range somewhere between \$100,000 to \$200,000 and approximately 6-12 months to complete.

The city will also need to participate in the IDOT Design and Engineering process for Main Street. Be ready to convey the public's preferences for parking, decorative pavement, and lights.

Responsibilities

It will be the City of Mascoutah's responsibility to stay engaged and informed with the community to test and confirm preferences for the design of Main Street. For decisions such as the removal of parking, decorative pavement, and decorative lighting, it will be the responsibility of City staff to communicate with the community and IDOT.

It is the City of Mascoutah's responsibility to be prepared with a plan for stormwater improvements in conjunction with the IDOT design process for Main Street. The first step in this process will be to hire a civil engineer to investigate stormwater capacity, soils, and make recommendations for locations and sizes of stormwater improvements. These findings will be instrumental in the development of a comprehensive design for Main Street.

Partners

Saint Clair County and the Heartlands Conservancy are potential partners for assistance in developing green infrastructure in downtown Mascoutah.

Long term, a business district or CID will be the ideal manager of the downtown streetscape.

Supporting Documents

See Urban Design appendix for content related to costs and implementation of the parking areas, pedestrian paths, and history walk.

See Transportation appendix for content related to costs and implementation of wayfinding, signage, greenway paths and trails.

Mascoutah Great Streets

Transportation White Paper

November 6, 2020

APPENDIX C

ABOUT

CBB served as the transportation planner on the Mascoutah Great Streets Project. The project focused on Route 177/Main Street from 6th Street to Mine Road in a city-wide and regional context. Route 177/Main Street is a state route which runs through the heart of downtown Mascoutah, see **Figure 1**. Fortunately, IDOT has scoped a resurfacing project for Route 177/Main Street from County Road to Mine Road, scheduled for a letting date of June 2022, allowing for the prime opportunity to implement plan recommendations for the corridor in coordination with this project.

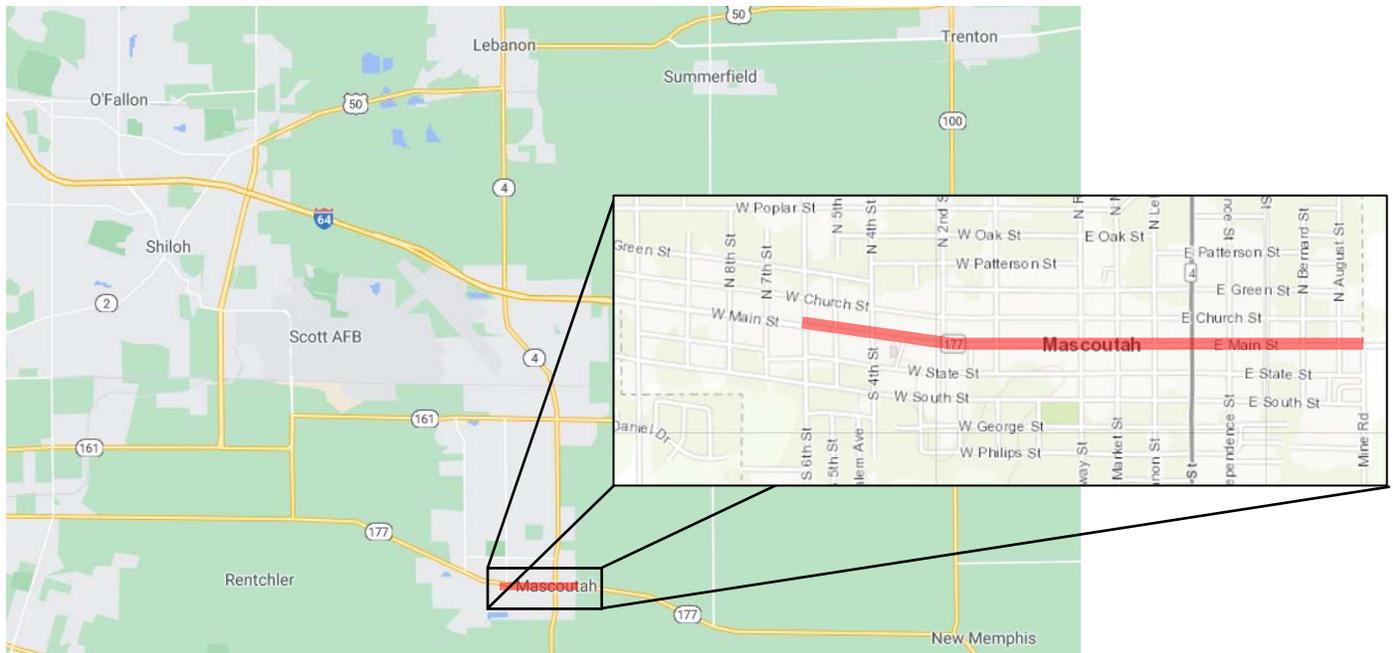


Figure 1 Mascoutah Great Streets Project Location

The project included an existing conditions analysis, engagement process to understand the community's needs and goals, and plan development. We found that the community's transportation improvement needs are largely pedestrian and bike focused. The existing grid of roadways through town provides a healthy vehicular network, however the community lacks an equally connected pedestrian and bike system. The existing Big Ditch trail on the north side of town has cultivated an avid bike community in town and has increased the demand for more bike facilities. We also found a resounding sentiment to make the Route 177/Main Street corridor more pedestrian oriented, rather than vehicular oriented. Lastly, we found there is a desire to attract more visitors to the City of Mascoutah. To achieve these goals, our Great Streets plan has recommended new and improved regional and city-wide bike and pedestrian facilities, an improved pedestrian experience through the Route 177/Main Street corridor, and a comprehensive wayfinding signage plan. At the end of this document, we discuss implementation and next steps to assist the City of Mascoutah in making this plan a reality.

EXISTING CONDITIONS

The existing conditions in the project study area are analyzed to understand the needs and opportunities of the community. Information was gathered from field observations as well as multiple sources including IDOT and City officials, and our findings were verified by community feedback throughout the engagement process. The existing conditions analysis considers the roadway infrastructure, route jurisdiction, traffic volumes, crash data, the existing parking supply, and the bike and pedestrian facilities.

Roadway Infrastructure

The existing conditions analysis begins first with assessing the roadway infrastructure. In this section, we will analyze the typical section, functional classification, and existing pedestrian facilities. While the main focus of the Great Streets project is Route 177/Main Street from 6th Street to Mine Road, Church Street is also analyzed as it is an important corridor in creating a holistic network for vehicles, pedestrians, and bicyclists in downtown Mascoutah.

Route 177/Main Street

The typical section on Route 177/Main Street consists of one-lane in each direction, sidewalks of varying widths on both sides, and on-street parking. The roadway is classified as a minor arterial roadway. The existing pedestrian facilities on Route 177/Main Street are in disrepair and not ADA compliant. These items are further discussed below.

Typical Section

The typical section refers to the amount of space that is characterized to various modes of travel within the public right-of-way. Route 177/Main Street is a two-lane roadway with varying shoulder widths and on-street parking. Route 177/Main Street is broken into three segments:

1. West Segment – 6th Street to 2nd Street
2. Central Segment – 2nd Street to Route 4/Jefferson Street
3. East Segment – Route 4/Jefferson Street to Mine Road

See **Figure 2**.

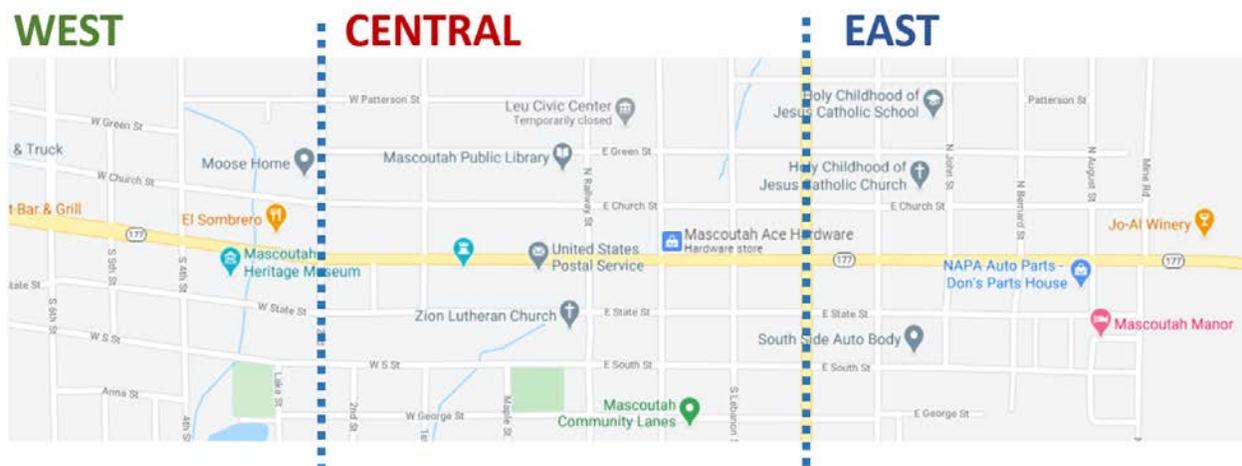


Figure 2 Route 177/Main Street Corridor Segments

In the west segment, the typical section on Route 177/Main Street consists of one 12-ft driving lane in each direction, 9 ½ -ft parking lane on each side, 5 ½ -ft sidewalks on each side, and a 2-ft to 4-ft buffer between the edge of sidewalk and edge of curb on each side. See **Figure 3**.



Figure 3 Route 177/Main Street West Segment Typical Section

In the central segment, the typical section consists of one 11 ½ -ft driving lane in each direction, one 8-ft parking lane on the north side, and an 8-ft sidewalk on both sides of the roadway. There is minimal to no buffer between the sidewalk and the edge of curb in this portion of the corridor. See **Figure 4**.



Figure 4 Route 177/Main Street Central Segment Typical Section

In the east segment, the typical section consists of one 12-ft driving lane in each direction, an 8 1/2 -ft to 9-ft parking lane on each side, two 5 1/2 -ft sidewalks, 0 – 3-ft buffer on the south side and no buffer on the north side between the sidewalk and edge of curb. See **Figure 5**.



Figure 5 Route 177/Main Street East Segment Typical Section

It is important to note the section of Route 177/Main Street between Lebanon Street and Independence Streets was recently reconstructed. This project reconstructed the pavement on Route 177/Main Street, updated the sidewalks and curb ramps, and added bump-outs and decorative elements. Decorative lighting was added, and some brick-stamped concrete was used for bump-outs and between the sidewalk and edge of curb where on-street parking was not available. See **Image 1** and **Image 2**. Image 1 Brick-Stamped Concrete



Image 1 Brick-Stamped Concrete



Image 2 Newly Constructed Bump-Out

Traffic Control

Along Route 177/Main Street between 6th Street and Mine Road, all intersections are side-street stop controlled except for three. The intersection at 6th Street is signalized and the intersection at Route 4/Jefferson Street and at Railway Street are all-way stop controlled. See **Figure 6**.



Figure 6 Traffic Control on Route 177/Main Street

Functional Classification & Speed Limit

When evaluating roadway operations, it is important to consider how the facility works (or is intended to work) within the surrounding street network. The “hierarchy” of roadways and their usage is described by their “functional classification”.

The purpose of roadway functional classification is to formally describe how travel is channeled through our roadway network and to determine project eligibility for federal funds. Roadways are classified according to their urban or rural setting and the type of service they provide based on considerations such as: connectivity, mobility, accessibility, vehicle miles traveled, average annual daily traffic, and abutting land use.

For nomenclature purposes, those roadways that provide a high level of vehicular mobility are called “arterials”; those that provide a high level of accessibility are called “locals”; and those that provide a more balanced blend of mobility and access are called “collectors”.

Context sensitivity and livability form the environment through which mobility and access should be considered. For this reason, arterials typically are roadways with high traffic volumes and are frequently the route of choice for intercity buses and trucks.

Route 177/Main Street is a minor arterial roadway since it spans across the state of Illinois. Minor arterials provide service for trips of moderate length, serve geographic areas that are smaller than their higher (“major”) arterial counterparts and offer connectivity to the higher arterial system (major arterials, expressways, freeways, interstates). In an urban context, they interconnect and augment the higher arterial system, provide intra-community continuity and may carry local bus routes. The general range for daily traffic volumes on a minor arterial is 3,000-14,000 vehicles per day (vpd).

Within the project limits, Route 177/Main Street is 30 mph. However, just outside of city limits, the speed limit increases to 55 mph. It is important to slow down vehicles as they transition from the rural high-speed roadway outside of town, to the low-speed Main Street through the city limits. It is also important to provide safe pedestrian facilities to provide access for active transportation uses throughout the corridor. Speed has a direct impact on crash severity and is more likely to be lethal when motorists are travelling at higher speeds. In fact, when a pedestrian is hit by a motor vehicle travelling at 40 mph,

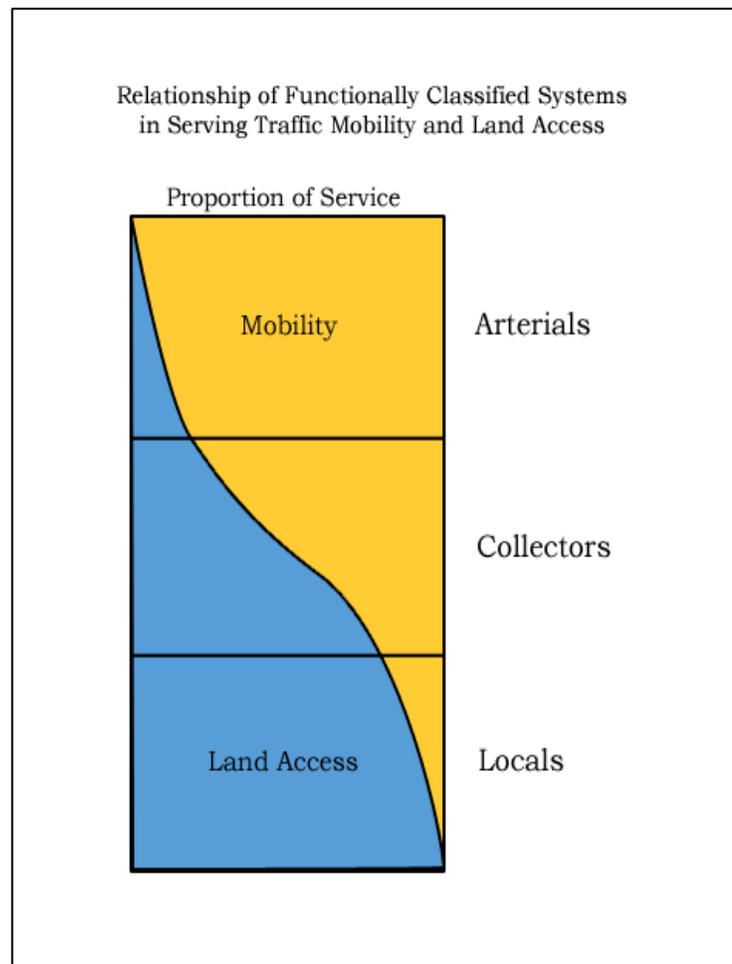


Figure 7 Roadway Functional Classification – vehicular

the odds of surviving the crash are 1 in 10; just 10-percent, see **Figure 8**. From community feedback, speeding is currently not a major issue on Route 177/Main Street in the City limits of Mascoutah.

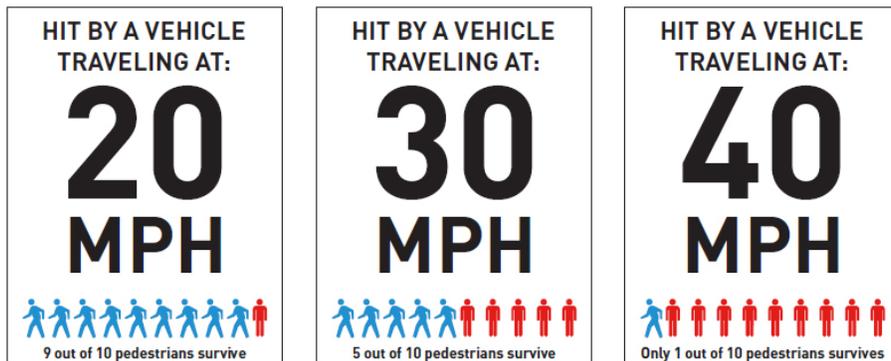


Figure 8 Chance of surviving a crash as a pedestrian at varying speeds

Sidewalks, Pedestrian Facilities, and ADA

As discussed in the typical sections segment above, continuous sidewalks currently exist on both sides of Route 177/Main Street in the project area and they vary in width between 5 ½ -ft to 8-ft. The condition of the sidewalk, however, is poor in some locations. We received feedback from multiple community members that the status of the sidewalk does not make for a very pedestrian-friendly environment in downtown Mascoutah. The cracks and crumbling pavement of the sidewalk is not amenable to people of all abilities and most curb ramps are not ADA compliant. Also, the presence of light poles in the sidewalk constricts the clear width of the sidewalk in some locations to substandard levels, which is also not compliant with ADA standards. See **Image 3** through **Image 5**.



Image 3 Sidewalk Pavement Cracks on Route 177/Main St



Image 4 ADA Non-Compliant Sidewalk Curb Ramp on Route 177/Main St



Image 5 Sidewalk Restricted Clear Width on Route 177/Main St

There are some marked pedestrian crosswalks in the project corridor. At the signalized intersection with 6th Street, there are marked crosswalks on every approach with pedestrian countdown timers, making this a great crossing location for pedestrians on Route 177/Main Street, see **Image 6**. Most other intersections have one standard marked crosswalk on the east side of the intersection. There are no pedestrian crossing signs to warn vehicles to yield to pedestrians, which is not ideal for these locations

since Route 177/Main Street traffic is free flow. There is one mid-block crossing in the corridor outside the post office west of Railway Street. This mid-block crossing also does not have any pedestrian crossing signage, see **Image 7**. The sidewalks were recently repaved and ADA curb ramps were added to the section of Route 177/Main Street between Lebanon Street and Independence Street. The pedestrian facilities on the rest of Route 177/Main Street should be updated in coordination with the future IDOT resurfacing project.



Image 6 Crosswalks at the Signal at 6th Street



Image 7 Mid-Block Standard Crosswalk on Route 177/Main St

Church Street

Church Street's existing roadway infrastructure was also analyzed, as it was identified as a potential important component to the transportation system of downtown Mascoutah. The typical section on Church Street consists of one lane in each direction, a sidewalk on the north side, and on-street parking. The roadway is classified as a local road. The lack of pedestrian facilities on the south side of Church Street present a barrier for pedestrians in the corridor.

Typical Section

The typical section on Church Street is difficult to define as there are no pavement markings to delineate the travel lanes and on-street parking, see **Image 8**. From curb to curb, the roadway width is approximately 33-ft in the study area. The north side of the street is mostly neighbored by houses and some public uses like the library, churches, and offices. The south side of the street is largely crowded by driveways into the rear parking lots for the businesses that front Main Street. As such, on-street parking occurs on the north side and not on the south side of Church Street. The 33-ft roadway width allows for 12-ft for each travel lane and 9-ft for parking. The north side of the street has a continuous sidewalk. East of 2nd Street, there is 3-ft to 6-ft buffer between the sidewalk and edge of curb, while west of 2nd Street, there is no buffer provided. There are no sidewalks present on the south side of Church Street.



Image 8 Church Street Typical Section

Traffic Control

The intersection at Railway Street and Church Street is all-way stop controlled. The remaining intersections on Church Street are side-street stop controlled. For the intersections at 6th Street, Route 4/Jefferson Street, Independence Street, and John Street, Church Street is stop-controlled and the cross-street is free-flow. At the remaining intersections within the study area, Church Street is free-flow and the cross-streets are stop controlled.

Functional Classification & Speed Limit

Church Street is classified as a local road. Locally classified roads account for the largest percentage of all roadways in terms of mileage, both in Mascoutah and nationwide. They are not intended for use in long distance travel, except at the origin or destination end of the trip, due to their provision of direct access to abutting land. Bus routes generally do not run on local roads since they are often designed to discourage through traffic. As public roads, they should be accessible for public use throughout the year. The general range of daily traffic volumes on a local road is less than 1,000 vpd.

According to the city ordinance for local streets, the speed limit is 25 mph¹. Speeding is not currently a major problem on Church Street.

Sidewalks, Pedestrian Facilities, and ADA

As discussed earlier, sidewalks are provided on the north side of Church Street, but not on the south side. Since most businesses are on the south side of Church Street, the lack of pedestrian facilities is a barrier for pedestrian connectivity through downtown Mascoutah. The sidewalks on the north side are in fairly good condition. There are no marked crosswalks on Church Street but there are some curb ramps that are ADA compliant.

¹ https://library.municode.com/il/mascoutah/codes/code_of_ordinances?nodeId=CH24MOVECO_SCDSPZO

Route Jurisdiction

It is important to consider route jurisdiction in planning for future improvements since not all roadways are under the City of Mascoutah’s control. Within city limits, there are three roads which are owned and maintained by IDOT:

1. Route 177/Main Street,
2. Route 4/Jefferson Street, and
3. 6th Street.

Any recommended improvements on these routes will need to be approved and coordinated with IDOT and must comply with their policies and standards. The City has more control over roadway improvements under their own jurisdiction. The timing of this Great Streets project is optimal since IDOT has scoped out a reconstruction project of Route 177/Main Street from County Road to Mine Road for the multi-year program of 2022-2026. They are in the beginning phases of scoping the project, providing an opportunity for the City of Mascoutah to partner with IDOT in implementing the Great Streets plan in coordination with this project.

Traffic Volumes

Traffic volumes are also important to consider when evaluating the roadway. Traffic volumes were pulled from IDOT’s Traffic Count GIS Mapping Tool. This tool provides average annual daily traffic (AADT) volumes for certain roadways throughout the state of Illinois. As shown **Figure 9**, volumes are fairly low in the City of Mascoutah. For reference, volumes in the City of St. Louis can reach upwards of 30,000 vehicles per day (vpd) on non-interstate routes. With volumes in Mascoutah less than 7,000 vpd, traffic congestion is not a major issue. This tool also provides average truck volumes. We found there to be 2% of truck traffic on Route 177/Main Street, showing it is not a major truck route.



Figure 9 AADT's in Mascoutah²

Crashes

We completed a high-level safety analysis of Route 177/Main Street using the most recent available crash data, which is from 2010 to 2017. We found that 86 crashes occurred on Main Street within

² <http://www.gettingaroundillinois.com/gai.htm?mt=aadt>

the project limits. This comes to an average 10.8 crashes per year. As shown in **Figure 10**, there was spike in the number of crashes in 2015. However, the number of crashes remains fairly steady over the eight years outside of that anomaly. Federal Highway Administration (FHWA) breaks down crashes into severity categories. “PD” means property damage only. “A-injury” is the least severe injury where injuries are not visible and “C-injury” is the most severe injury where the crash resulted in a disabling injury. “K” refers to fatal crashes. There were six disabling injury crashes and zero fatal crashes over the eight years on Route 177/Main Street within the study area.

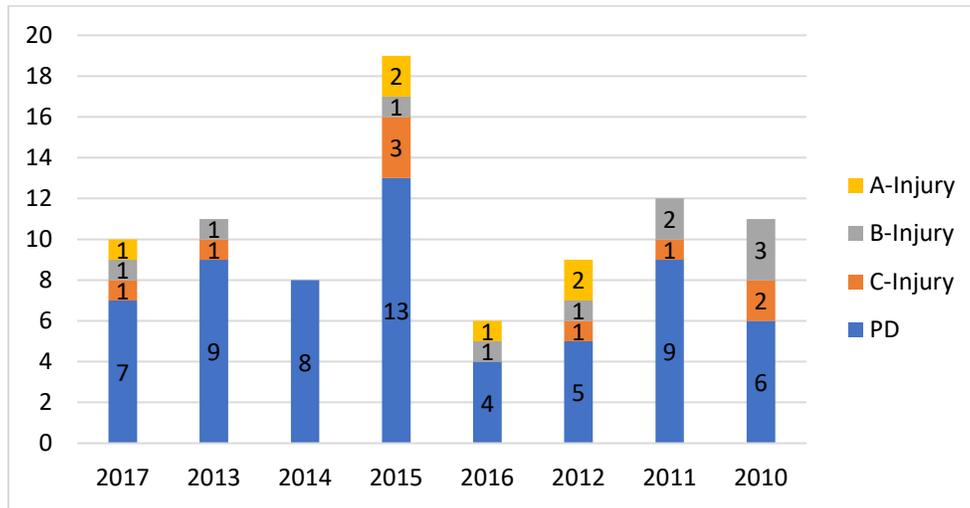


Figure 10 Crashes on Route 177/Main St by Year and Severity

Figure 11 shows the breakdown into types of crashes. It’s distributed mostly between parked motor vehicle, rear ends, fixed objects, angle, and turning crashes. This shows there is not one main issue that contributes to the types of crashes within the study area.

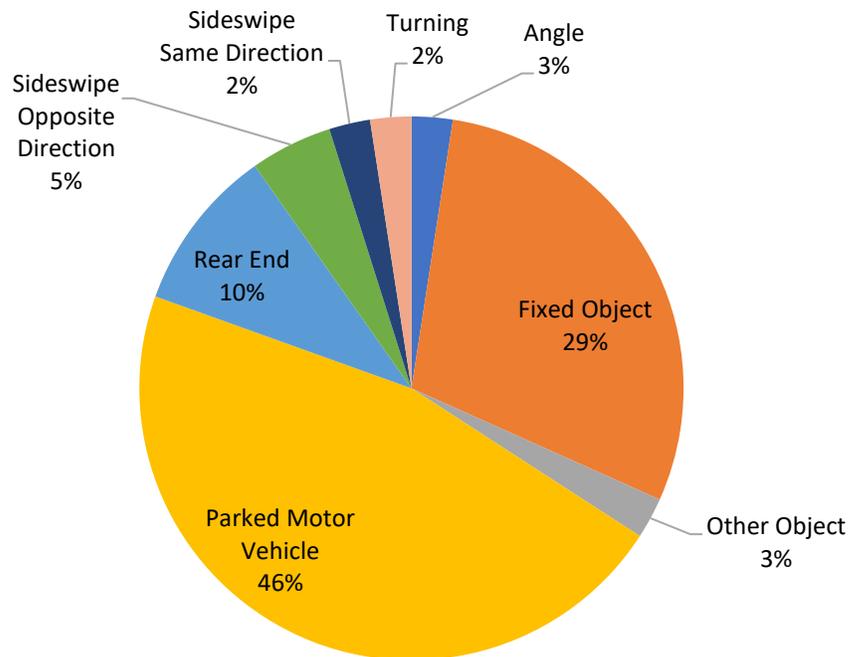


Figure 11 Crash Type Breakdown for Route 177/Main Street (2010-2017)

From the crash data, we found the highest number of crashes occur at the intersection of Route 4. 12 crashes occurred at this intersection within the study period, comprising 14% of the total crashes along Route 177/Main Street. This is to be expected since it is the heaviest traffic volume intersection. The second highest number of crashes occurred at the intersection of Railway Street, followed by the intersection with 6th Street. These are the other two busiest intersections along Route 177/Main Street. Crash data along Route 177/Main Street is shown in **Figure 12**. Overall, crash rates and severities are low on Route 177/Main Street within the project corridor.



Figure 12 Crash Data Points along Route 177/Main Street (2010-2017)

Parking

Another key component to consider with transportation is parking. In order for businesses to thrive, we want to ensure plenty of parking, but we don't want an abundance of parking that is taking up valuable real estate space. Currently, there is an ample amount of parking in downtown Mascoutah. As discussed previously, there is on-street parking available along Route 177/Main Street and on Church Street to the north. There is also on-street parking available on State Street, the parallel street south of Route 177/Main Street. In addition, many of the cross-streets north and south of Route 177/Main Street have some on-street parking spots and many of the businesses have their own parking lots on street corners and in the rear.

Based on aerial imagery, we estimated 95 on-street parking spaces on Route 177/Main Street from 6th Street to Mine Road. North of Route 177/Main Street, we estimated 480 parking spots in parking lots and 165 on-street parking spaces on Church Street from 6th Street to Mine Road. To the south, we estimated approximately 300 parking spots in lots south of Route 177/Main Street and 125 on-street parking spots along State Street. **Figure 13** below shows these parking estimates throughout the entire study corridor (east, central and west segments), indicating ample parking supply.

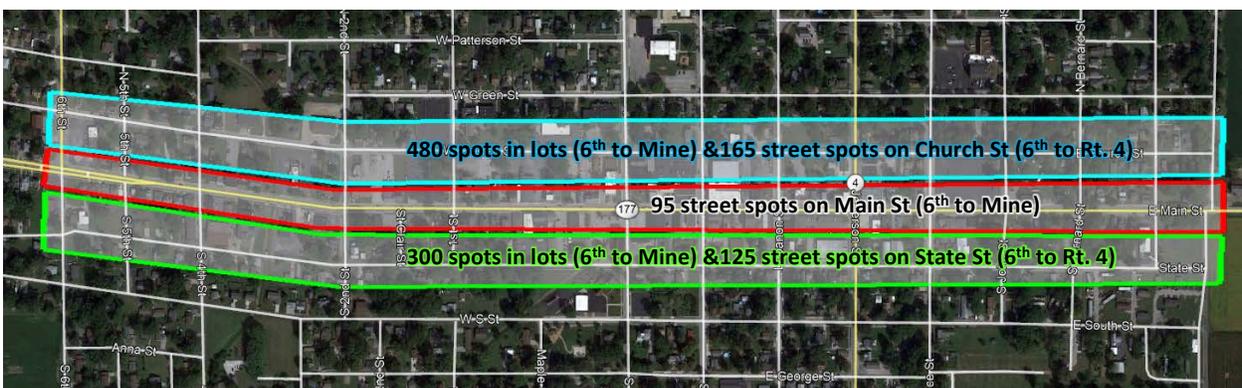


Figure 13 Parking Supply Estimates around Route 177/Main Street

Bike and Pedestrian Facilities

The City of Mascoutah currently has one bike trail on the north side of town along the Big Ditch, shown in **Image 9**. This path runs next to Scheve Park, a popular destination in town, and has cultivated a growing active transportation community in Mascoutah. Since its implementation, the City has actively pursued more opportunities to expand the bike and pedestrian network in town. They are currently pursuing funding opportunities to extend the existing trail across the Big Ditch and follow the west side of Route 4/Jefferson Street north to Onyx Drive. They are also pursuing funding for a new trail along the L&N Railroad ROW on the south side of town. In speaking with community members, we have found some of the more experienced bikers will bike in the roadway throughout town and that there is a desire to expand the bike-pedestrian network.



Image 9 Existing Big Ditch Trail

Some pedestrian facilities through town are in need of repairs and updates. There are a number of local streets which do not have sidewalks and some sidewalks through town are in poor condition, as well as a number of pedestrian facilities that are not ADA compliant. Through the engagement process, we found the residents desire more pedestrian facilities and pavement repairs.

Transit

Currently public transportation plays a small role in the Mascoutah Community. There are no bus lines in the project area, but there is a Metrolink stop nearby at Scott Air Force Base. A planned Metrolink Expansion to the Mid-America Airport which will enhance transit access to the community in future years, see **Figure 14**.



Figure 14 City of Mascoutah Public Transit



ENGAGEMENT SUMMARY

Community engagement is key component in the planning process. It is important to understand the community’s needs, vision, and goals for us to provide the best recommendations that serve the residents of Mascoutah. Due to the current COVID-19 crisis, all engagement had to be conducted virtually. This included virtual meetings with key stakeholders and community members as well as video presentations with online surveys. The team held three rounds of virtual engagement:

1. Existing Conditions and Community Goals;
2. Preliminary Options; and
3. Preliminary Plan.

The community provided important feedback on current conditions in Mascoutah and their preferences on the options developed by the team. The community’s responses and the options presented are discussed below. All transportation related survey data is provided in **Appendix 2**.

Regional Options

The demand for regional bike trail connections was explored. We found that 91% of survey respondents indicated they would use a bike path to connect to a regional trail network. The team presented three options for regional trail connections: a connection to St. Clair County’s MetroBikeLink trail network in Belleville, IL; a connection to Scott Air Force Base, and a connection to MidAmerica Airport to serve as a multi-modal hub once the MetroLink extension is completed from Scott Air Force Base. We found that the connection west to the MetroBikeLink network was the highest priority, followed by the connection to Scott Airforce Base, and lastly the connection to MidAmerica Airport was the lowest priority. See **Figure 15**.

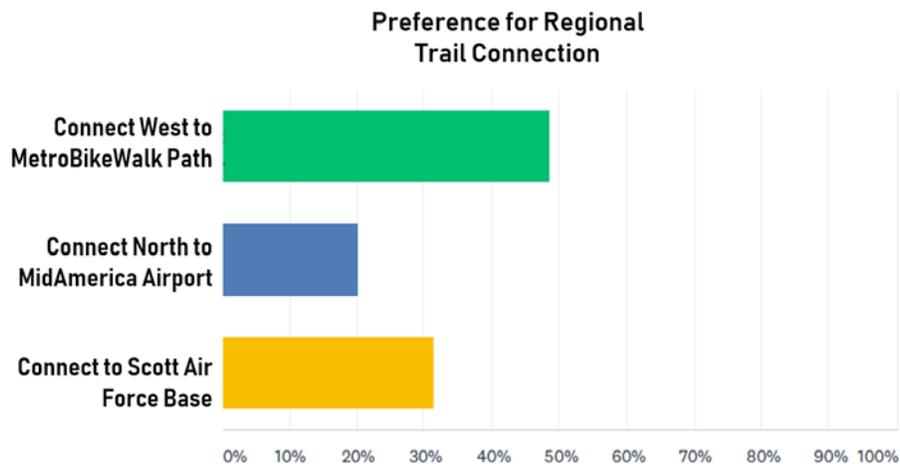


Figure 15 Survey Responses

We found that most survey respondents preferred off-street bike facilities, like a shared use path, compared to on-street facilities, like dedicated bike lanes in the roadway shoulder. We also found that most respondents currently drive to work and even if a regional trail network were implemented, it would not allow them to bike to work. Some survey respondents had some concerns with land acquisition, security, and maintenance of a regional trail connection.

We also discussed regional wayfinding signage to help attract visitors to the town of Mascoutah. This included brown attraction signs off I-64 and directional signage off Route 177 and Route 158. Most survey respondents (64%) were in support of these types of regional signage.

City-Scale Options

We also explored a city-wide bike and pedestrian network through the City of Mascoutah. We presented an initial network of shared use paths and bike boulevards through the town. See **Figure 16**.

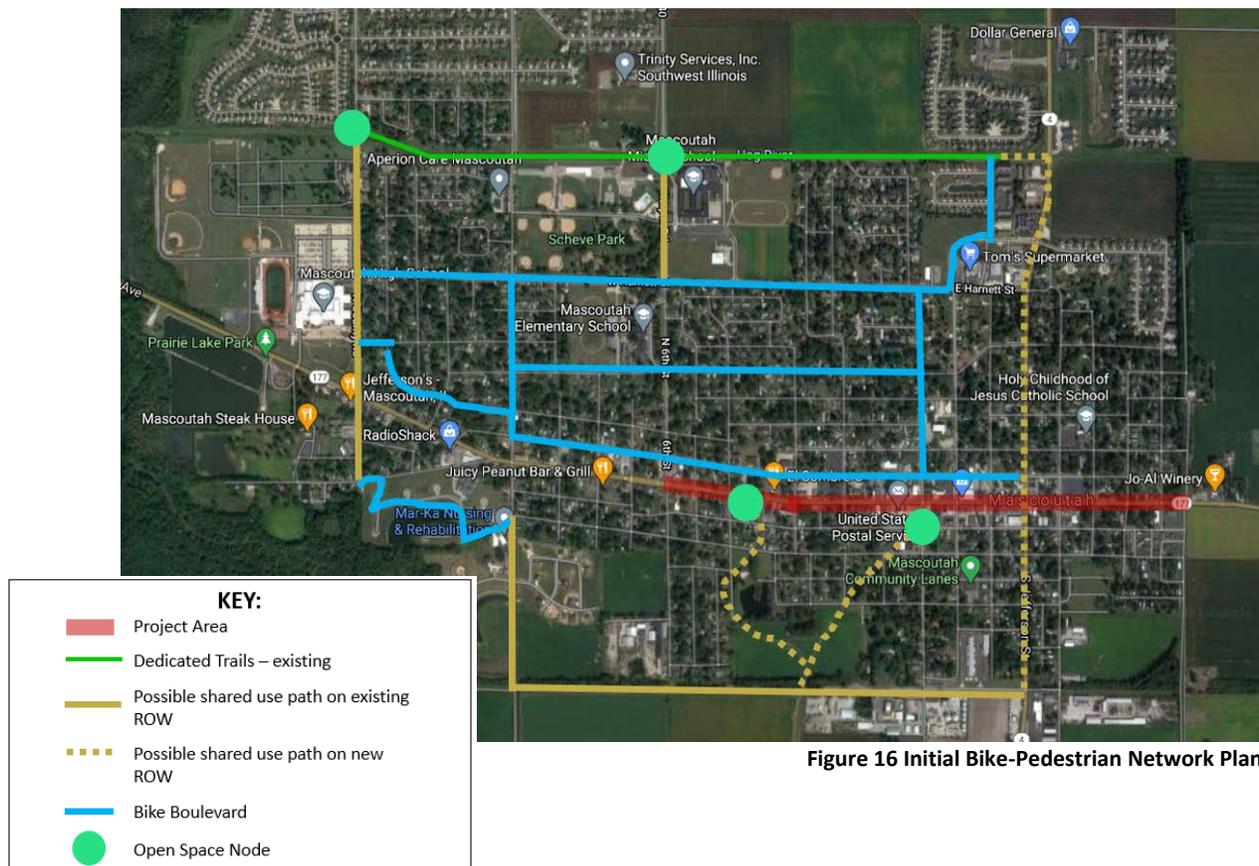


Figure 16 Initial Bike-Pedestrian Network Plan

We found most survey respondents generally liked the proposed network with some adjustments. Some recommended adjustments included not using Harnett Street as a bike boulevard, staying off Route 4/Jefferson Street, adding connections from north of the Big Ditch trail, and implementing a shared use path on Church Street instead of a bike boulevard. These comments have been incorporated into the final plan, presented under the “Recommendations” section.

We presented a wayfinding network for city wide bike-pedestrian network as well. Survey respondents were overwhelming in support of this addition (80% in favor of the wayfinding network). We also asked about using entry signage on Route 177 east of Mine Road similar to the Mascoutah monument on Route 4 off I-64. Most survey respondents were indifferent to the idea or thought it would not be worth the cost. See **Figure 17**.

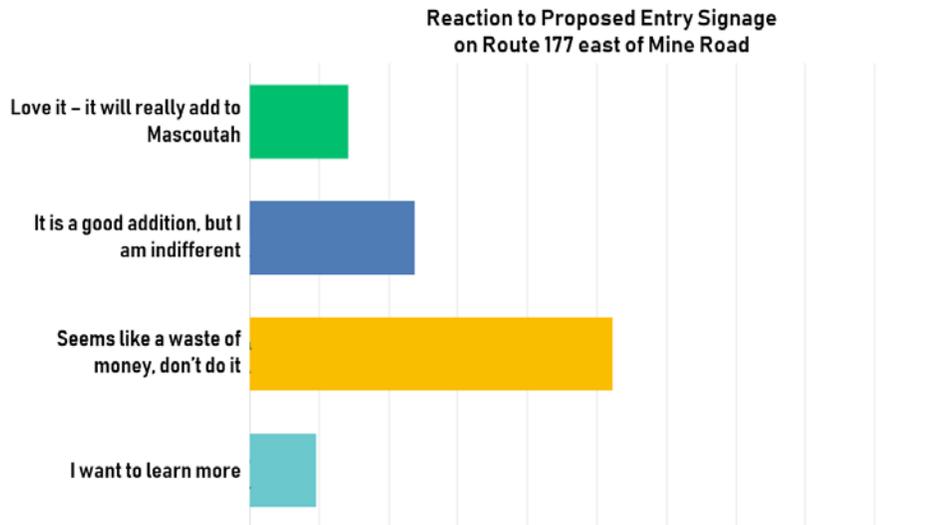


Figure 17 Survey Responses

Corridor Options

We presented different lane configurations for each segment along Route 177/Main Street to understand the community’s vision for transportation uses of the corridor. Options presented include maintain existing lanes, remove on-street parking and widen sidewalks, and to utilize some existing on-street parking spots for parklets or food truck parking.

Parklets are intended to provide people a place to stop, rest, and commune while downtown. In **Image 10**, parking spaces have been turned into outdoor dining area for a restaurant. This allows pedestrians to use their full designated space on the sidewalk, without being impeded by other uses. Parklets could also be used for enhanced green spaces or bike parking. They allow the flexibility for quick project turnarounds and the chance to restore parking capabilities when needed. However, because of safety and liability reasons, parklets and temporary food trucks are not allowed on state routes. Thus these will not be incorporated into the final plan for Route 177/Main Street.



Image 10 Example Parklet

Overwhelmingly, we found survey respondents preferred removing on-street parking from Route 177/Main Street and providing wider sidewalks for each corridor segment. 52% of survey respondents indicated they wouldn’t mind parking only on side-streets or back lots off Route 177/Main Street and 33% of respondents indicated they would prefer to keep some parking on Route 177/Main Street, but they wouldn’t mind walking a few blocks to their destination. We found that 56% of survey respondents preferred removing parking and implementing wider sidewalks on Route 177/Main Street in the west segment, 50% wanted this configuration in the central segment, and 70% wanted this configuration in the east segment. See **Figure 18**, **Figure 19**, and **Figure 20**. While most respondents were in favor of relocating on-street parking off Route 177/Main Street, there were still some people who were hesitant to do so.

West Segment (Route 177/Main Street from 6th Street to 2nd Street)

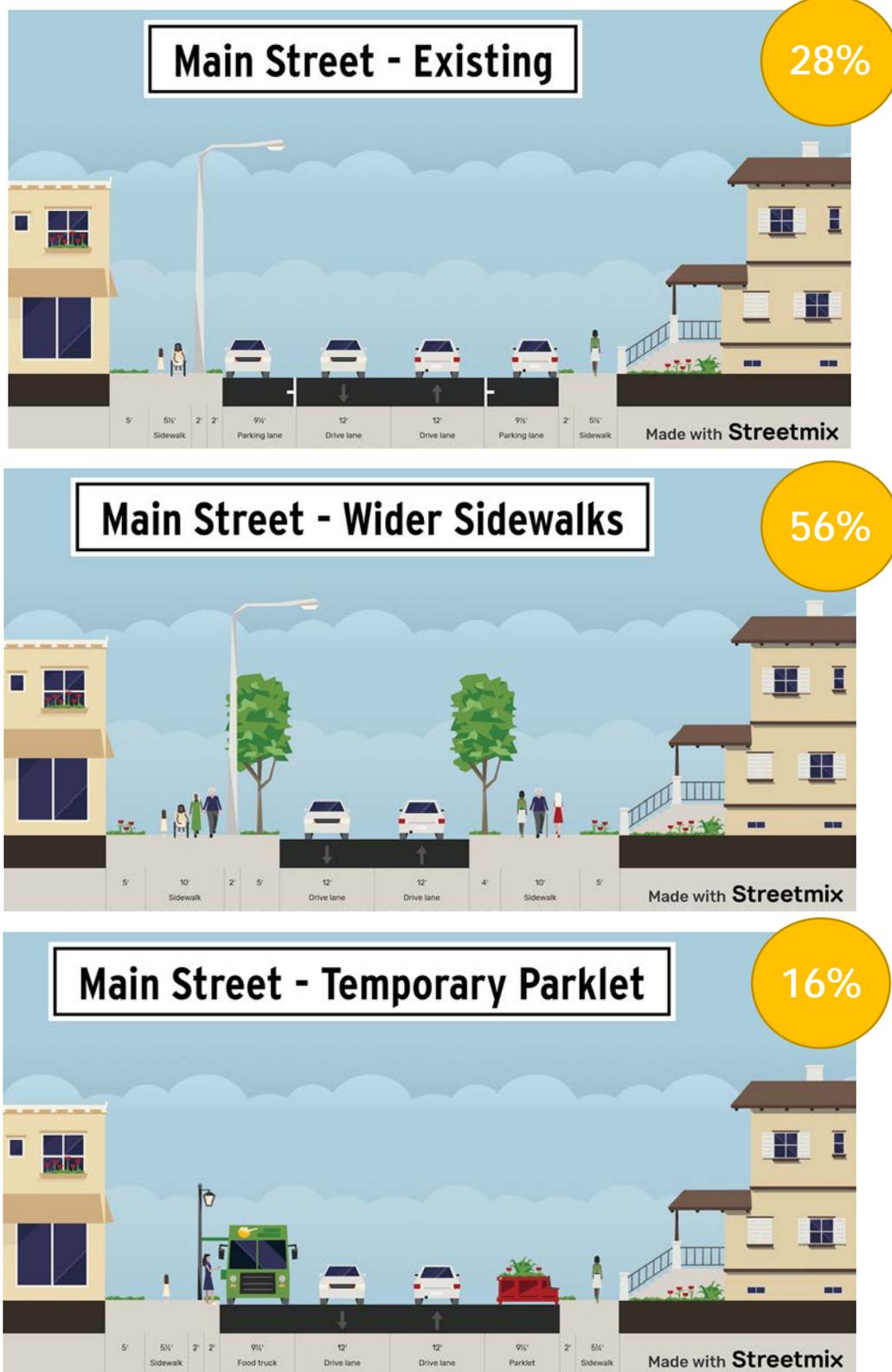


Figure 18 West Segment Corridor Voter Preferences

Central Segment (Route 177/Main Street from 2nd Street to Lebanon Street)

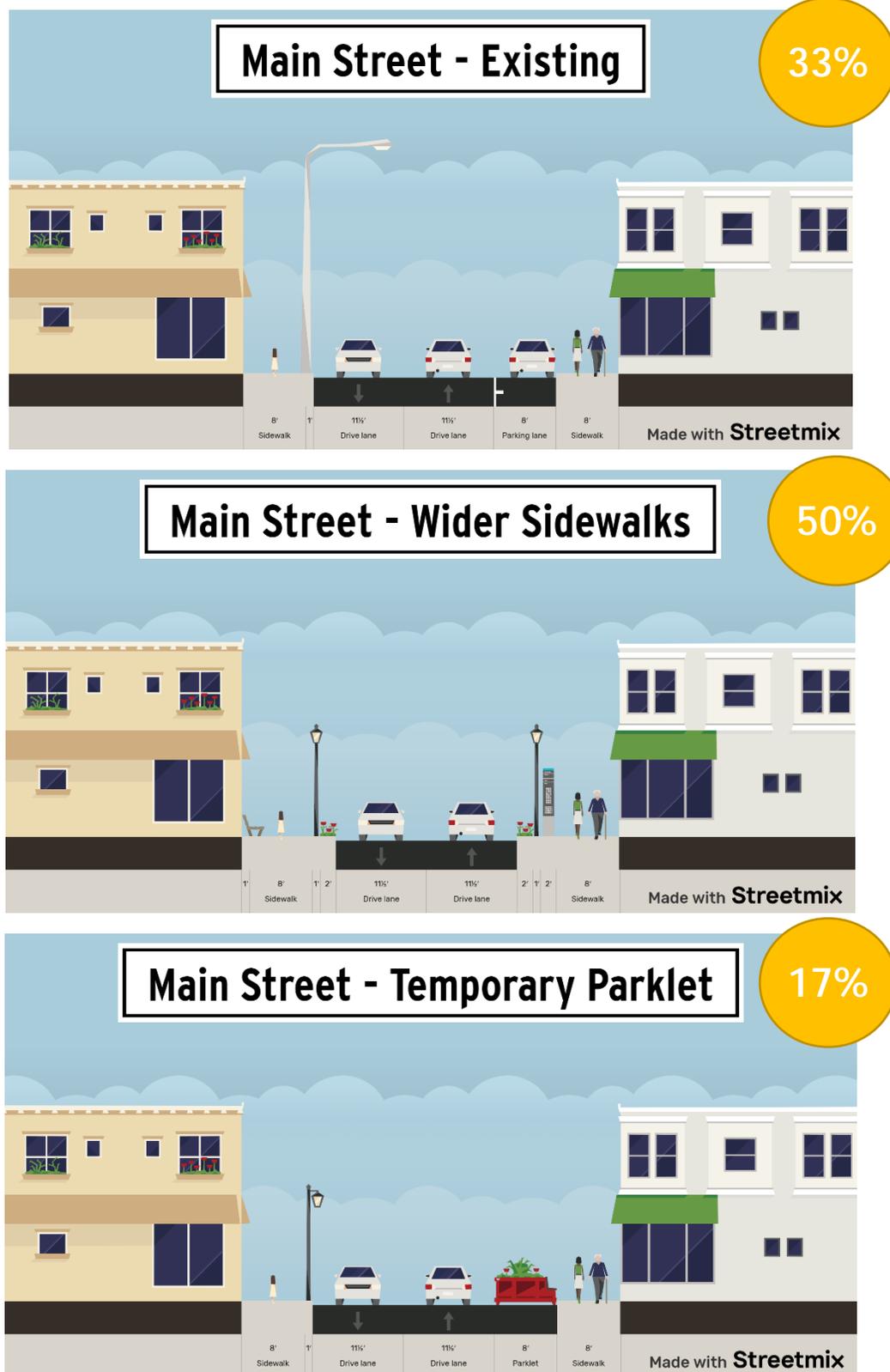


Figure 19 Central Corridor Voter Preferences

East Segment (Route 177/Main Street from Independence Street to Mine Road)

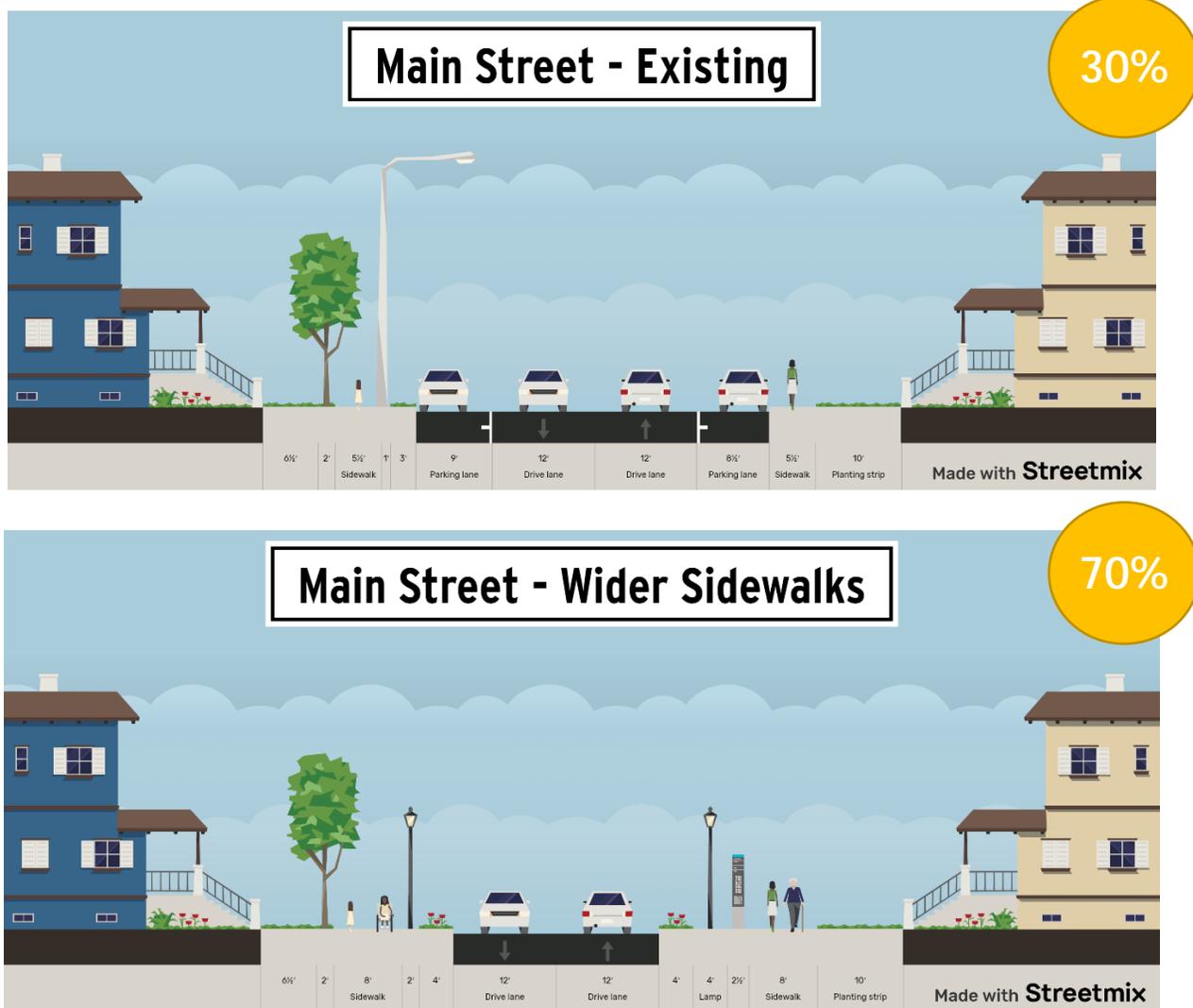


Figure 20 East Segment Corridor Voter Preferences

We also presented a wayfinding signage plan for Route 177/Main Street at both the vehicular scale and pedestrian scale. These signs would help direct visitors to parking, key destinations, and all the amenities in the corridor. 76% of survey respondents were in favor of including these in the plan.

RECOMMENDATIONS

Based on the existing conditions analysis and engagement with the community, design concepts were developed to enhance the transportation network in Mascoutah from a regional scale, city scale, corridor scale, and down to individual key intersections. Within the city, the vehicular transportation network is very healthy. There is an existing great grid network of surface streets which allow people to drive between key sites within the city with ease. Traffic congestion and safety are not major issues for drivers in Mascoutah. The bigger needs are to enhance the pedestrian and bike network in town and ways to attract outside visitors to the City Center. To that end, we have proposed a robust bike and pedestrian

network through the city, possible regional connections to St. Clair County MetroBikeLink System and Scott Air Force Base, and a comprehensive wayfinding network to enhance the visitor experience.

Regional Connections

Through our multi-step engagement process, we found there is a great bike community in Mascoutah and there is a strong appetite to connect the city to multiple regional major destinations. In the order of most preferred to least, those include:

1. St. Clair County MetroBikeLink System;
2. Scott Air Force Base; and
3. Mid America Airport.

Figure 21 shows the existing bike trails and some regional connection options from the City of Mascoutah. Since most of these connections are outside of city limits, partnering and planning is key to implementation. These trails would require coordination with several entities, including St. Clair County Transit, IDOT, and potentially Metro East Park and Recreation District. These connections would be a long-range goal for the City of Mascoutah but partnering and planning should begin as soon as possible. Refer to “Implementation and Partnerships” section at the end of this document for next steps in establishing these connections.

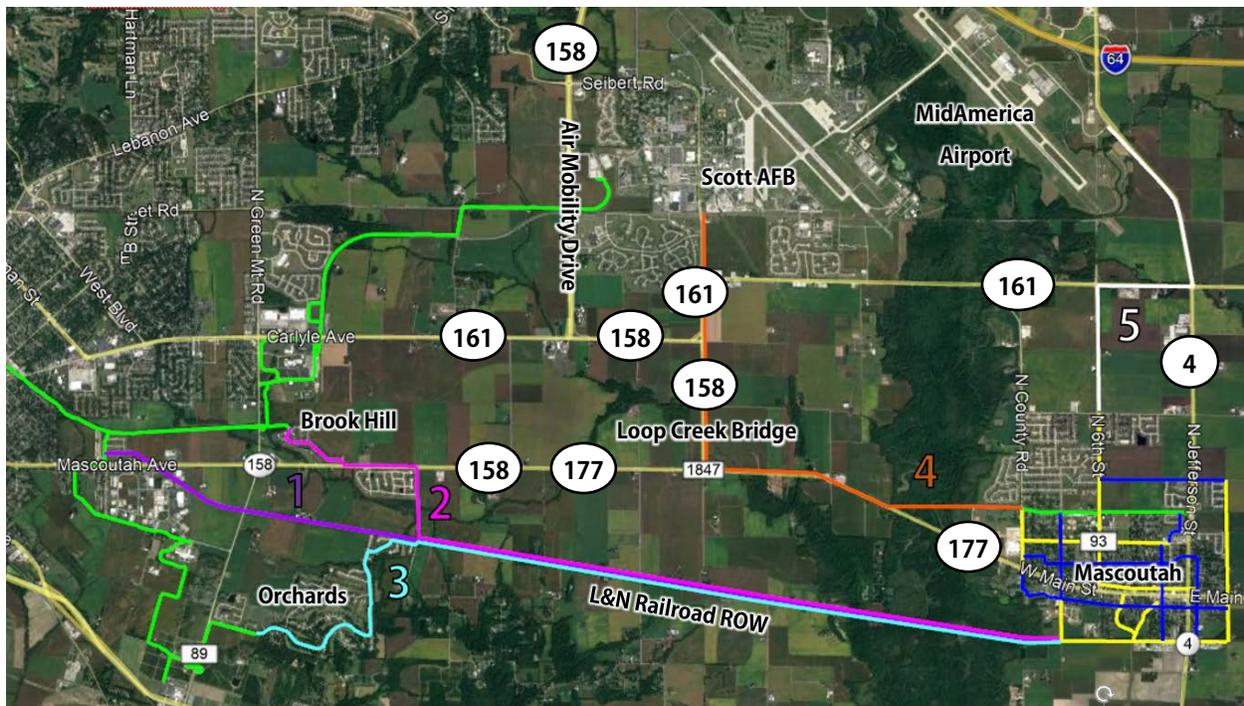


Figure 21 Regional Bike Network Connection Options

Legend

- Existing
- Future Bike Boulevard
- Future SUP
- Option 1 - L&N Rail ROW Path
- Option 2 - L&N ROW Path
- Option 3 - L&N Rail ROW
- Option 4 - Extension of Existing SUP to SAFB
- Option 5 - Connection to MidAmerica Airport

We found the highest demand to be for a connection to St. Clair County MetroBikeLink System. This network is expansive, following the Metrolink for 14 miles from the Fairview Heights stop out to Scott Airforce Base. It is a great dedicated shared use path for bikes and pedestrians and is heavily utilized throughout St. Clair County, see **Image 11**. It would be a great asset to the bike community in Mascoutah if a connection was achieved. The figure shows a portion of the existing MetroBikeLink System in Belleville, shown in bright green. As shown in **Figure 21**, there are multiple options for establishing a bike path to the existing trail network. Options 1 – 3, shown in purple, pink, and bright blue, utilize the L&N Railroad right-of-way (ROW) to convert the old rail system into a bike trail. Using the L&N Railroad ROW is an attractive option since there are grants available for this particular type of project. In fact, the City of Mascoutah is actively pursuing such a grant to convert the L&N Railroad ROW into a trail on the south side of town from County Road to Mine Road. This potential future trail could be extended further west to connect to the MetroBikeLink network in a few different ways. The biggest obstacle would be establishing a trail through the wetland area just west of town including the crossing of Silver Creek and Loop Creek, see **Figure 22**.



Image 11 St. Clair County MetroBikeLink Trail

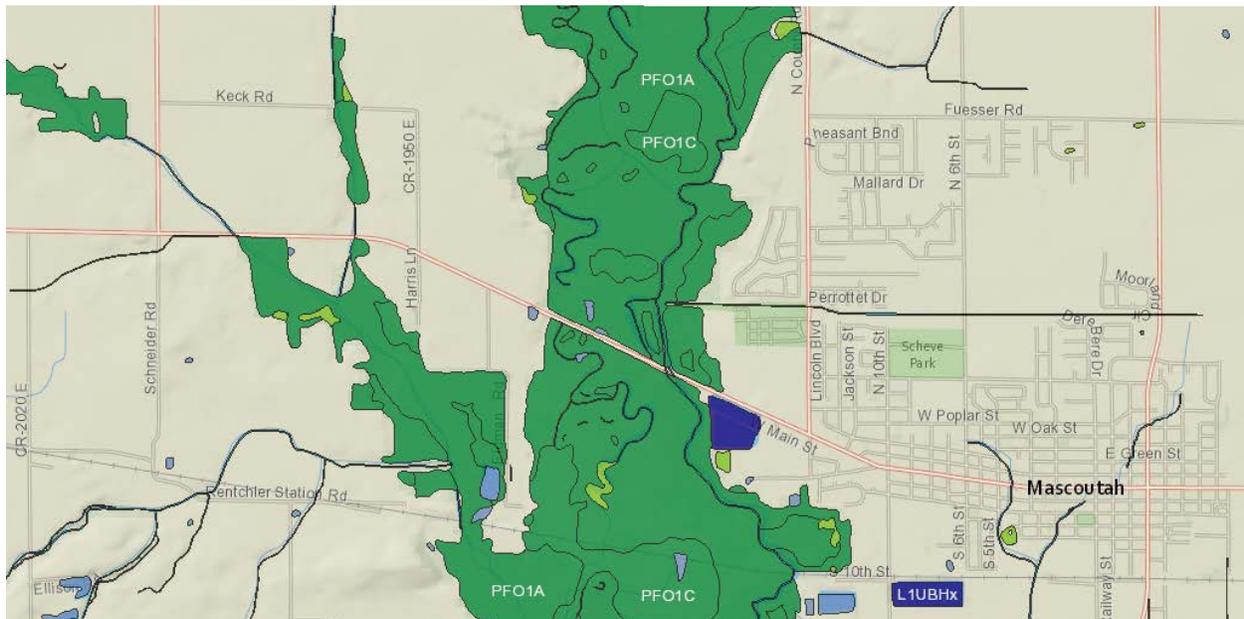


Figure 22 Wetland Area west of Mascoutah³

In **Figure 21**, option 1 (purple) is the most direct route to the MetroBikeLink System along the L&N Railroad ROW. It would require the construction of approximately 7.7 mi of new shared use path and also

³ <https://www.fws.gov/wetlands/data/mapper.html>

would require a new separated grade crossing of the trail over Green Mount Road. This option would likely be the most expensive.

Option 2 (pink) involves constructing a new shared use path along the L&N ROW up to Plum Hill School Road, transition bikes to share the road with vehicles on the low volume Pun Hill School Road up to Route 177, construct a new shared use path along Route 177 up to the Brook Hill neighborhood, and transition bikes to roads through the neighborhood to reach the Brook Manor Connector north of the neighborhood. This trail would consist of 5.7 mi of new shared use path and 1.2 mi of shared road facilities.

Option 3 (blue) also involves constructing a new shared use path along the L&N ROW up to Plum Hill School Road, but instead direct bikers south to access the existing MetroBikeLink Connection through the Orchards Neighborhood. This trail would consist of 3.5 mi of new shared use path and 2.2 mi of shared road facilities.

Option 4, shown in orange in **Figure 21**, shows a potential connection to Scott Air Force Base (Scott AFB). This would require 4.5 mi of new shared use path. The biggest obstacles would be establishing a trail through the wetland area just west of town including the crossing of Silver Creek and Loop Creek as well as obtaining the necessary ROW. It is also important to know that the current bridge over Loop Creek on Route 158 between Route 161 and Route 177 overtops frequently. IDOT is currently evaluating a possible extension of Route 158/Air Mobility Drive southward to Route 177. If this extension occurs, another possible route for the shared use path to avoid the Loop Creek bridge would be to extend the shared use path further west along Route 177 to the Route 158/Air Mobility extension and follow Route 158/Air Mobility north to the Scott Air Force Base.

Option 5, shown in white in **Figure 21**, shows a potential connection to the MidAmerica Airport. The St. Clair County MetroBikeLink system is also planning for a future extension of the trail from Scott AFB to the MidAmerica Airport, which would further enhance the appeal of this connection and could support a connection to Scott AFB if establishing a connection across the wetland and along Scott Drive proves too difficult or expensive. A connection to the MidAmerica Airport has the potential to serve as a multi-modal hub for the City of Mascoutah. As a multi-modal hub, one could ride their bike to the station, transfer to the train, and commute to work, see **Image 12**. This option would require the construction of 3 to 4 mi of new shared use path. It's full potential will only be realized once the future extension of the MetroLink and MetroBikeLink System is completed.



Image 12 Example Bike Shelter and Multi-Modal Hub

These regional connections would be an asset for the residents of Mascoutah, but also would provide a connection for regional bicyclists to access the City of Mascoutah and its future city-wide bike network.

Regional Wayfinding Plan

A regional wayfinding plan can help attract visitors to experience the City of Mascoutah. For example, adding a brown attraction sign off I-64 that reads “Historic Mascoutah Heritage Museum – Next Exit” could increase the number of tourists in Mascoutah, see **Image 13**. Signs similar to the existing monument located on Route 4 to the north is also recommended to be added to the other entrances of town, see **Image 14**. All signs on state routes must meet IDOT policy. Refer to the “Implementation and Next Steps” Section at the end of this document for more information.



Image 13 Example Brown Attraction Freeway Sign



Image 14 Existing Mascoutah Welcome Monument

City Scale Recommendations

The proposed recommendations on the city scale include enhanced pedestrian and bike facilities throughout the town and enhanced way finding. These recommendations are made based off survey responses, available right-of-way, and the existing conditions analysis.

Recommended Bike and Pedestrian Network

Based on engagement with the community, there is an avid bike community in Mascoutah. There is a great existing multi-use path on the north side of town along the Big Ditch (Hog River) which is heavily utilized. Extending this network throughout town was identified as a priority for the residents of Mascoutah. To achieve a well-connected bike and pedestrian network, a variety of facilities are proposed through the city. Specifically, a network of sidewalks, shared use paths, and bike boulevards are proposed to allow residents to safely walk and bike throughout the City, see **Figure 23**.

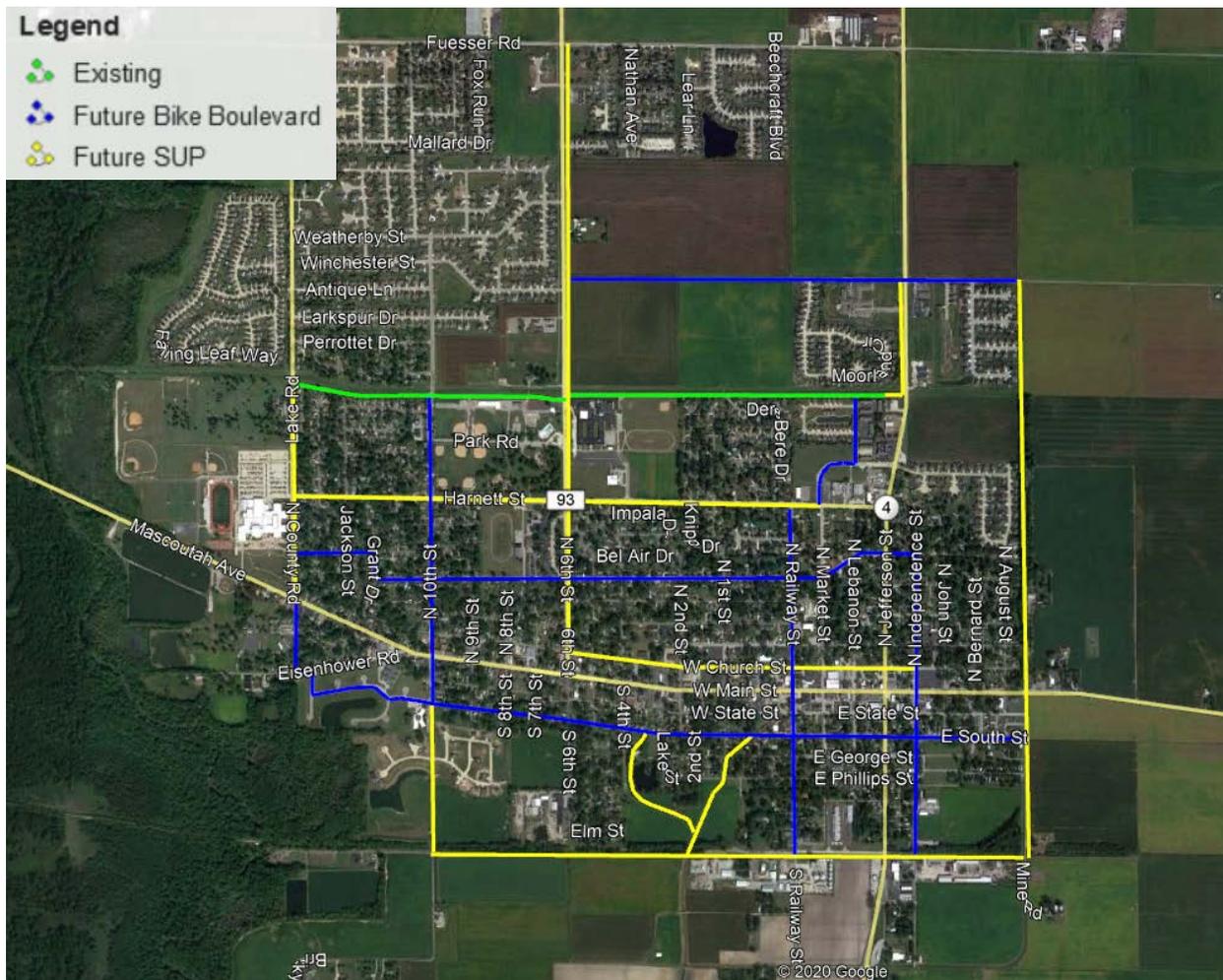


Figure 23 City of Mascoutah Bike-Ped Network Plan

Sidewalks should be implemented along all roadways where possible, however, sidewalks on the south side of Church Street are identified as a high priority for the Great Streets Plan in downtown Mascoutah. As discussed previously, shared use paths are dedicated paved facilities for bikes and pedestrians separated from the roadway. Shared use paths, shown in yellow in **Figure 23**, are proposed where there is available green space or along higher volume roads. Specifically, shared use paths are recommended on:

- Church Street;
- Harnett Street;
- 6th Street;
- Mine Road;
- N County/Lake Road; and
- L&N Railroad ROW.

All shared use path crossings in town are proposed as at-grade crossings since the roads are low speed and low volume. The shared use path crossings over roadways with a speed limit of 30 should have a trail crossing sign with advanced warning as shown in **Figure 24**. Most proposed crossings are across 30

mph roadways. Any crossing over roadways with a speed limit of 35 mph is recommended to have a pedestrian actuated warning beacon to enhance driver awareness and crossing safety, see **Figure 25**. These locations include the L&N Railroad ROW crossing over Route 4/Jefferson Street, the proposed trail on County Road across Route 177/Main Street, and the proposed extension of the existing Big Ditch trail north along Route 4/Jefferson Street at the intersection with Onyx Road. Note, the trail crossing at the intersection of Route 4/Jefferson Street and Onyx Road should be carefully considered since this is near to the entry point of town. The speed limit drops from 55 mph to 35 mph only 550-ft north of the intersection. To ensure vehicles are slowing down enough to provide a safe crossing for bikes and pedestrians, intersection improvements may be necessary. As development occurs in this area, the City should could work with IDOT to move the speed limit change further north. This intersection is discussed further under the “Key Sites” section of this report. These recommendations are consistent with IDOT policy TRA-23⁴ and the MUTCD.



Figure 24 Shared Use Path Crossing Signs for 30 mph Roadways



Figure 25 Pedestrian Actuated Beacon Crossing Signs for 35 mph Roadways

As discussed previously, bike boulevards are facilities where bikes share the roadway with vehicles. To enhance vehicular awareness of bikes and increase rider safety, share the road pavement markings and enhanced signage are recommended along bike boulevards, see **Figure 26**.



Figure 26 Bike Boulevard Signing and Pavement Markings

⁴ [http://idot.illinois.gov/Assets/uploads/files/About-IDOT/Misc/Peer-Exchange-Seminar/08%20-%20Module%20%20-%20Illinois%20Midblock%20Pedestrian%20Crossing%20Guidelines%20\(Marshall%20Metcalf\).pdf](http://idot.illinois.gov/Assets/uploads/files/About-IDOT/Misc/Peer-Exchange-Seminar/08%20-%20Module%20%20-%20Illinois%20Midblock%20Pedestrian%20Crossing%20Guidelines%20(Marshall%20Metcalf).pdf)

Bike boulevards, shown in blue in **Figure 23**, are proposed on low volume roads with limited right-of-way space. Specifically, bike boulevards are recommended on:

- 10th Street;
- Railway Street;
- Independence Street;
- Poplar Street;
- South Street;
- Onyx Road;
- Fountain View Drive;
- Mascoutah Plaza Drive;
- Lincoln Boulevard; and
- Winding Oaks Lane.

Note, the bike boulevard on Poplar Street would require some minimal additional pavement to complete a bike connection from the terminus of the Poplar Street to Independence Street, see **Figure 27**. This should be feasible since this area is already owned by the city. Also, Onyx Road is shown to extend to Harper Road in **Figure 23**. Although Onyx Road currently terminates east of 6th Street, there may be future plans to extend Onyx Road to 6th Street and the bike boulevard should extend to Harper Road if this connection is completed.



Figure 27 Poplar Street Terminus

Since bike boulevards require minimal changes to infrastructure and are relatively easy to implement, these are recommended to be the first phase of implementation of expanding the bike network in Mascoutah. Initial loop of bike boulevards are recommended on:

- 10th Street from the Big Ditch Shared Use Path to South Street;
- Railway Street from Harnett Street to South Street;
- Independence Street from Poplar Street to South Street;
- Poplar Street from 10th Street to Route 4/Jefferson Street;
- South Street from 10th Street to Independence Street;
- Fountain View Drive;
- Mascoutah Plaza Drive between Fountain View Drive and Harnett Street;

- Temporarily on Market Street between Harnett Street and Poplar Street until a shared use path is implemented on Harnett Street; and
- Temporarily on Church Street between 6th Street and Independence Street until a shared use path is implemented through the corridor.

See **Figure 28**.

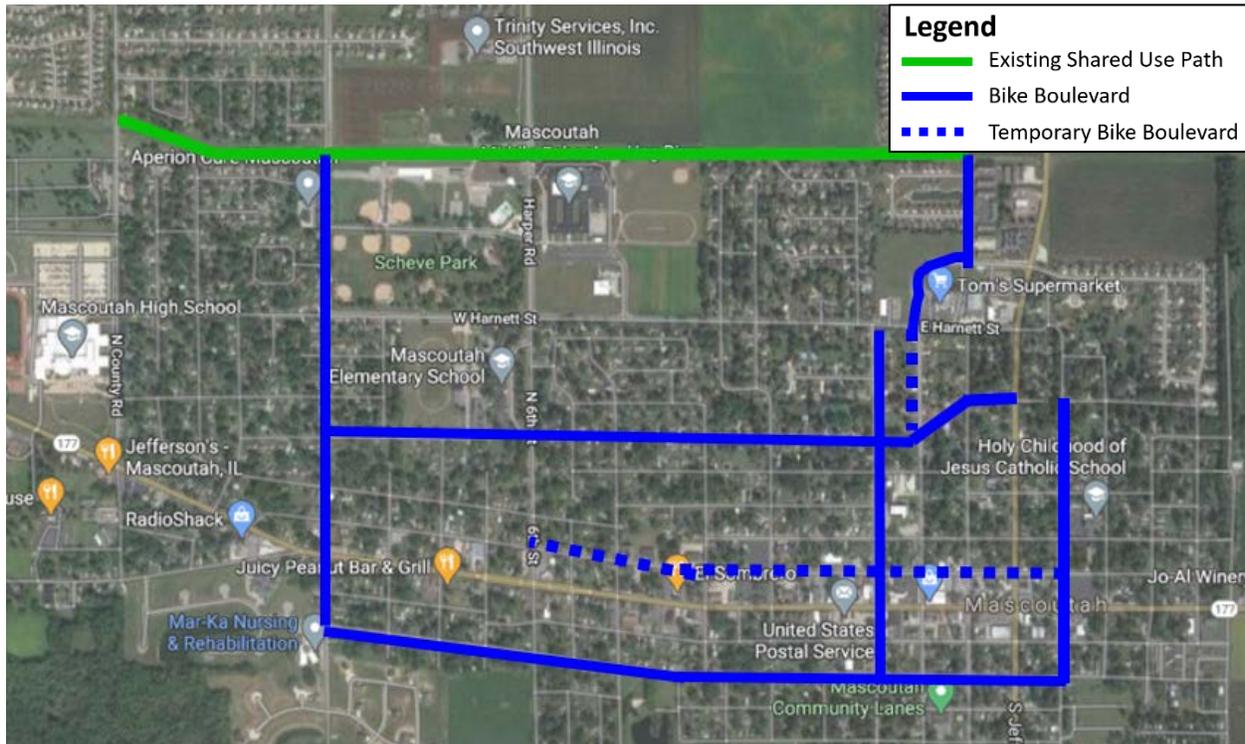


Figure 28 Mascoutah Bike Boulevard Network Initial Loop

City Wayfinding Signage Plan

Wayfinding signage is an important component to a well-designed pedestrian and bike network. It helps identify safe routes for bike riders and direct people to key destinations through town. It also enhances the experience on the trail system and is a good way to brand the town of Mascoutah for visitors. On the proposed bike trails, we recommend implementing informational and directional signage at strategic intersections.

Informational signage can inform riders when they have arrived at key destinations, their location within the bike network, and various route options through town. This type of signage includes maps, mile markers, and travel time to destinations. These are placed at trail heads and key locations in the bike network. Examples of informational signage are shown in **Figure 29**.



Bicycle wayfinding map of trails and amenities



Bicycle wayfinding map located at bicycle stations



Bicycle trail map with amenity guide and trail information



Mile marker and trail identification

Figure 29 Example Informational Wayfinding Signage

Directional signage is used to guide riders throughout the bike network. They are placed at route intersections to direct bikers to their destinations and to designate safe routes. They should include destinations, directional arrows, and distances. Examples of directional signage include are shown in **Figure 30**.



Posted signage with arrows and distance will notify riders of where to turn and how long of a ride it is to key locations



Bicycle Boulevard sign and identification



Pavement Markings give clear directions and help notify riders and drivers of the bike lane



Posted directional route arrows direct riders when to make turns

Figure 30 Example Directional Wayfinding Signage

A preliminary wayfinding signage plan for the proposed bike network is shown in **Figure 31**. All proposed wayfinding signage proposed along IDOT routes (Route 4/Jefferson Street, 6th Street, and Route 177/Main Street) must go through a permitting process through IDOT's Bureau of Operations. Note, there are limitations to size and messages allowed for wayfinding signage along IDOT routes.

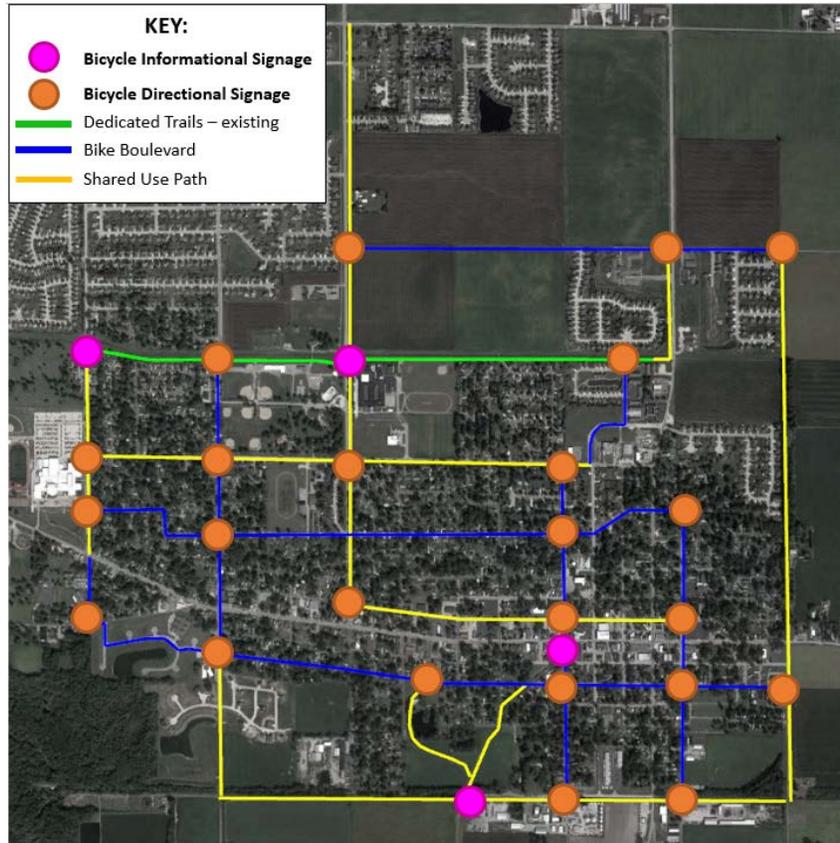


Figure 31 Preliminary Bike Wayfinding Signage Plan

Corridor Scale Recommendations

From community feedback, the most prominent transportation need for the project corridor is pedestrian focused, not vehicular. We found that the community desired a more walkable Main Street with a more well-defined sense of place. To accomplish these goals, the corridor scale recommendations for Route 177/Main Street and Church Street include pedestrian enhancements, parking relocation, and enhanced wayfinding signage. Proposed typical sections and a typical one-block plan view for each segment of the Route 177/Main Street and for the Church Street corridor are shown in **Appendix 1**.

Corridor Wide Pedestrian Treatments

There are a number of pedestrian enhancements to improve safety and walkability throughout the corridor. These include updated crosswalk marking and signage and bump-outs at corner locations. Currently, the crosswalks along Route 177/Main Street are the standard two parallel line markings and the paint is wearing down. Upgrading the crosswalks to continental crosswalks improves visibility and driver awareness of pedestrians. This can be a cost-effective way to enhance the pedestrian experience and safety through the corridor, see **Image 15** and **Image 16**. All crosswalks in the corridor will need to be maintained by the City, and per IDOT standards, crosswalks should be limited to one across Route 177/Main Street at each intersection.



Image 15 Existing Standard Crosswalk



Image 16 Proposed Continental Crosswalk

Enhanced signage is also a cost-effective way to improve pedestrian safety through the corridor. Any mid-block crossings across Route 177/Main Street within the project limits should have a standard pedestrian crossing sign, as shown in **Image 17**. It is also recommended to implement these at uncontrolled intersection approaches for busier intersections. Installing these at the eastbound and westbound approaches at the intersection of Route 177/Main Street and Railway Street is recommended. As previously discussed under “City Scale Recommendations”, the recommended signage for the shared use path crossing on Church Street across Route 4/Jefferson Street is shown in **Figure 32**.



Image 17 Standard Pedestrian Crossing Sign

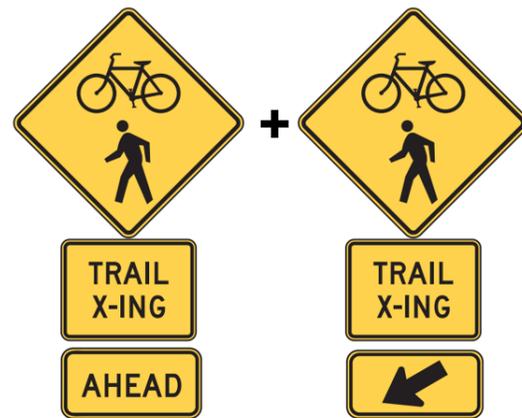


Figure 32 Shared Use Path Crossing Signage

Bump-outs are also recommended to enhance the pedestrian facilities through the corridor. Bump-outs help pedestrians by shortening the crossing distance and minimizing the time they are in the roadway. They also help increase visibility and driver awareness of pedestrians about to cross the road. Note, bump-outs should be designed to maintain the 30-ft minimum roadway width along Route 177/Main Street. These bump outs are recommended on the corners at the intersections of side-streets with Route 177/Main Street and would further enhance the experience and safety for pedestrians in the corridor. They will need to be designed to maintain sight lines and have appropriate curb radii for truck turning movements. See **Figure 33**.



Figure 33 Example of Bump-Out Shortening Crossing Distance

Also, all sidewalks in the corridor should be updated to ADA standards. This includes providing adequate curb ramps and clear width on the sidewalks in accordance with AASHTO’s Green Book and US Access Board’s Public Right-of-Way Accessibility Guidelines (PROWAG).

Route 177/Main Street Corridor Segments

The proposed plan for Route 177/Main Street is broken down into three segments based on the existing roadway characteristics of the corridor: west, central and east. These are shown in **Figure 34**.

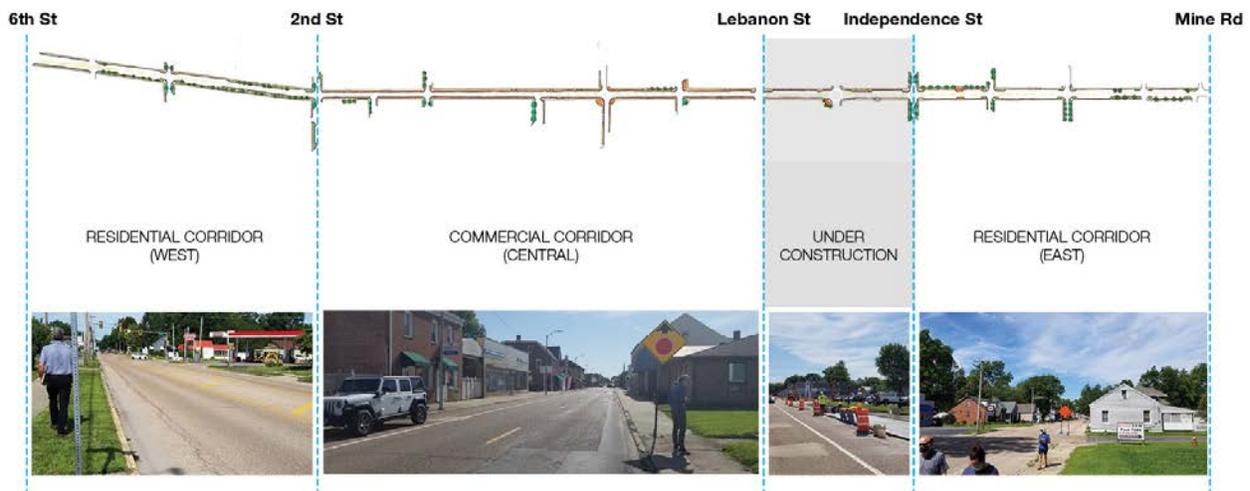


Figure 34 Route 177/Main Street Corridor Segments

As discussed previously, the section between Lebanon Street and Independence Street was recently reconstructed with new pavement, decorative lighting, and red brick stamped concrete. IDOT is in the beginning stages of the scoping process for the reconstruction of the rest of Route 177/Main Street from County Road to Mine Road (Contract #76L30). It is scoped for a letting date June of 2022. The timing of this Great Streets Plan with the upcoming reconstruction project provides the perfect opportunity to implements elements of the proposed plan in coordination with this project. The decorative elements recently implemented from Lebanon Street to Independence Street set a precedent for what the rest of the corridor could look like.

Route 177/Main Street West Segment

The proposed configuration for the West segment is to maintain the current lane configuration on Route 177/Main Street with street parking on both sides from 6th Street to 2nd Street, but to enhance the corridor with bump-outs at cross-street intersections. The proposed bump-outs will increase pedestrian crossing safety and allow more green space through the corridor. While the most popular option for the west segment was to remove street parking and widen the sidewalks from survey respondents, street parking was decided to be maintained in this portion to serve the local residences and to limit the impact to the parking supply on Route 177/Main Street with the removal of street parking in the Central Segment. The typical section is shown in **Figure 35**. See **Appendix 1** for a typical plan view (shown for the block between 4th Street and 2nd Street) in the West Segment.

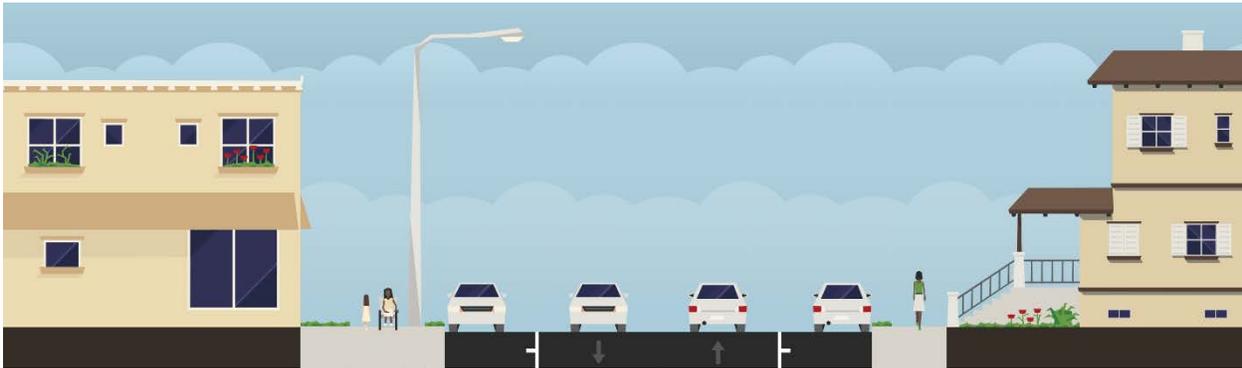


Figure 35 Route 177/Main Street West Segment Typical Section

Route 177/Main Street Central Segment

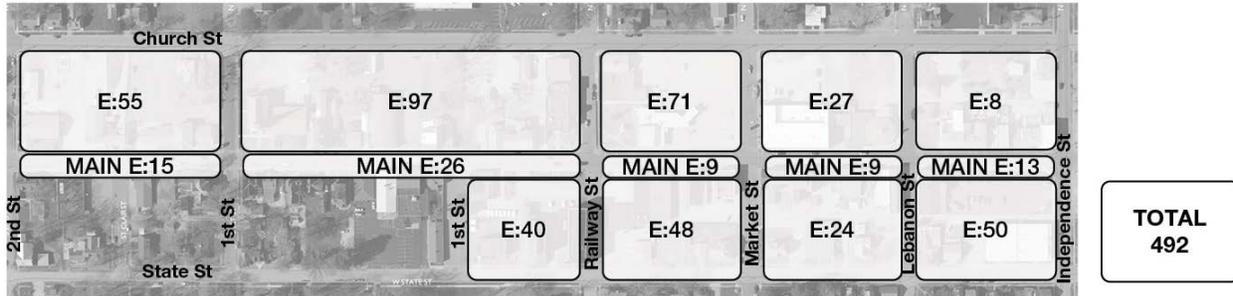
The proposed configuration for the Central segment includes moving the curb to provide wider sidewalks and to relocate the parking off Route 177/Main Street from 2nd Street to Lebanon Street to side-streets and back parking lots. Relocating the parking would provide room for 12-ft sidewalks on each street, which would allow for more street amenities including decorative lighting, wayfinding signage, planters, and benches. This proposed lane configuration was the most popular option from survey results. The typical section is shown in **Figure 36**. See **Appendix 1** for a typical plan view (shown for the block between 1st Street and Railway Street) in the Central Segment.



Figure 36 Route 177/Main Street Central Segment Typical Section

There is adequate parking on side-streets and in surface lots to accommodate the relocation of the approximately 75 spots lost on Route 177/Main Street. The existing parking lots can be reorganized in a more efficient manner to maximize the available space. With the reorganization of the existing surface lots and side-street parking, the total number of available parking spaces can be increased approximately from 492 to 557, see **Figure 37**. This would require coordination with local business owners but may become necessary as downtown Mascoutah attracts more visitors. In addition, there is ample parking supply along Church Street and State Street, just a block away from Route 177/Main Street.

EXISTING PARKING ALONG MAIN AND BETWEEN CHURCH AND STATE STREETS



PROPOSED PARKING ON MAIN BETWEEN CHURCH AND STATE STREETS



Figure 37 Parking Supply Optimization

It is important to provide safe connections from the parking supply to Route 177/Main Street. Sidewalks along the side-streets should be updated and maintained and a sidewalk along the south side of Church Street will be important to provide a safe connection for pedestrians from these back lots to Route 177/Main Street. Additional mid-block access points between the building along Route 177/Main Street are also recommended for a fully integrated downtown network. These spaces require adequate lighting and signage to enhance pedestrian experience and safety. An existing pedestrian pass-through on Route 177/Main Street is shown in **Image 18**. Wider pass-through can be enhanced with furnishings, planters, and lighting and converted to parklets, as shown in **Image 19**.



Image 18 Existing Pedestrian Pass-Through on Route 177/Main Street



Image 19 Example of Furnished Pedestrian Pass-Through (created by DTLS)

Route 177/Main Street East Segment

The proposed configuration for the West segment of Route 177/Main Street is similar to that of the East segment. The existing lane configuration will be maintained and enhanced with corner bump-outs at the side-street intersections. These bump-outs will provide safer pedestrian crossings and more green space for a more aesthetically pleasing entrance to downtown Mascoutah. While the most popular option for the west segment was to remove street parking and widen the sidewalks from survey respondents, street parking was decided to be maintained in this portion to serve the local residences and to limit the impact to the parking supply on Route 177/Main Street with the removal of street parking in the Central Segment. The typical section is shown in **Figure 38**. See **Appendix 1** for a typical plan view (shown for the block between Independence Street and Bernard Street) in the East Segment.



Figure 38 Route 177/Main Street East Segment Typical Section

Transitions

Roadways approaching Main Street should be designed to transition traffic to lower speeds and a down-town, multi-modal environment. Given the more urban character of the road as you approach the study corridor, treatments should be added as transition pieces to alert motorists to slow down, as well as to inform people (motorists, bicycles and pedestrians) they have arrived in Downtown. Transition elements include enhanced wayfinding, high visibility crosswalks, bump-outs, potential monument signage, decorative lighting, and branding that matches the feel of Downtown Mascoutah. These elements should be incorporated at the intersections on Main Street at 6th Street and Mine Road. The existing signal at 6th Street serves as a good entry point from the west side of the corridor to slow down vehicles through Downtown Mascoutah and provides a good pedestrian crossing point. Roadway

segments approaching the corridor should accommodate people walking and riding bicycles as well as motorized traffic.

Church Street Corridor

Enhanced pedestrian and bike facilities are recommended on Church Street. We recommend the implementation of a 10’ shared use path on the north side of the road and a 5’ sidewalk on the south side of the road. The shared use path is recommended on the north side since there is more available space and there are less driveways and curb cuts on the north side of the road, reducing the number of conflict points for bikes and pedestrians using the facility. In segments where there is street parking, there is little room for a green space buffer as shown in the image on the left. However, in areas where street parking cannot fit between driveways on Church, there will be a larger green space buffer between the roadway and the pedestrian facility as shown in the image on the right. These facilities will provide for a better connected and holistic transportation network in downtown Mascoutah. Typical sections and a typical plan view for Church Street are shown in **Appendix 1**.

Corridor Wayfinding Signage Plan

Implementing unique wayfinding signage along the corridor will help create a street identity and direct visitors and residents to parking and key sites. Strategically placing these signs throughout the corridor will welcome visitors at the entrance and direct drivers and pedestrian to shopping, restaurants, and other attractions. We recommend adding both pedestrian and vehicular scale signage along and around the Main Street corridor.

Vehicular scale signage along the Route 177/Main Street corridor should clearly guide visitors to parking and amenities, while helping build an appropriate character. **Figure 39** shows where we recommend vehicular scale signage be installed along the corridor and the type of signage that would be appropriate at these locations.



Figure 39 Vehicular Scale Corridor Wayfinding Signage Plan

We recommend adding “Welcome to Main Street” signs at corridor entry points, Parking directional signs at intersections, public parking signs in public lots, and adding banners to lamppost along the corridor. **Figure 40** shows an example for each type of proposed vehicular scale wayfinding signage.



Figure 40 Example Wayfinding Signage

Parking signs should be installed in public parking lots so that visitors have a clear indication on where they can park. Having parking directional signs installed at key intersections to direct visitors to those lots will make parking clear and simple. Banners on lamp post corridor wide can help to build a character for the street and can advertise events and businesses. Note, only 1/3 of the banner can be devoted to a business brand and the banners must adhere to the Outdoor Advertising guidelines for the state Route. Adding Entry Signs at the ends of the corridor will help a driver know they have arrived in downtown Main Street and can also inform them of what direction amenities are located.

Pedestrian scaled signage is recommended to direct visitors after they've arrived in Downtown Mascoutah. They also help to build upon the corridor's character and branding. They are utilized to show what amenities exist along the corridor and where they are located. This includes maps and directional signage. Pedestrian Scale signage is smaller in design to allow for adequate usage by all users. **Figure 41** shows recommended pedestrian scale wayfinding signage along the corridor.

Sign Locations

- Small-Scale Directional Sign
- Wayfinding Map Kiosk

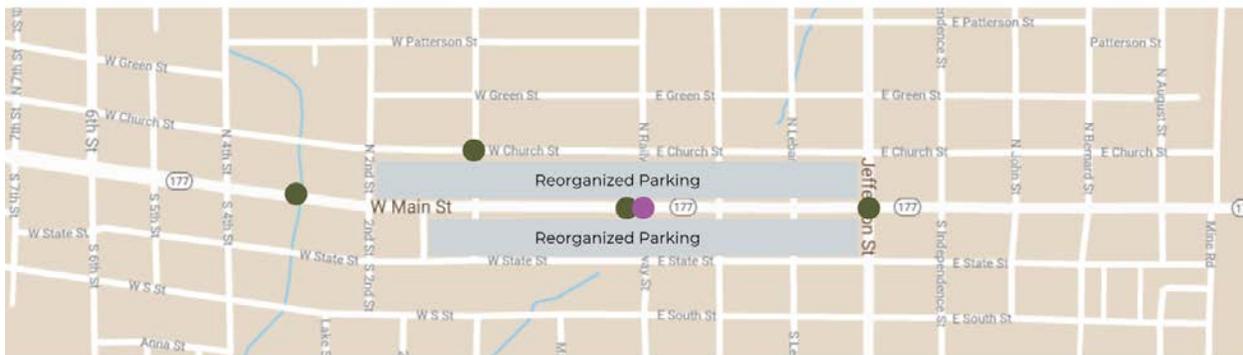


Figure 41 Pedestrian Scale Corridor Wayfinding Signage Plan

Pedestrian directional signage is recommended at major intersections, like the intersections at Route 4/Jefferson Street and at Railway Street to direct pedestrians to key destinations along the corridor including the History Museum and popular businesses. A map kiosk is recommended at the intersection of Route 177/Main Street with Railway Street as it is the heart of the corridor. The map would help

pedestrians identify how to reach important destinations and quickly see what all downtown Mascoutah has to offer. Example pedestrian scale wayfinding signage is shown in **Figure 42**.



Destination maps show city wide amenities and routes to get there



Directional signs at main crossroads will direct pedestrians to their destinations. They can also show walking time.

Figure 42 Example Pedestrian Scale Wayfinding Signage

Key Site Recommendations

There are a few key sites which will require some more detailed design and consideration. These include the intersection of Route 177/Main Street with Railway Street, the midblock crossing of the proposed Pig Creek corridor, and the intersection of Route 4/Jefferson Street with Onyx Drive.

Route 177/Main Street & Railway Street Intersection

The intersection of Route 177/Main Street and Railway Street is the heart of downtown Mascoutah. It is the location of City Hall, the gazebo park (Haas Park), and the historic mural building. As discussed in the urban design white paper, there are multiple urban design elements proposed for this intersection including an outdoor stage area at the Old Mill site, water features, and prominent green space. The proposed transportation elements at this intersection include continental crosswalks and standard pedestrian crossing signs as Route 177/Main Street is uncontrolled at this intersection and heavy pedestrian traffic is anticipated at this intersection. Also, aesthetically pleasing pedestrian paths are shown to mimic the existing path through the Gazebo park. See **Figure 43**.



Figure 43 Route 177/Main Street and Railway Street Rendering (created by RDG)

Pig Creek Corridor Crossing over Route 177/Main Street

A historical walking path along Pig Creek is proposed to connect to the existing Heritage Museum to highlight Mascoutah’s history in a new way. This is discussed further in the Urban Design White Paper; however, the transportation component is important to consider in the design of this corridor. This path is intended for pedestrians only and is not meant for bikes as a shared use path. The crossing over Route

177/Main Street should be well-designed to provide a safe crossing for pedestrians, is consistent with IDOT policy, and still aesthetically consistent with the rest of the corridor if possible. A standard pedestrian crossing sign with a warning sign is recommended for this mid-block crossing as well as pedestrian scale wayfinding signage to direct users along the path across Route 177/Main Street. A rendering created by RDG of what the Pig Creek Walking Corridor could look like is shown in **Figure 44**.



Figure 44 Pig Creek Historical Walking Path Rendering (created by RDG)

Route 4/Jefferson Street & Onyx Drive

The intersection of Route 4/Jefferson Street and Onyx Drive is important as it is the main entry point into town. Most visitors are coming from the I-64 & Route 4 interchange to the north and this intersection has some of the first developments you see when driving into town, see **Image 20**. Just north of this intersection, the speed limit changes from 55 mph to 35 mph, signifying you are entering the more developed part of Mascoutah. The city of Mascoutah is seeing rapid growth, particularly to the north due to the large draw from Scott Air Force Base, and there is likely to be new developments off Onyx Drive if the current growth trends continue. If development occurs, the intersection control will likely need to be updated to handle new traffic volumes.

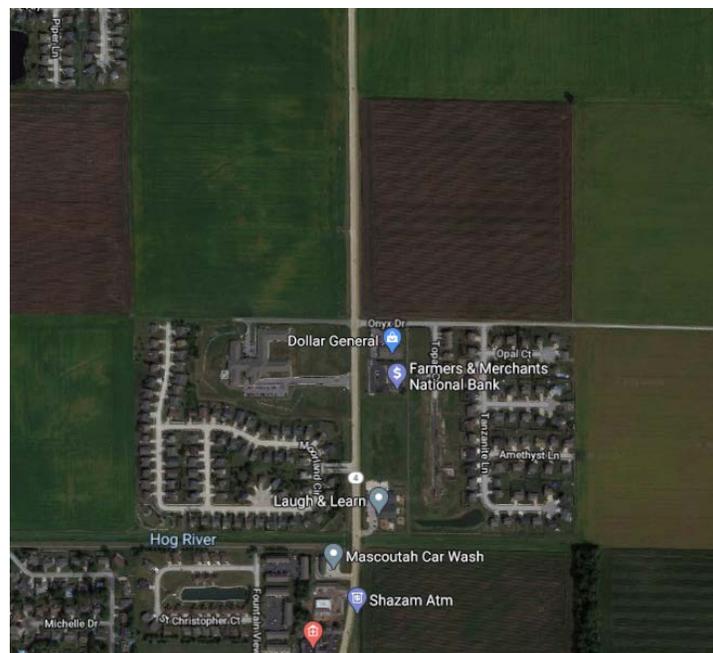


Image 20 Route 4/Jefferson Street and Onyx Drive Intersection



Currently, the intersection is side-street stop controlled. A roundabout or signal will likely be needed in the future. Traffic control at this intersection would serve multiple purposes, aside from providing better access for any future developments. It also would be beneficial to the City of Mascoutah to help slow down vehicles as they arrive in town and can provide a better pedestrian and bike crossing over Route 4/Jefferson Street. Since the speed limit changes only 550-ft north of this intersection and there is currently no physical changes to the roadway at this location to force vehicles to slow down, it is difficult to ensure the vehicles are travelling at a desired speed for an uncontrolled trail crossing. A study would need to be completed in coordination with IDOT to determine the best design for this intersection.

We have developed preliminary drawings of a roundabout design and a signal at this intersection. See **Exhibits 1** and **2**. The roundabout concept should accommodate the crossing of bikes and pedestrians and properly designed to transition bikes between the proposed on-street bike boulevard on Onyx Drive and the shared use path on the west side of Route 4/Jefferson Street. The drawing shows one way of doing this with the use of ramps through the curb section, however, this should be optimized in the final design. A signal option could only be constructed once traffic volumes grow enough to meet warrants.

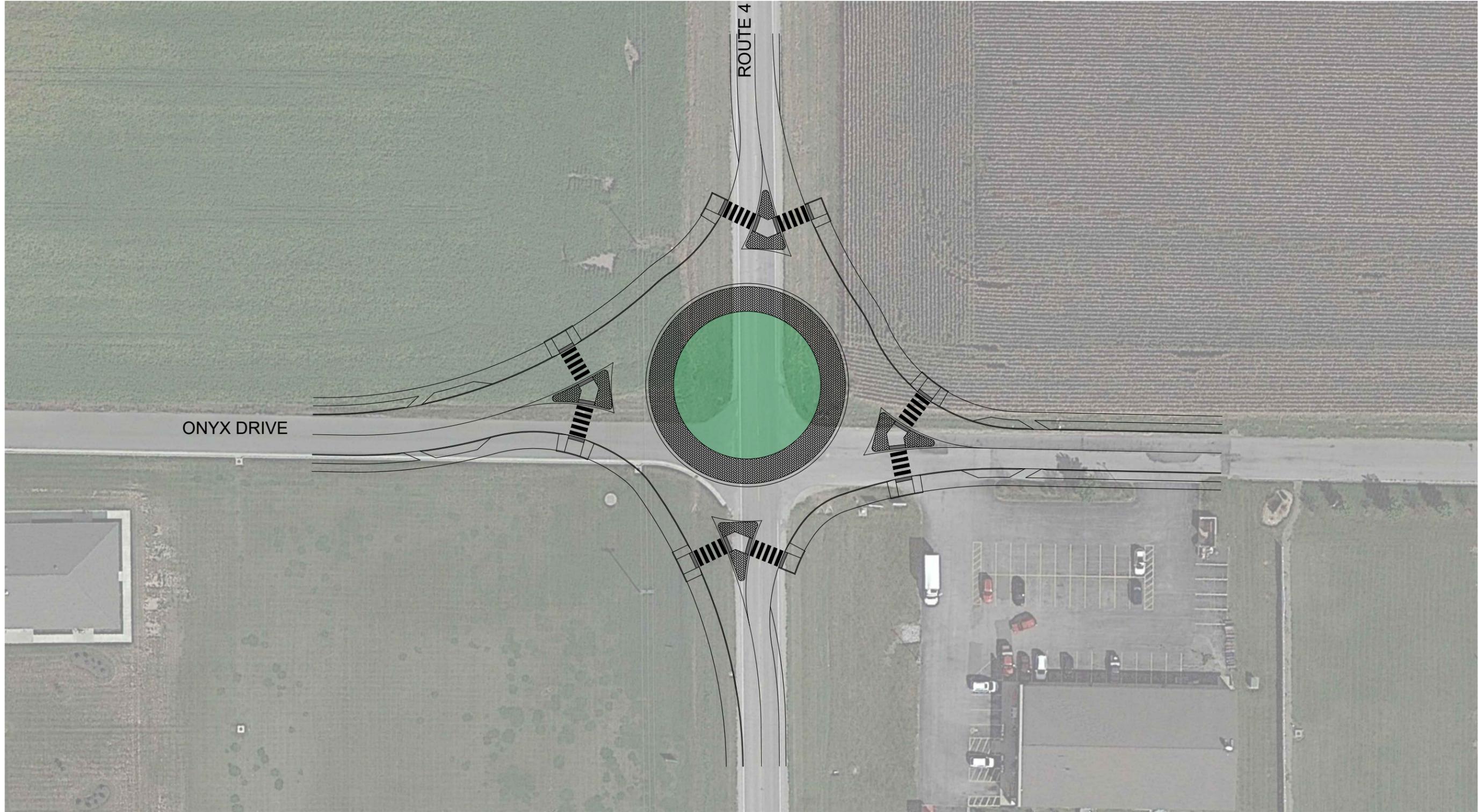


EXHIBIT 1: Route 4 and Onyx Drive Roundabout



EXHIBIT 2: Route 4 and Onyx Drive Signalized



IMPLEMENTATION AND PARTNERSHIPS

The ultimate success of great plans is for them to become reality. Identifying a phasing plan, next steps, and key partnerships will help the City of Mascoutah in pursuing these efforts. High-level planning cost estimates are also provided to help the City start programming items in their budget and identify required funding. Lastly, potential funding sources are discussed to help identify opportunities for financial assistance.

Project Phasing

The transportation elements of the proposed Great Streets plan are anticipated to occur in the following phases, based on feasibility, partnerships, and need:

1. Immediate Term (0 – 5 years)
 - a. Bike Boulevard Initial Loop
 - i. *Since minimal infrastructure is needed for bike boulevards, an initial loop is recommended for immediate implementation to extend the existing bike network*
 - b. Route 177/Main Street Improvements
 - i. *An IDOT resurfacing project for this route is scoped for 2022 – 2026 and any improvements on Route 177/Main Street desired by the City of Mascoutah should be coordinated with this project*
 - c. Downtown Parking Reconfiguration
 - i. *An agreement with the local business owners should be completed prior to the completion of the Route 177/Main Street reconstruction parking to ensure there is adequate parking for downtown Mascoutah*
2. Near Term (5 – 10 years)
 - a. City-Scale Bike and Pedestrian Network with Wayfinding Plan
 - i. *It will take some time to acquire and allocate funding for the proposed bike/ped network, however, most paths are within city ROW and should require minimal coordination. Paths on IDOT routes or that require ROW takings will generally take longer to implement*
 - ii. *The City scale bike and pedestrian network should include enhanced crossings at streets, bike and pedestrian scale lighting, and stormwater/drainage features*
 - b. Intersection Design Study for Route 4/Jefferson Street and Onyx Drive
 - i. *The design for this intersection should be studied in coordination with IDOT in the near term to plan for future development needs and identify a safe bike and pedestrian crossing*
3. Long Term (10 – 20 years)
 - a. Regional Bike and Pedestrian Network
 - i. *This network will require multiple studies, partnerships, and a large budget and will likely take years to implement*



Programs, Costs, and Partners

The steps to implement elements of this plan depend on the design elements, required funding, and necessary partners. The process for implementing the transportation elements of the Great Streets plan are outlined for the implementation of shared use paths, bike boulevards, and wayfinding.

Bike Boulevards

Bike Boulevard Program should be developed by the City of Mascoutah. This program should be created in order to acquire the necessary funding, complete design studies, implement the design, and maintain the boulevards. Projects for the bike boulevard program will be routed through the Parks and Recreation Commission and public engagement done through City Council. This process will ensure projects are completed in order of need, as well as continue to get feedback through a public engagement process. Projects will be prioritized for funding programs as money is available.

Since all bike networks are proposed on City-owned routes, no outside partners are necessary. Projects will include route alignment finalization, professional engineering, and construction. Approximate costs for engineering and construction are included below.

Key Partners: None

Approximate Cost: \$55 k per mile for signing and pavement marking, additional \$355 k per mile if roadway need to be milled and overlaid, \$415 k per mile for curb and gutter if needed, and \$30 k per mile for inlet adjustments if needed

Potential Sequencing:

- *Develop Program*
- *Conduct engineering studies to identify preferred route network and phasing*
- *Community Engagement*
- *Develop engineered drawings and cost estimates*
- *Acquire funding*
- *Construct Bike Boulevards*
- *Maintain Bike Boulevards*

Local Shared Use Path Network

A Shared Use Path Program should be developed by the City of Mascoutah. This program should be created in order to acquire the necessary funding, complete design studies, construct the trails, and maintain the trails. This program would be similar to other programs the City has such as plans for Scheve Park and Prairie Lake Park. Projects for the shared use path system will be routed through the Parks and Recreation Commission and public engagement done through City Council. This process will ensure projects are completed in order of need, as well as continue to get feedback through a public engagement process. Projects will be prioritized for funding programs as money is available.

Projects will include route alignment finalization, professional engineering, and construction. This network requires more than just one design study, it will require continual long-term maintenance of the trails and wayfinding signage. Any paths along state routes (Route 4/Jefferson Street and 6th Street) will need to be coordinated with IDOT. Approximate costs for engineering and construction are included below. It should be noted that these per mile costs do not include any land acquisition for potential ROW purchasing or utility relocation. That cost will be estimated during the professional engineering phase.

Key Partners: IDOT, Adjacent Landowners



Approximate Cost: \$1 million per mile

Potential Sequencing:

- *Develop Program*
- *Conduct engineering studies to identify preferred route network and phasing*
- *Community Engagement*
- *Develop engineered drawings and cost estimates*
- *Acquire funding*
- *Acquire necessary ROW*
- *Construct Shared Use Paths*
- *Maintain Shared Use Paths*

Wayfinding Signage

A Wayfinding Signage Program is recommended to be developed by the City of Mascoutah. This program should be created in order to acquire the necessary funding, complete design studies, implement the design, and maintain the signage. This will require continual long-term maintenance of the signs to ensure they stay up-to-date and in good condition. Projects for the wayfinding signage program in downtown Mascoutah will be led by the City but will also involve the Economic Development Commission. Local business owners can be potential funding partners to include their branding on certain signs.

Any wayfinding signage on state routes will need to go through IDOT's permitting process. For the recommended "brown" attraction signage off I-64, there are certain conditions that must be met for IDOT to implement such signage. For example, according to IDOT's SIGNING TO TRAFFIC GENERATORS AND MOTORIST SERVICES POLICY, a minimum, annual number of visitors of approximately 300,000 to qualify for additional signage off I-64. If this is met and the City of Mascoutah is able to provide valid documentation to support this claim, IDOT will consider the request. If approved, the City of Mascoutah would not need to fund this signage. Otherwise, the City of Mascoutah could consider purchasing billboard advertisement off I-64 to attract visitors. See the Freeway Generator Policy in **Appendix 3**. Approximate costs for engineering and construction are included below.

Key Partners: IDOT, Local Business Owners

Approximate Cost: \$500 - \$2,500 per sign, depending on size, post, and design of sign panel (standard "off the shelf" road signs with galvanized post will be much cheaper than decorative trail signs on fluted posts)

Potential Sequencing:

- *Develop Program*
- *Conduct engineering studies to identify preferred signage and locations*
- *Community Engagement*
- *Develop engineered drawings and cost estimates*
- *Acquire funding*
- *Implement Signage*
- *Maintain Signage*

Route 177/Main Street Improvements

An IDOT resurfacing project for this route is scoped for 2022 – 2026 and any improvements on Route 177/Main Street desired by the City of Mascoutah should be coordinated with this project. Enhanced crosswalks, removing parking for wider sidewalks (Central segment per parking recommendations), enhanced lighting, drainage improvements, sidewalk and pavement updates, pedestrian connections to



backlot parking (per parking recommendations) and the proposed Main Street reconfiguration, enhanced wayfinding (bike scale, pedestrian scale, and motorist scale) should be considered in this project. If desired, the City will need to work with IDOT to secure a design exception for narrow lanes for the future reconstruction of Main Street between N County Road and Lebanon Street and between Independence Street and Mine Road in order to maintain the same lane widths as those recently constructed on Main Street between Lebanon Street and Independence Street. The City will need to work with IDOT to incorporate design features such as stamped concrete, wayfinding signing, and decorative light fixtures for the future reconstruction of Main Street between N County Road and Lebanon Street and between Independence Street and Mine Road as were recently incorporated in construction on Main Street between Lebanon Street and Independence Street. Incorporation of these elements may include IDOT/City cost-share and/or maintenance agreements. Any special features will need to adhere to current IDOT Statutes and Department policies.

Key Partners: IDOT, Local Business Owners

Downtown Parking Reconfiguration

The removal of on-street parking in the Central Segment is recommended for the addition of wider sidewalks. The back-lot parking (Church and State Streets) should be consolidated to encourage Main Street business parking in these lots. The City should work with private landowners to develop a public-private shared parking agreement. The terms of this agreement will need to be finalized but should include the following items:

1. Cost associated with parking consolidation (will landowners charge a fee for public use?)
2. Length of agreement
3. Any restrictions on timeframes for public parking
4. Other considerations important to the City or private landowners

Various guidance exists on these types of agreements, but one such resource is published by Nelson Nygaard ([Maximizing Urban-Core Parking with Private-Public and Private-Private Parking Agreements](#))

Key Partners: IDOT, Local Business Owners

Regional Shared Use Path Network

Regional shared use paths outside of city limits will need to be coordinated with several entities, including St. Clair County Transit, IDOT, and potentially Metro East Park and Recreation District. The first step in achieving a connection to St. Clair County's MetroBikeLink network is to submit a letter of interest to their board of directors outlining potential trail paths and funding options. If the board agrees to incorporate a connection to Mascoutah in their network, then they will create a Memorandum of Understanding (MOU) Agreement with the City of Mascoutah and develop a plan for the planning, engineering, and construction of the new trail as well as potential cost-sharing and maintenance agreements between entities. Any trail options that are along state routes will also need to be coordinated with IDOT. Approximate costs for engineering and construction are included below. It should be noted that these per mile costs do not include any land acquisition for potential ROW purchasing or utility relocation. That cost will be estimated during the professional engineering phase.

Key Partners: IDOT, SCCTD, MEPD, Adjacent Landowners

Approximate Cost: \$1 million per mile



Potential Sequencing:

- *Submit letter to SCCTD Board*
- *Enter into MOU Agreement*
- *Develop Plan*
- *Conduct engineering studies to identify preferred trail location and route*
- *Community Engagement*
- *Develop engineered drawings and cost estimate*
- *Acquire funding*
- *Acquire necessary ROW*
- *Construct Route*
- *Maintain Route (depending on agreements made with SCCTD)*

Potential Funding Sources

The City of Mascoutah should look for every opportunity to implement these projects by finding federal funding available for assistance with professional engineering, implementation and construction of both the larger and smaller projects recommended. A few funding ideas related to transportation are included here. Mascoutah will also need to continue to champion the ideas found in this plan to build political will for setting aside match money for any potential grant opportunities. It should be noted this is not an exhaustive list, rather a starting point, and more ideas should be considered.

- **CMAQ**
Congestion Mitigation Air Quality Grants are given in non-attainment regions. Annually, East West Gateway Solicits transportation projects that help improve air quality, on a competitive application basis. More information is provided at: <https://www.ewgateway.org/transportation-planning/transportation-improvement-program/competitive-transportation-programs/>
- **STP-S**
Surface Transportation Program – Suballocated grants are for projects that preserve and improve the conditions and performance on any federal aid route. Annually, East West Gateway solicits transportation projects that match certain criteria on a competitive application basis. More information is provided at: <https://www.ewgateway.org/transportation-planning/transportation-improvement-program/competitive-transportation-programs/>
- **BUILD**
Better Utilizing Investments to Leverage Development Grants are the new TIGER (transportation investments generating economic recovery) grants. These are for investments in surface transportation infrastructure projects that will have a significant local or regional impact. More information is provided at: <https://www.transportation.gov/BUILDgrants>
- **TAP**
Transportation Alternative Program grants are eligible for projects promoting transportation alternatives, such as walking and biking. This is also solicited by East West Gateway on a competitive application basis. More information is provided at: <https://www.ewgateway.org/transportation-planning/transportation-improvement-program/competitive-transportation-programs/tap/>
- **ITEP**
Illinois Transportation Enhancement Program grants provide additional state funding from IDOT for projects for bike and pedestrian facilities, including converting abandoned railroad corridors



to trails. More information is provided at: <http://www.idot.illinois.gov/transportation-system/local-transportation-partners/county-engineers-and-local-public-agencies/funding-opportunities/ITEP>

- **Others**

Additionally, more funding opportunities may be available through health, pedestrian, biking or other related agencies. The City should identify priority projects and continue to search for ways of implementation. IDOT and the City of Mascoutah are also good funding sources.

Action Steps

In order to start the process of implementing this Great Streets plan, the following first steps are recommended to be taken in the immediate future:

1. **Develop Programs:** Programs should be created for the implementation and maintenance of shared use paths, bike boulevards, and wayfinding signage through the town of Mascoutah to set a plan for funding allocation, construction, and maintenance of the new facilities
 - a. The bike boulevard program will involve a final network plan, as well as professional engineering and construction for project implementation. This route plan will be routed through the Parks and Recreation commission, and estimated costs per mile is included in the programs and partners section (previously).
 - b. The shared use path program will involve prioritization of future projects by routing plans through the Parks and Recreation Commission. Each of these projects will include a future alignment study, professional engineering and design, as well as construction. Associated costs with these projects is included in the programs and partners section (previously).
2. **Begin IDOT Route 177/Main Street Project Partnership:** IDOT has already been involved in this Great Streets project and is aware the City is interested in implementing aspects of this plan with their resurfacing project (Contract #76L30). The City should begin allocating funding for the costs associated with the plan.
3. **Begin Engagement with Local Business Owners regarding Downtown Parking Lot Reconfiguration:** The City should begin speaking with local business owners about their interests in the downtown parking consolidation and reconfiguration. The City should begin drafting an agreement for this plan.
4. **Begin Seeking Grants:** The City of Mascoutah should begin seeking funding assistance to implement the desired changes for Mascoutah, in particular, the initial bike boulevard loop through the City
5. **Begin St. Clair County Transit District Partnership:** This involves submitting a letter to the board requesting a regional connection from the City of Mascoutah to their MetroBikeLink network, with potential routes and funding options. From there, the board will consider the request and, if approved, will enter into a Memorandum of Understanding agreement with the City and set a plan.

APPENDICES

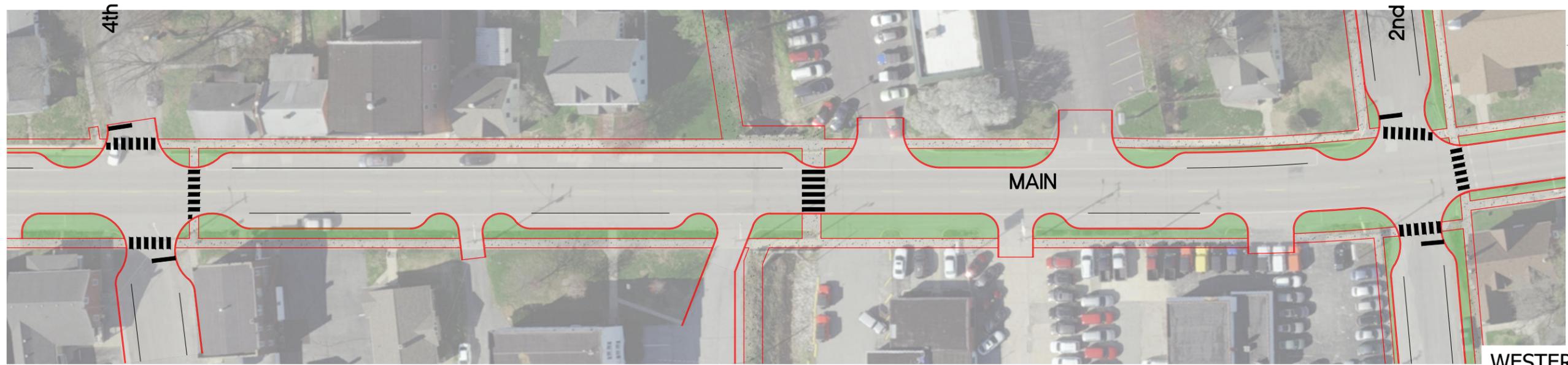
Appendix 1 – Proposed Route 177/Main Street and Church Street Typical Sections and Plan Views

Appendix 2 – Transportation Related Survey Data

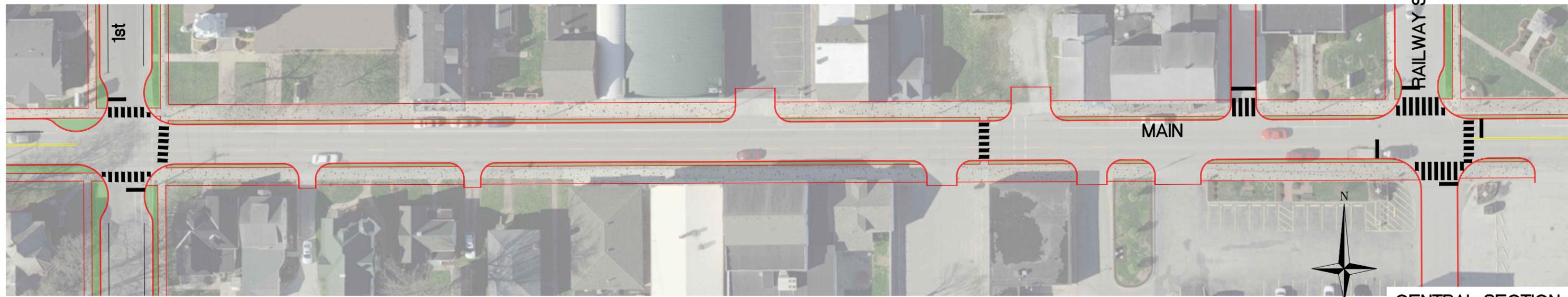
Appendix 3 – Freeway Generator Signs Policy

Appendix 1

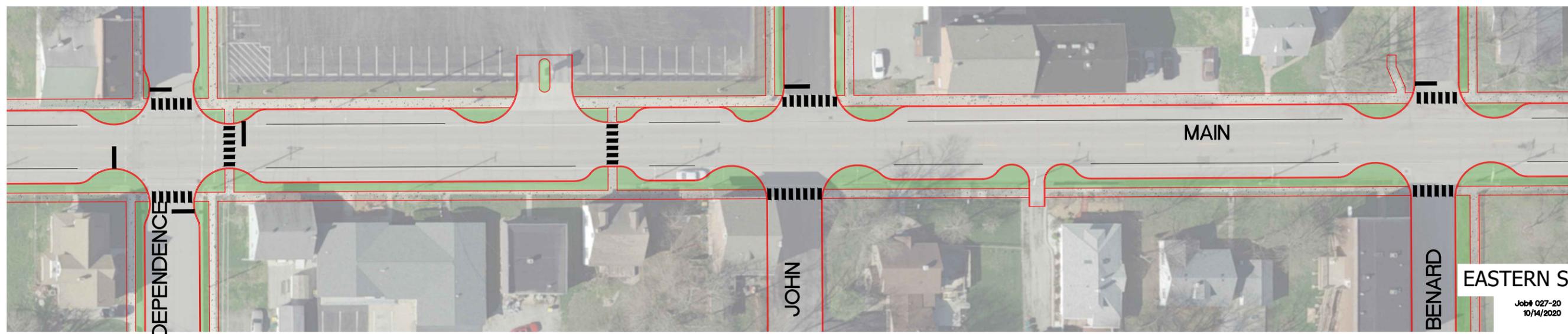
Proposed Route 177/Main Street and Church Street Typical
Sections and Plan Views



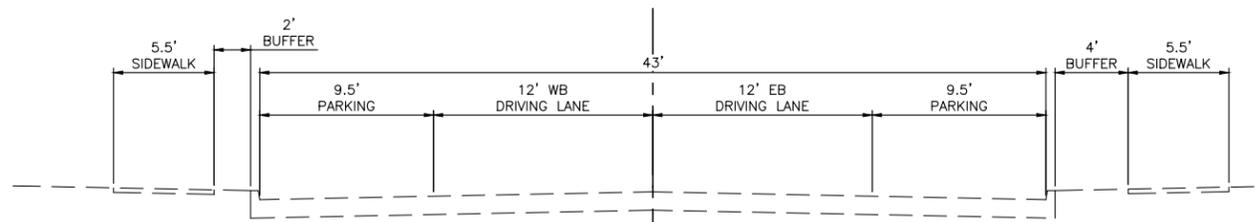
WESTERN SECTION



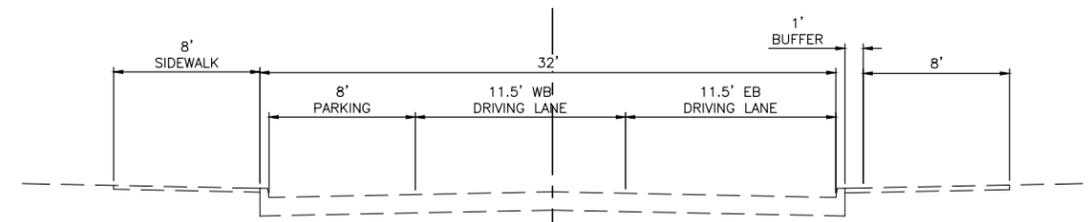
CENTRAL SECTION



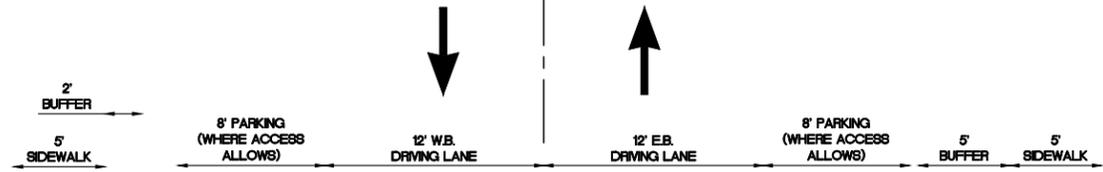
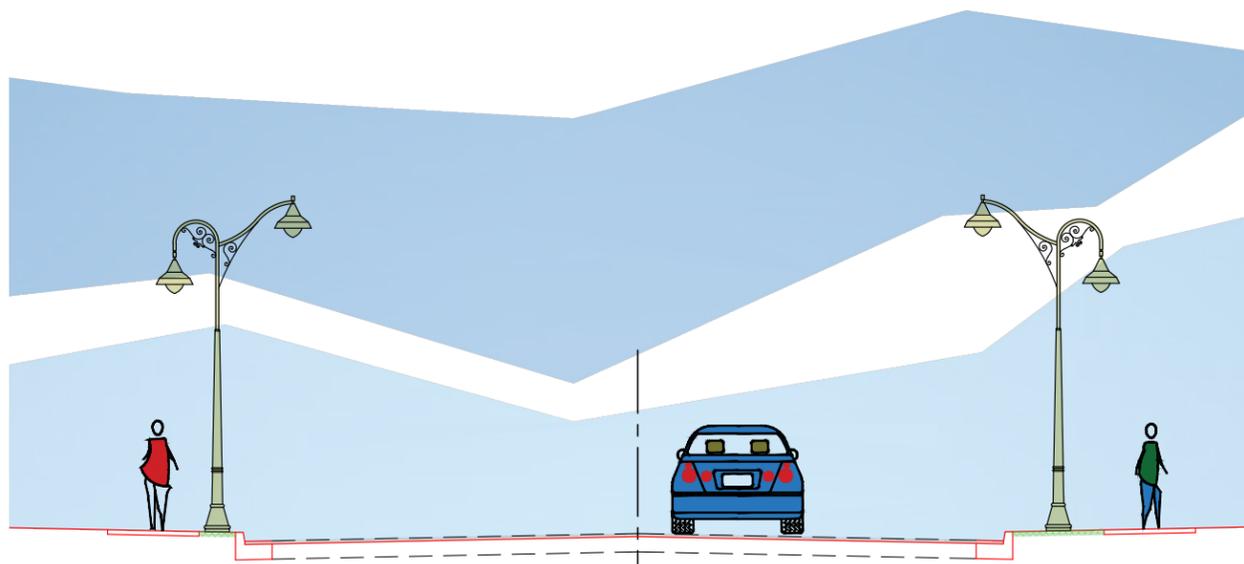
EASTERN SECTION



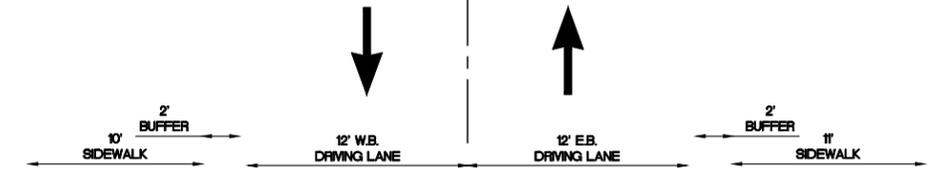
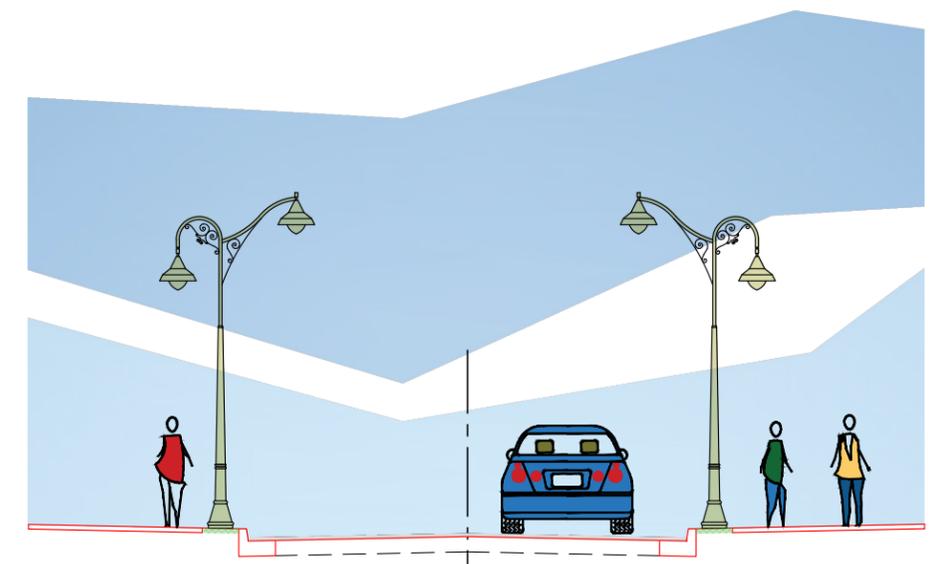
TYPICAL SECTION
E. MAIN STREET (IL 177)
WEST OF 2nd STREET
(LOOKING EAST)
EXISTING



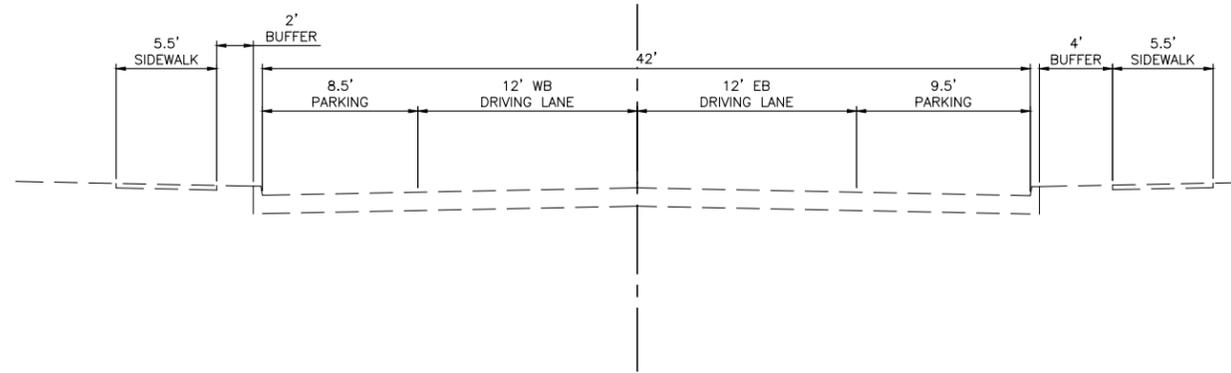
TYPICAL SECTION
E. MAIN STREET (IL 177)
BETWEEN 2nd STREET
AND JEFFERSON ST (IL 4)
(LOOKING EAST)
EXISTING



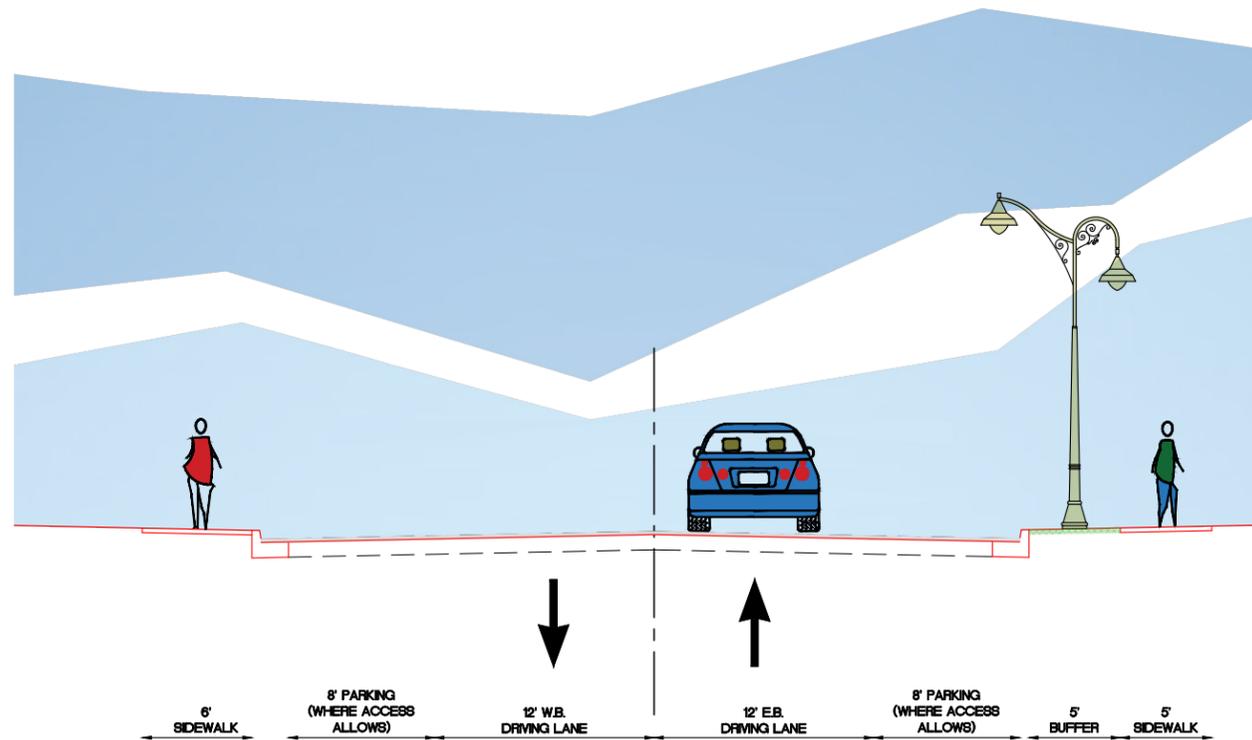
WESTERN TYPICAL SECTION
E. MAIN STREET (IL 177)
WEST OF 2nd STREET
(LOOKING EAST)



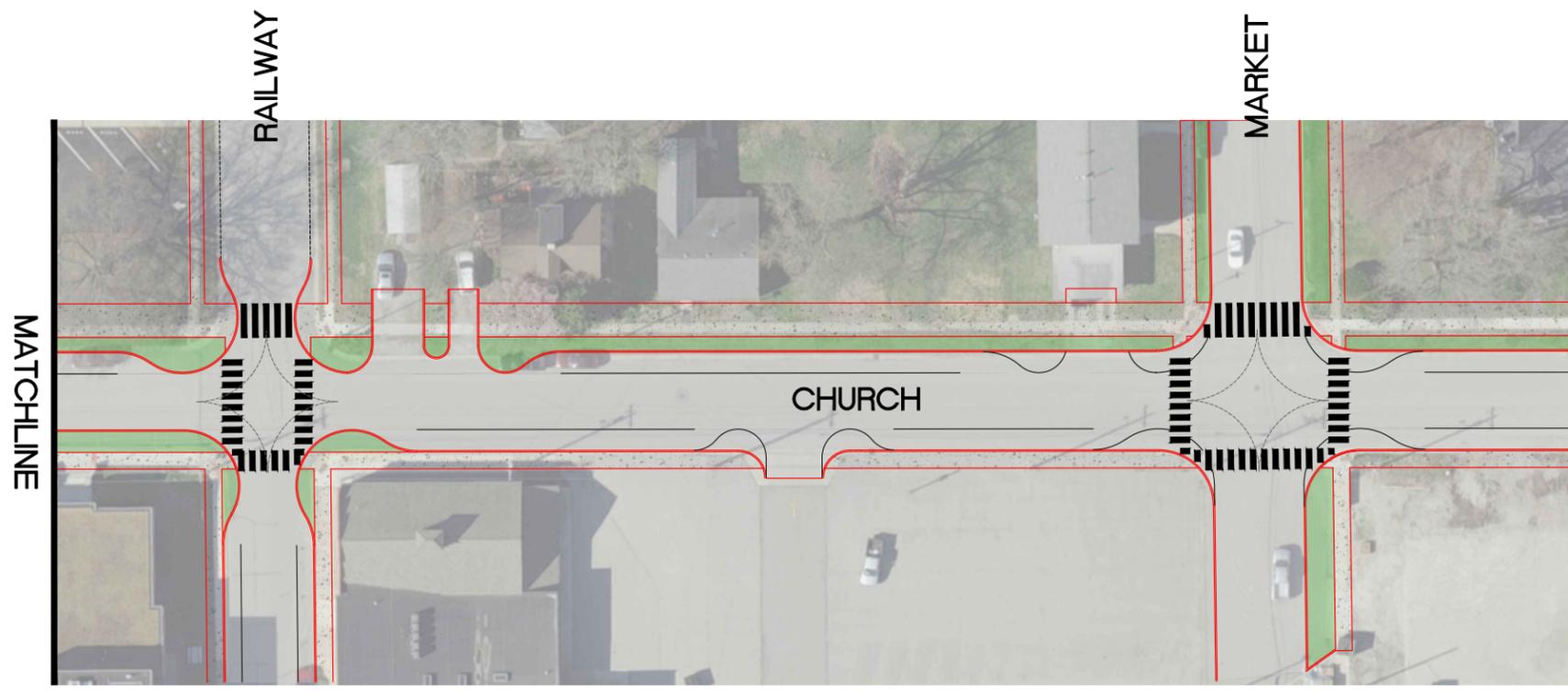
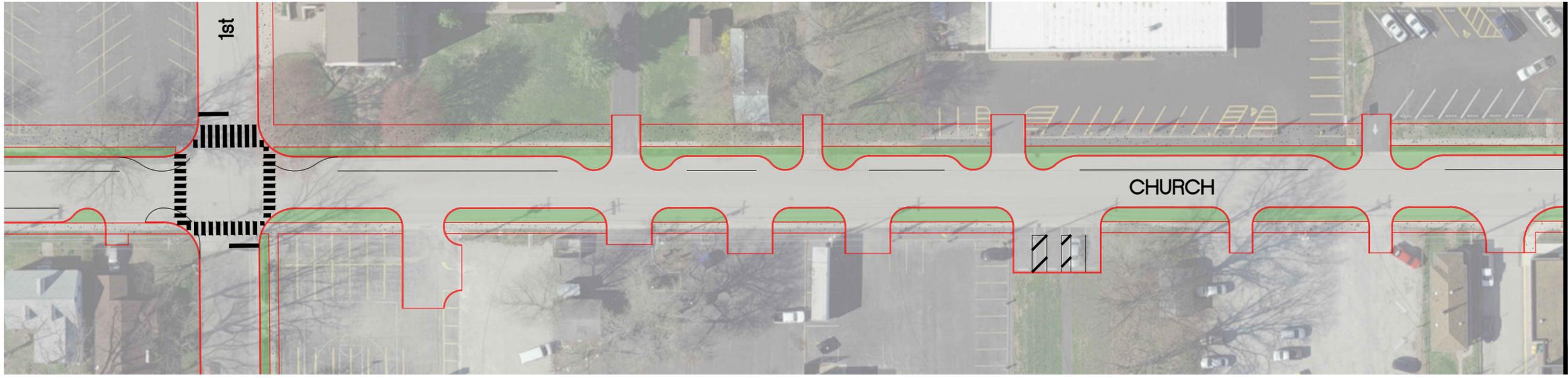
CENTRAL TYPICAL SECTION
E. MAIN STREET (IL 177)
BETWEEN 2nd STREET
AND JEFFERSON ST (IL 4)
(LOOKING EAST)

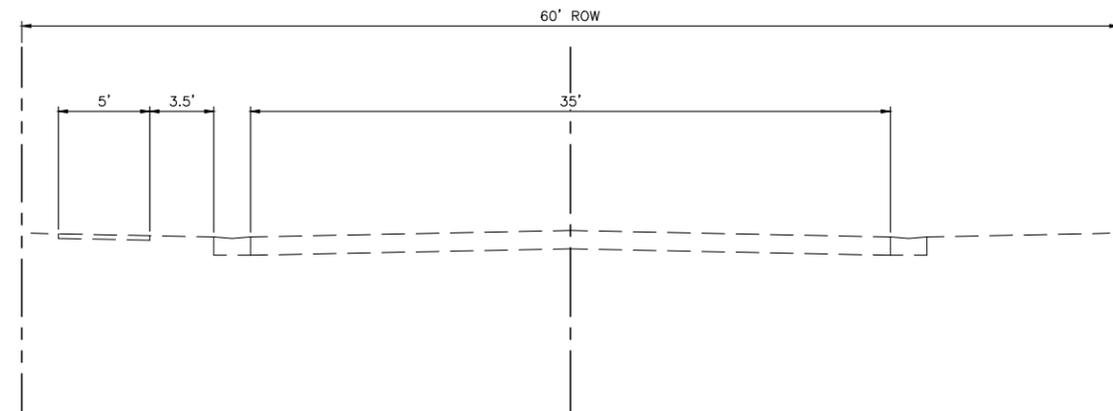


TYPICAL SECTION
 E. MAIN STREET (IL 177)
 EAST OF JEFFERSON ST (IL 4)
 (LOOKING EAST)
 EXISTING

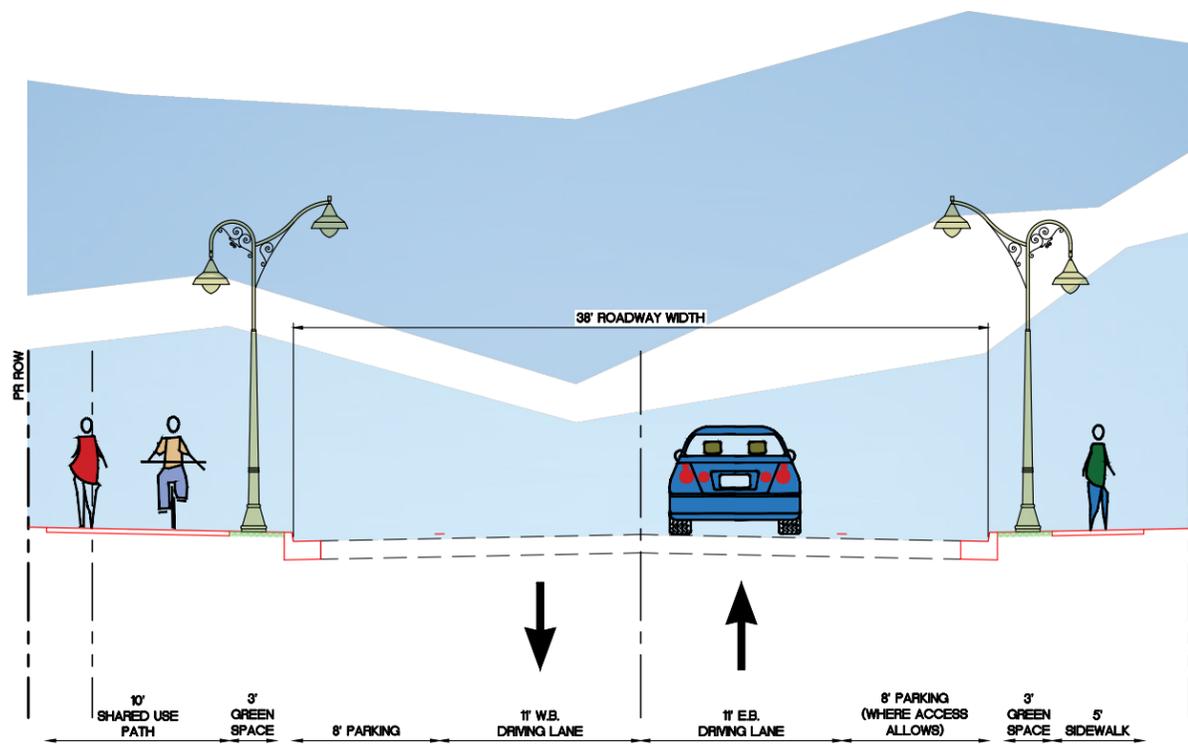


EASTERN TYPICAL SECTION
 E. MAIN STREET (IL 177)
 EAST OF JEFFERSON ST (IL 4)
 (LOOKING EAST)

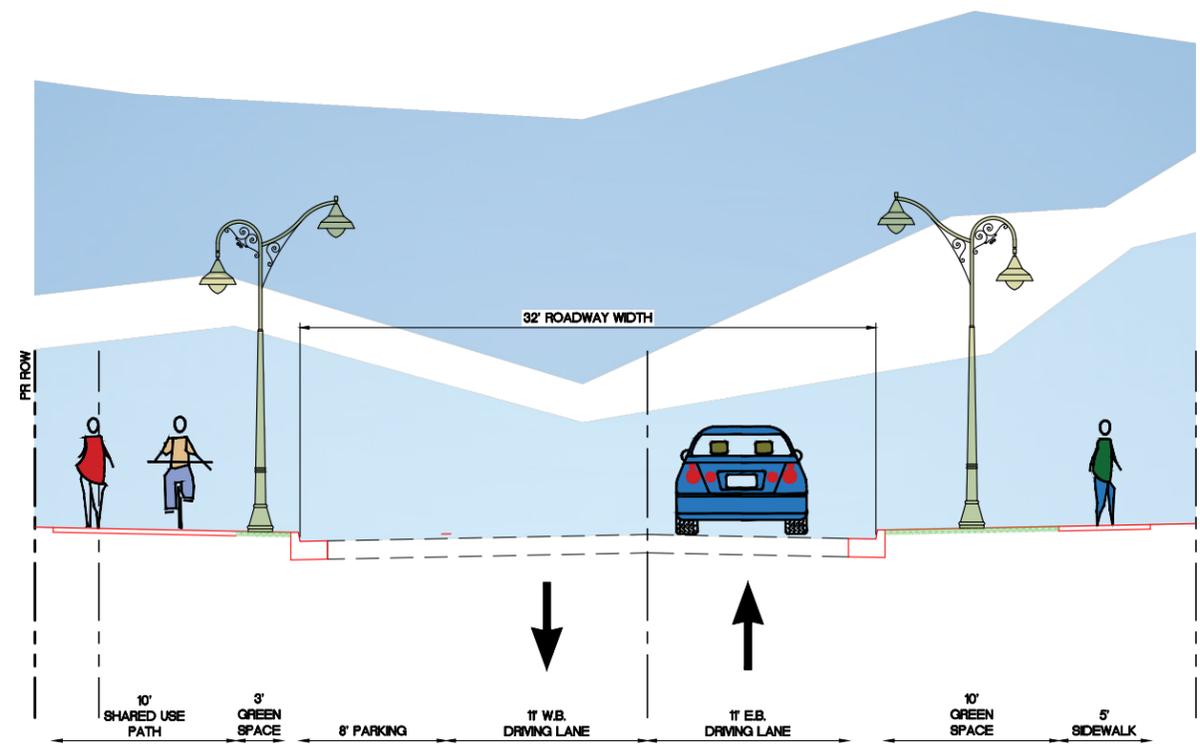




TYPICAL SECTION
CHURCH STREET
(LOOKING EAST)
EXISTING



TYPICAL SECTION
CHURCH STREET
(LOOKING EAST)
PARKING WHERE ACCESS ALLOWS



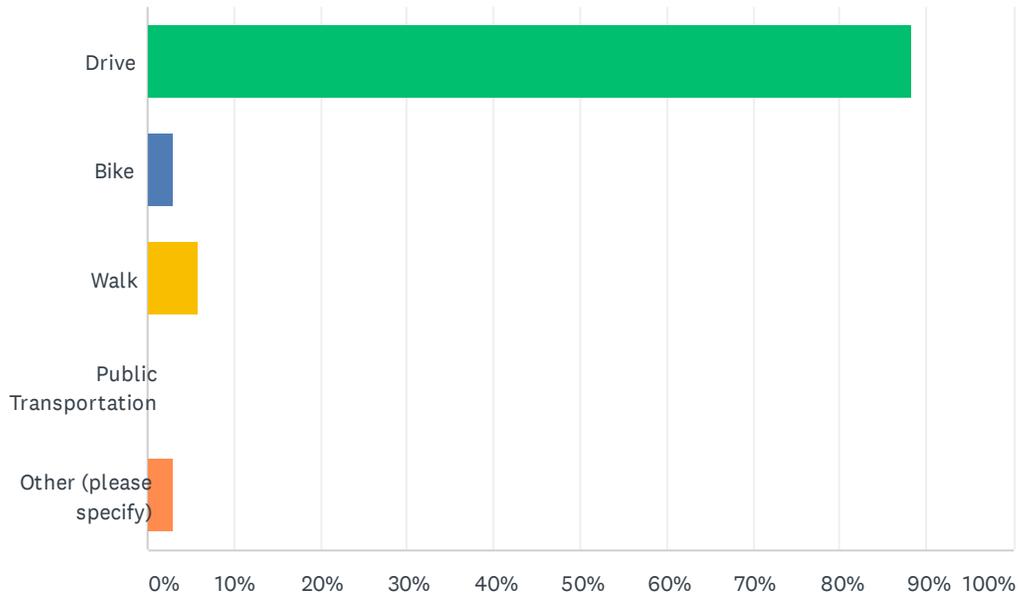
TYPICAL SECTION
CHURCH STREET
(LOOKING EAST)
NO PARKING DUE TO ACCESS

Appendix 2

Transportation Related Survey Data

Q1 What is the primary way you get around Mascoutah?

Answered: 34 Skipped: 0

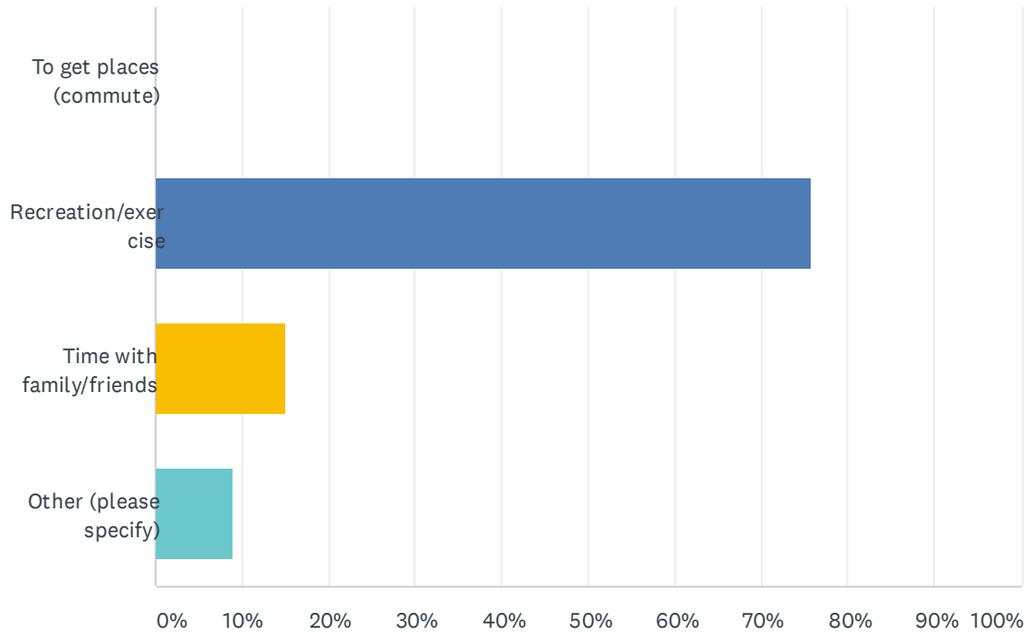


| ANSWER CHOICES | RESPONSES | |
|------------------------|-----------|-----------|
| Drive | 88.24% | 30 |
| Bike | 2.94% | 1 |
| Walk | 5.88% | 2 |
| Public Transportation | 0.00% | 0 |
| Other (please specify) | 2.94% | 1 |
| TOTAL | | 34 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|---------------------------|--------------------|
| 1 | Bike and Driving equally. | 8/16/2020 11:54 PM |

Q2 Why do you walk or ride your bike?

Answered: 33 Skipped: 1

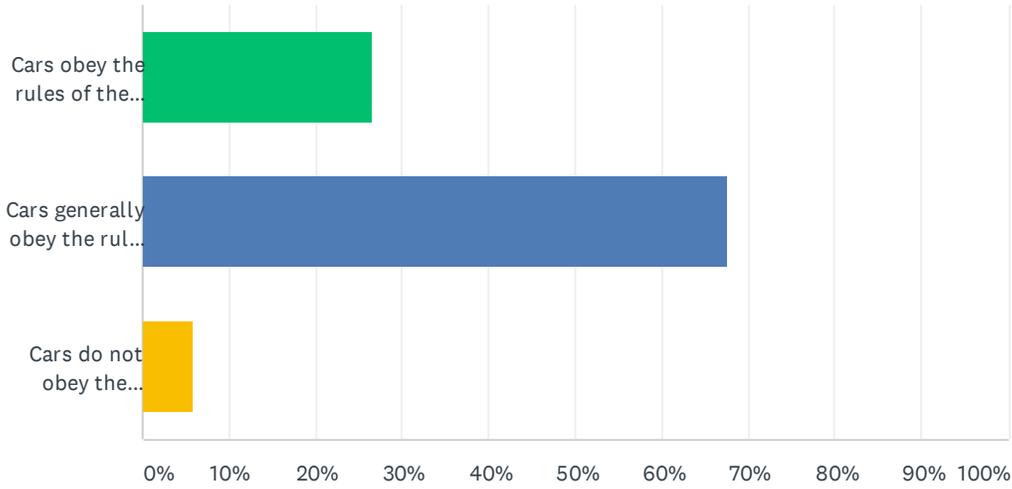


| ANSWER CHOICES | RESPONSES |
|--------------------------|-----------|
| To get places (commute) | 0.00% 0 |
| Recreation/exercise | 75.76% 25 |
| Time with family/friends | 15.15% 5 |
| Other (please specify) | 9.09% 3 |
| TOTAL | 33 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|--|-------------------|
| 1 | Walking the dog | 8/19/2020 1:28 PM |
| 2 | It is a combination of "to get places" "recreation/exercise" and also to enjoy a few alcoholic beverages and not need to drive home. | 8/19/2020 8:23 AM |
| 3 | I don't...this is a poorly designed survey | 8/13/2020 6:08 PM |

Q3 How do you feel about motor vehicle behavior on Main Street?

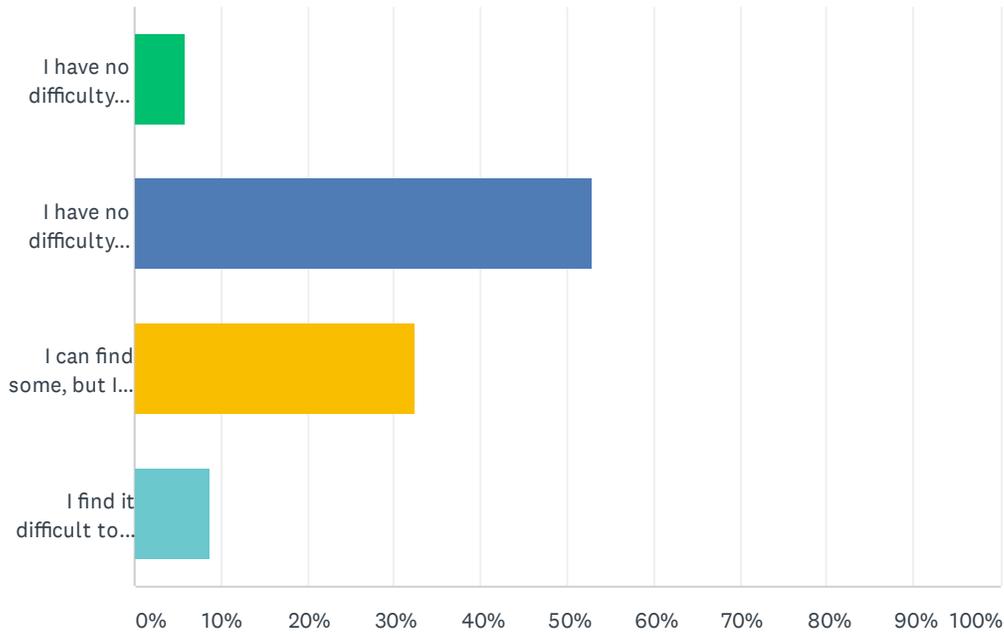
Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Cars obey the rules of the road. | 26.47% | 9 |
| Cars generally obey the rules, but some motorists speed or drive erratically. | 67.65% | 23 |
| Cars do not obey the traffic control rules. | 5.88% | 2 |
| TOTAL | | 34 |

Q4 How do you feel about the pedestrian access around Mascoutah?

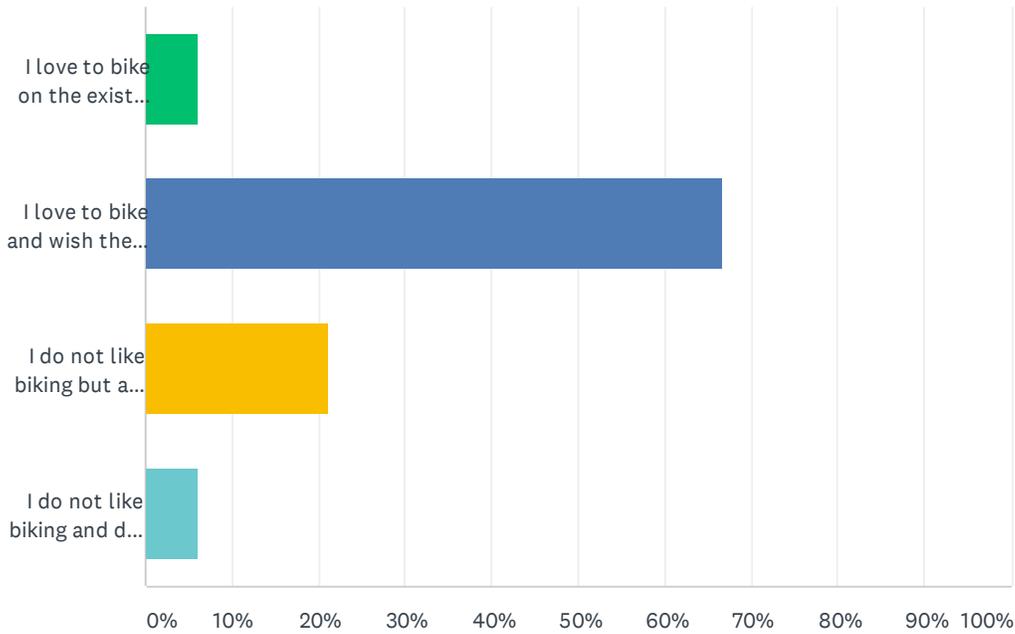
Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| I have no difficulty finding safe places to walk and like the existing spaces (sidewalks, trails, etc.). | 5.88% | 2 |
| I have no difficulty finding safe places to walk, but the existing spaces (sidewalks, trails, etc.) need to be repaired. | 52.94% | 18 |
| I can find some, but I wish there were more pedestrian spaces (sidewalks, trails, etc.). | 32.35% | 11 |
| I find it difficult to find safe places to walk. | 8.82% | 3 |
| TOTAL | | 34 |

Q5 How do you feel about the bicycle access around Mascoutah?

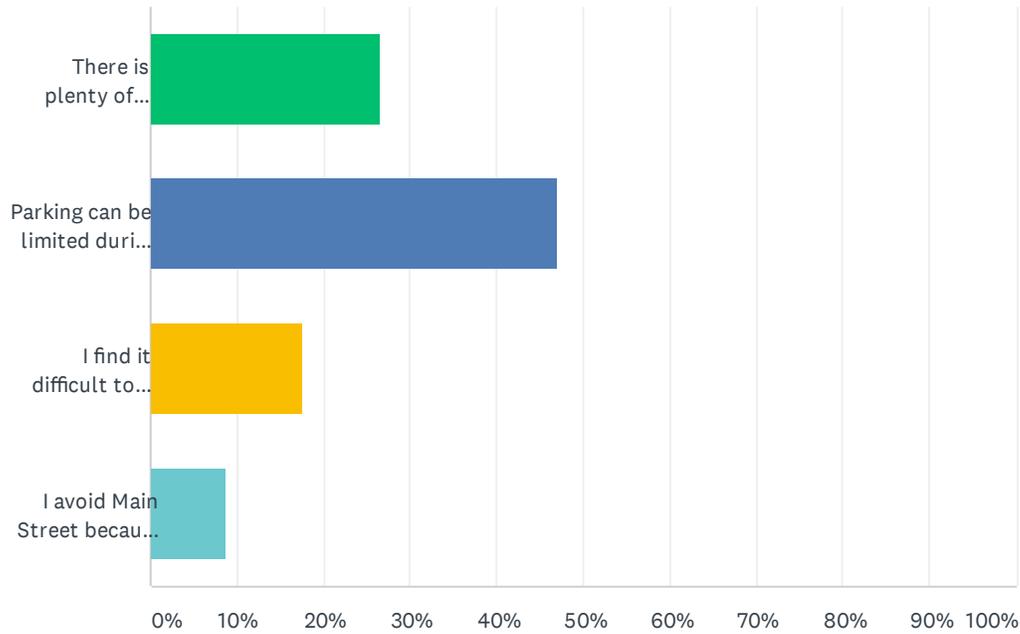
Answered: 33 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| I love to bike on the existing bike paths and do not need more. | 6.06% | 2 |
| I love to bike and wish there were more bike paths throughout the town. | 66.67% | 22 |
| I do not like biking but am open to seeing more bike paths in town. | 21.21% | 7 |
| I do not like biking and do not like sharing the roadway with bicyclists. | 6.06% | 2 |
| TOTAL | | 33 |

Q6 How do you feel about parking on Main Street?

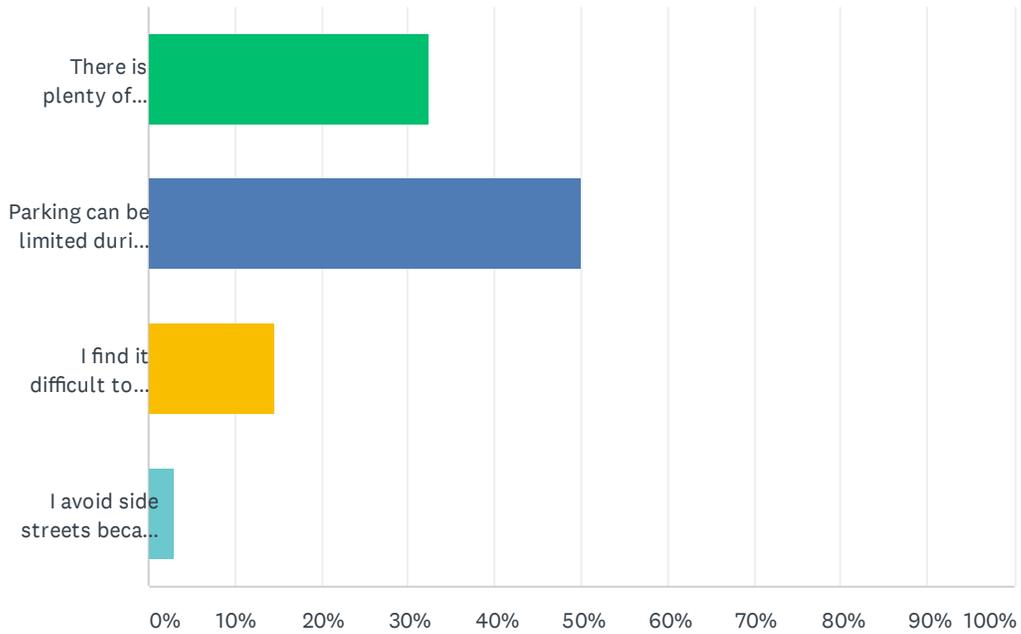
Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| There is plenty of parking. | 26.47% | 9 |
| Parking can be limited during peak events. | 47.06% | 16 |
| I find it difficult to park. | 17.65% | 6 |
| I avoid Main Street because of lack of parking. | 8.82% | 3 |
| TOTAL | | 34 |

Q7 How do you feel about parking on side streets (Church & State)?

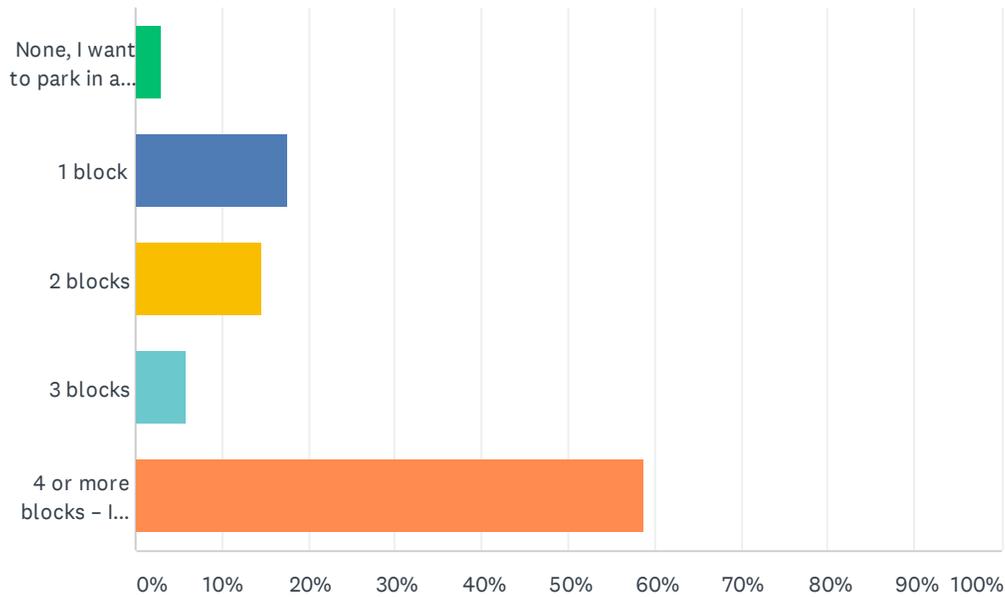
Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| There is plenty of parking. | 32.35% | 11 |
| Parking can be limited during peak events. | 50.00% | 17 |
| I find it difficult to park. | 14.71% | 5 |
| I avoid side streets because of lack of parking. | 2.94% | 1 |
| TOTAL | | 34 |

Q8 How many blocks do you feel safe/comfortable walking to get to your final destination on Main Street?

Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| None, I want to park in a lot or at the door. | 2.94% | 1 |
| 1 block | 17.65% | 6 |
| 2 blocks | 14.71% | 5 |
| 3 blocks | 5.88% | 2 |
| 4 or more blocks – I don't mind walking at all! | 58.82% | 20 |
| TOTAL | | 34 |

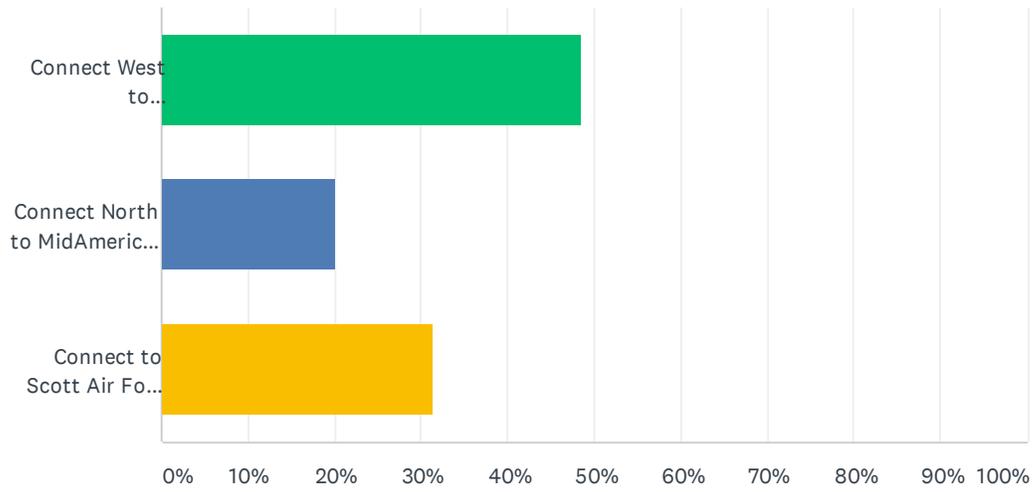
Q9 Are there any other transportation issues that we have not addressed that you would like to bring to our attention?

Answered: 16 Skipped: 18

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | I would like to be able to drive my golf cart on Main Street. | 8/23/2020 9:41 PM |
| 2 | Engine breaking of semi-trucks; it ruins the atmosphere. Need signs prohibiting engine breaking within town. | 8/19/2020 9:51 PM |
| 3 | More bike lanes would be great. I also think the speed limit is too low on the outskirts of town. On Main St I understand the 35 to 25 MPH, but on some of the larger streets that feed into the town, the speed limit hits 35 and there is no reason for it to be that slow, that soon. 45 MPH is what I'd normally expect. | 8/19/2020 8:23 AM |
| 4 | Need more handicapped spaces on main st. Police need to enforce the yellow no parking lines. | 8/18/2020 6:44 PM |
| 5 | Parking on State or Church would be fine. However, many spaces need to be fixed to clearly mark where we are able to park. Some people park in grass and on people's lawns. This would also need to be addressed before putting more parking in these areas. | 8/18/2020 6:41 PM |
| 6 | Pulling out onto main st from the side streets closer to rt 4 is dangerous. The buildings block the view of the road and to cant see if cars are coming | 8/18/2020 6:31 PM |
| 7 | Water flooding every time it rains | 8/18/2020 6:25 PM |
| 8 | In certain ares there's not much you can do to improve parking or sidewalk width due to the buildings being so close to the road. Market street to Jefferson for example... | 8/18/2020 4:59 PM |
| 9 | too many potholes | 8/18/2020 2:47 PM |
| 10 | On bicycle access - we don't need trails. We need roads that are gravel- and pothole-free. We need better driver responsibility and respect. This whole town should be bike safe and bike friendly. | 8/17/2020 6:47 PM |
| 11 | Biking is a joy north of Main Street. I avoid Main street on my bike usually because the sidewalk is absolutely horrible down much of it. | 8/16/2020 11:54 PM |
| 12 | Shouldn't be any parking on Main St. Too narrow for it. | 8/16/2020 9:09 PM |
| 13 | Citizens that live in the subdivisions north of Hog River cannot safely walk or bike to Main Street. Route 4 has no paths or sidewalk to do this in a safe manner. | 8/16/2020 7:25 PM |
| 14 | No | 8/13/2020 11:44 PM |
| 15 | Townsend square does not have sidewalk access to connect the neighborhood to the schools without crossing a 55 mph road. Prairie view needs a bridge from their community lot to the bike path. | 8/13/2020 5:54 PM |
| 16 | A fair amount of streets in town have no sidewalks. | 8/13/2020 5:18 PM |

Q1 Multiple shared-use path options have been presented. If only one can be implemented, which would you choose?

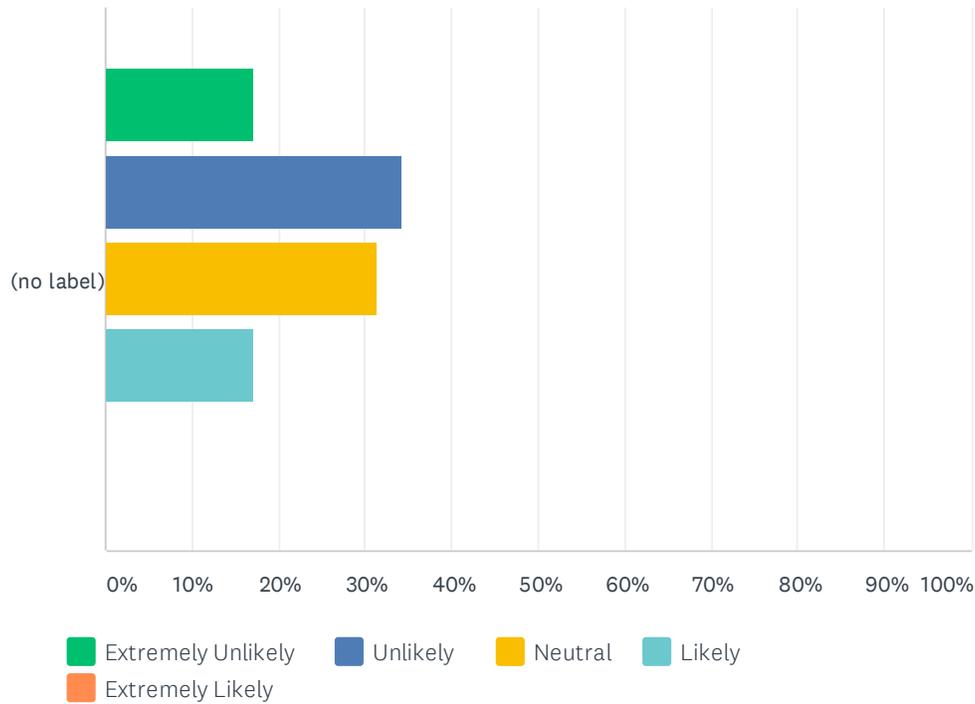
Answered: 35 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|-------------------------------------|-----------|----|
| Connect West to MetroBikeWalk path | 48.57% | 17 |
| Connect North to MidAmerica Airport | 20.00% | 7 |
| Connect to Scott Air Force Base | 31.43% | 11 |
| TOTAL | | 35 |

Q2 How likely are you to utilize the regional shared-use path system if it is primarily on-street (street shoulder)?

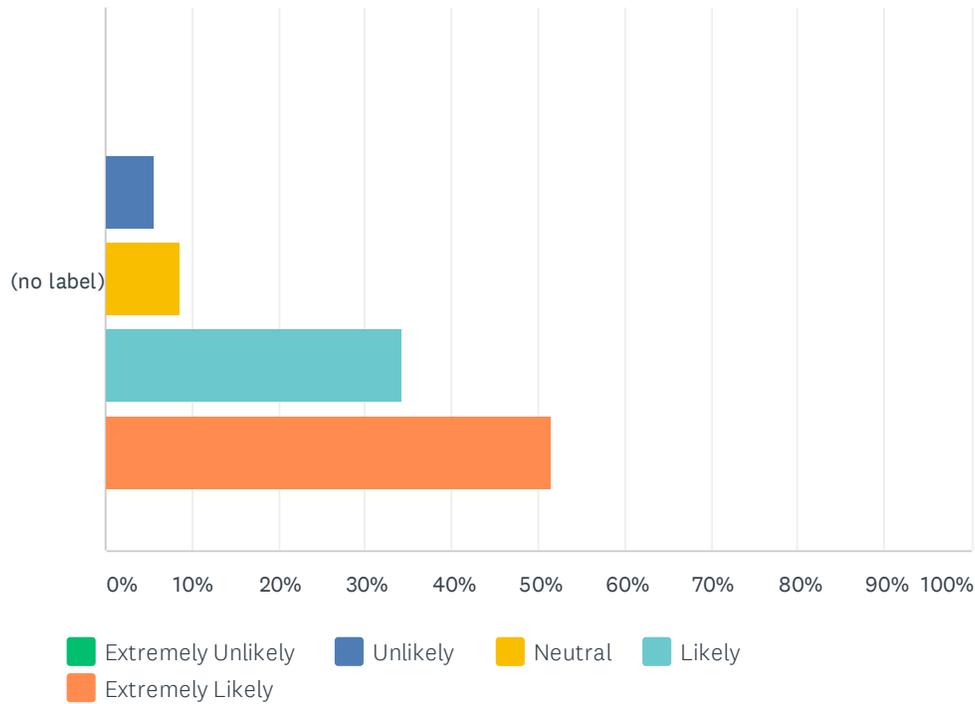
Answered: 35 Skipped: 0



| | EXTREMELY UNLIKELY | UNLIKELY | NEUTRAL | LIKELY | EXTREMELY LIKELY | TOTAL | WEIGHTED AVERAGE |
|------------|--------------------|--------------|--------------|-------------|------------------|-------|------------------|
| (no label) | 17.14% 6 | 34.29% 12 | 31.43% 11 | 17.14% 6 | 0.00% 0 | 35 | 2.49 |

Q3 How likely are you to utilize the regional shared-use path system if it is primarily off-street (greenway)?

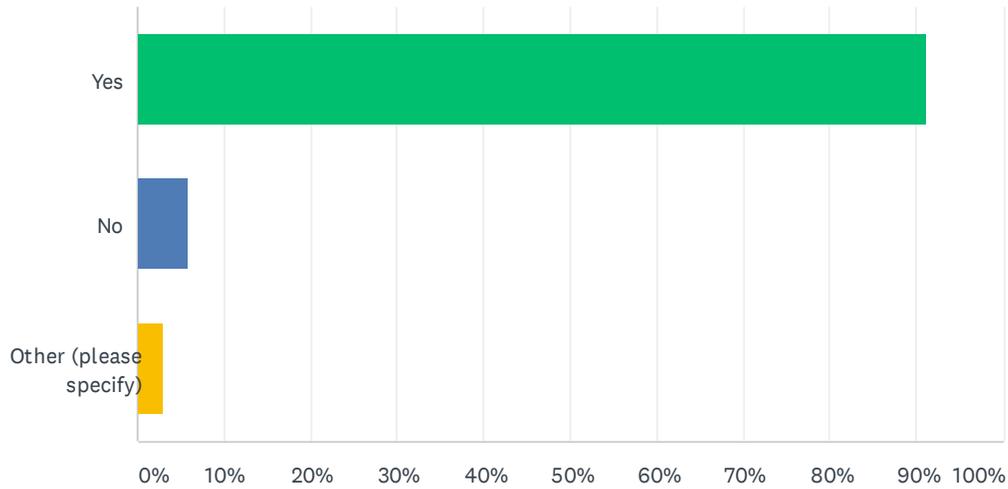
Answered: 35 Skipped: 0



| | EXTREMELY UNLIKELY | UNLIKELY | NEUTRAL | LIKELY | EXTREMELY LIKELY | TOTAL | WEIGHTED AVERAGE |
|------------|--------------------|------------|------------|--------------|------------------|-------|------------------|
| (no label) | 0.00% 0 | 5.71% 2 | 8.57% 3 | 34.29% 12 | 51.43% 18 | 35 | 4.31 |

Q4 Would you use a bike path to connect to the regional trail network?

Answered: 34 Skipped: 1

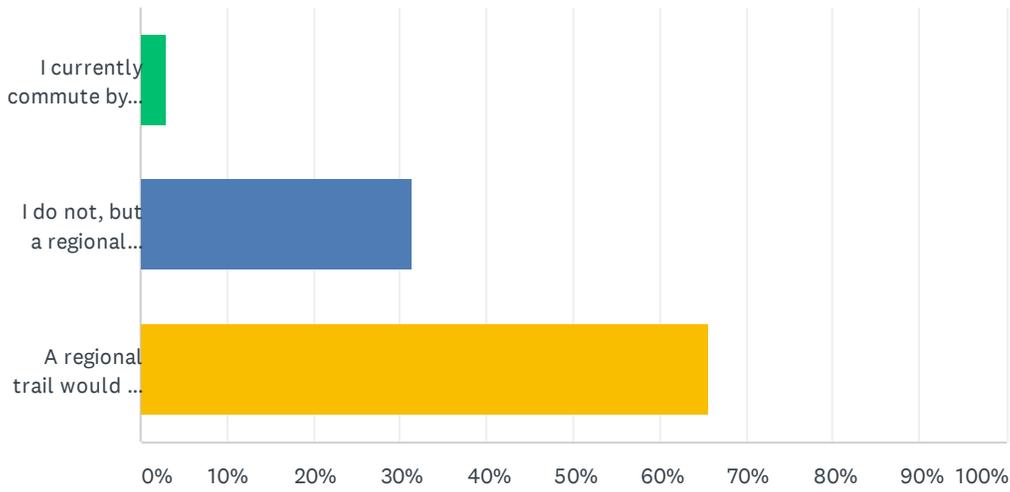


| ANSWER CHOICES | RESPONSES |
|------------------------|-----------|
| Yes | 91.18% 31 |
| No | 5.88% 2 |
| Other (please specify) | 2.94% 1 |
| TOTAL | 34 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|------------------------|-------------------|
| 1 | Rarely, if ever | 8/26/2020 3:33 PM |

Q5 Do you commute by bike or walking to work? If not, would a regional trail system allow you to? Choose the response that best describes you.

Answered: 35 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|----|
| I currently commute by walking or biking. | 2.86% | 1 |
| I do not, but a regional trail would allow me to do so. | 31.43% | 11 |
| A regional trail would not allow me to bike or walk to work. | 65.71% | 23 |
| TOTAL | | 35 |

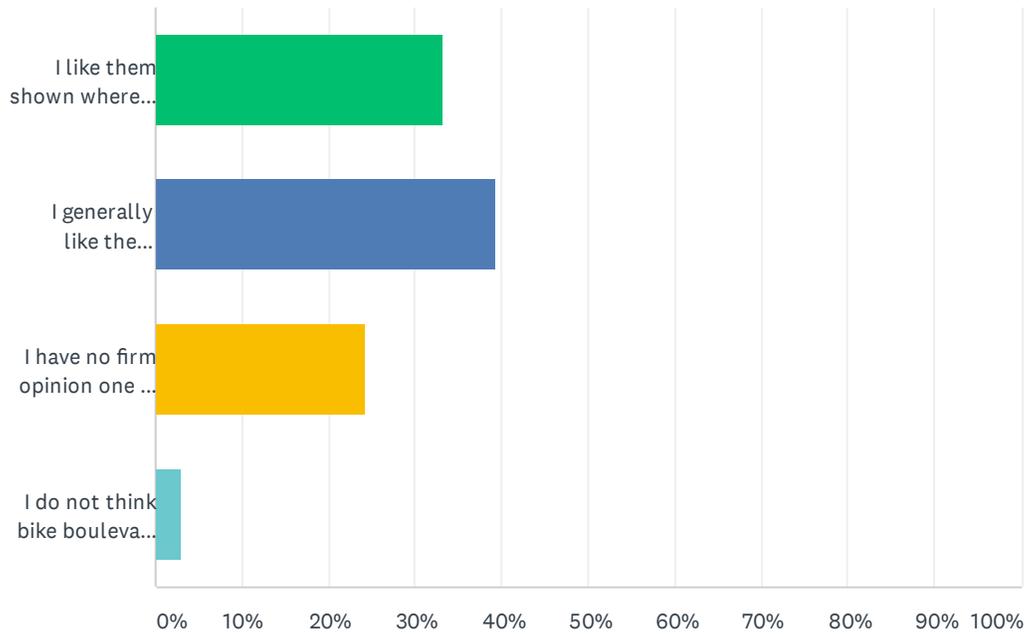
Q6 Besides financial challenges, what potential pitfalls do you foresee in developing a regional multi-use trail system in Mascoutah?

Answered: 20 Skipped: 15

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | Distance of the trail and cost. I would rather Mascoutah focus on fixing flooding problems before moving trying to make Mascoutah a tourist destination. | 9/2/2020 12:40 AM |
| 2 | Security | 9/1/2020 1:36 PM |
| 3 | I do not believe that city residents like these paths in near their homes. | 8/31/2020 9:51 PM |
| 4 | Some residents will be against a regional connection leading to loss of the small town charm. | 8/31/2020 9:28 AM |
| 5 | It seems that it would be difficult to find the best route to connect, ie along Rt. 161, there is the creek bottoms which floods often. I have seen floating paths used in other locales and wonder if that Would be an option. Going out of town along Rt. 177 has similar problems. But I definitely feel that if it could be done, it definitely would get much use and would open up so many opportunities to access areas around Belleville and St. Louis by connecting with the Metro Bike Trail. I think it would also appeal to potential homebuyers in Mascoutah. I would love to see it happen!! | 8/30/2020 8:29 PM |
| 6 | drainage | 8/28/2020 10:10 AM |
| 7 | challenges with state road transportation and challenges with crossing the Silver Creek wetlands | 8/27/2020 2:48 PM |
| 8 | Who maintains? | 8/26/2020 10:13 PM |
| 9 | Land Acquisition | 8/26/2020 9:36 PM |
| 10 | None | 8/26/2020 9:35 PM |
| 11 | Silver creek and connecting with the st clair county trails | 8/26/2020 8:49 PM |
| 12 | I don't think we need to connect regionally | 8/26/2020 7:52 PM |
| 13 | Location | 8/26/2020 5:38 PM |
| 14 | Mascoutah is very resistant to changing their "small town" feel so people need to be sold on any plans to improve their way of life. | 8/26/2020 5:01 PM |
| 15 | Safety and upkeep | 8/26/2020 4:00 PM |
| 16 | None | 8/26/2020 3:59 PM |
| 17 | Due to the position of Mascoutah, bikers would have to make a commitment to walk or bike a decent distance before reaching any area for any kind of purpose. | 8/26/2020 3:53 PM |
| 18 | Making it easier for less-desirable citizens to reach Mascoutah would not be ideal. | 8/26/2020 3:33 PM |
| 19 | Continue maintenance and upkeep | 8/26/2020 3:14 PM |
| 20 | Maintenance. Already seem to have issues with it in the parks at times. Also security would be a concern but overall that is great in town | 8/26/2020 2:47 PM |

Q7 Do you agree with the proposed locations of the Bike Boulevards (indicated in blue on the map below).

Answered: 33 Skipped: 2



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| I like them shown where they are. | 33.33% | 11 |
| I generally like the locations, but with some adjustments. | 39.39% | 13 |
| I have no firm opinion one way or the other. | 24.24% | 8 |
| I do not think bike boulevards should be used in Mascoutah. | 3.03% | 1 |
| TOTAL | | 33 |

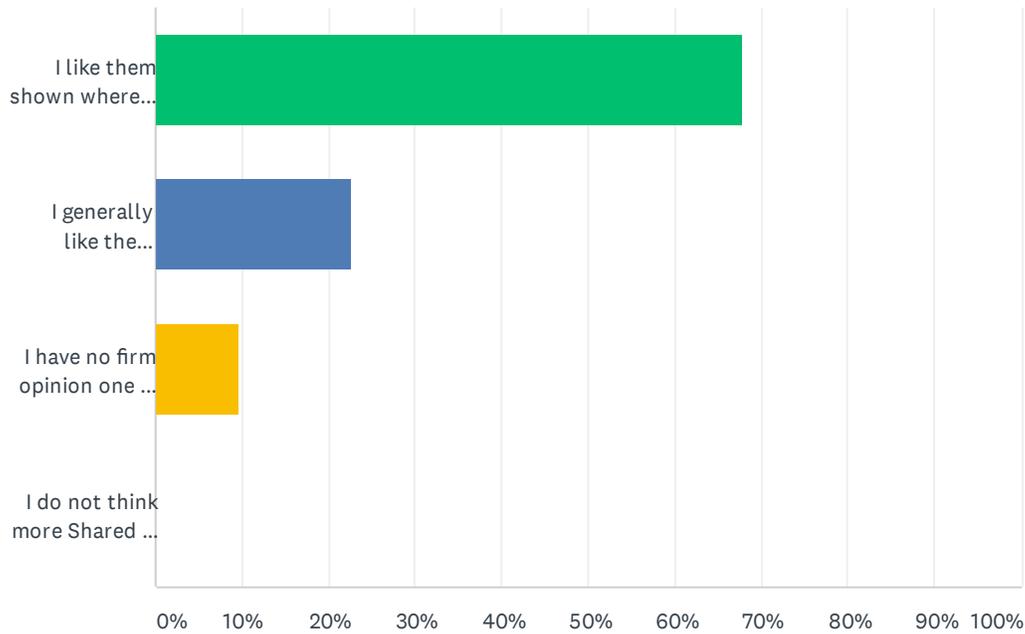
Q8 What changes would you suggest?

Answered: 10 Skipped: 25

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Leu Civic Center needs to be on one of the routes. | 8/31/2020 9:52 PM |
| 2 | Poplar Street is narrow so it may not be comfortable riding a bike there. | 8/31/2020 9:30 AM |
| 3 | I don't think Harnett and Sixth Streets should be bicycle boulevards. I think Poplar St. Would be a better East/west and I'm not sure which side streets would work better for North/South instead of Sixth Street. There are already many accidents at Harnett and Sixth Street Intersection especially when school is in session. I do like the idea of Church Street being used. | 8/30/2020 8:35 PM |
| 4 | A connection from north to south on both the east and west sides of town, forming a loop around the city | 8/28/2020 12:22 PM |
| 5 | jefferson is too busy to incorporate a bike trail | 8/26/2020 7:54 PM |
| 6 | All of it connected | 8/26/2020 5:39 PM |
| 7 | Speed limit would need to be enforced. | 8/26/2020 4:02 PM |
| 8 | dedicated bike lanes with solid white lines if in use with vehicle traffic so bikers don't take up the whole road | 8/26/2020 3:55 PM |
| 9 | Connecting a path to the SW bike paths | 8/26/2020 2:54 PM |
| 10 | Not sure why have corridor near nursing home in southwest portion. Also be cool to have one along LN ROW | 8/26/2020 2:49 PM |

Q9 Do you agree with the proposed locations of additional Shared Use Paths (indicated in yellow and dashed yellow on the map below).

Answered: 31 Skipped: 4



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| I like them shown where they are. | 67.74% | 21 |
| I generally like the locations but with some adjustments. | 22.58% | 7 |
| I have no firm opinion one way or the other. | 9.68% | 3 |
| I do not think more Shared Use Paths should be used in Mascoutah. | 0.00% | 0 |
| TOTAL | | 31 |

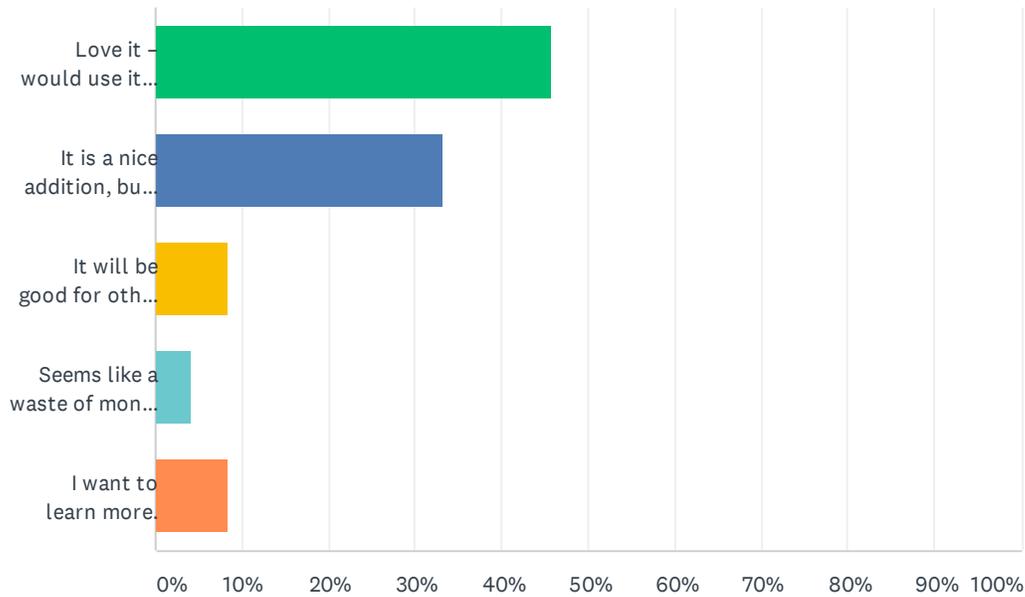
Q10 What changes would you suggest?

Answered: 7 Skipped: 28

| # | RESPONSES | DATE |
|---|--|--------------------|
| 1 | Get rid of the yellow dash trails. No one travels or visits the southern part of town. No reason for those trails. | 9/2/2020 12:42 AM |
| 2 | would not locate those on state routes | 8/27/2020 2:59 PM |
| 3 | Shared use of Brickyard road to County park about 1 mile south of town. | 8/27/2020 10:54 AM |
| 4 | Not using Jefferson St | 8/26/2020 7:57 PM |
| 5 | Need to connect all the subdivisions north of the "big ditch" so people can get into town and also to Dollar General. | 8/26/2020 5:02 PM |
| 6 | None at the moment | 8/26/2020 4:21 PM |
| 7 | Shift the beginnings of the paths on Route 4 to Fountain View Drive. A lot of drivers drive above the speed limit on Route 4 in this area. | 8/26/2020 3:56 PM |

Q1 What do you think about adding shared use paths along side-streets in Mascoutah (like N County Rd, Harper Rd, and/or Brickyard Rd)?

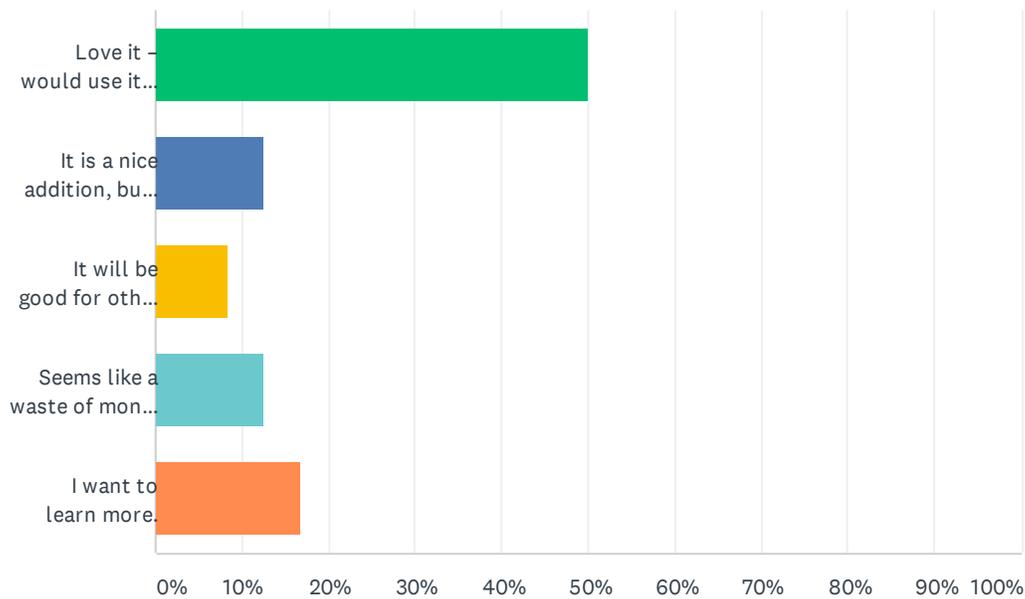
Answered: 24 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Love it – would use it all the time. | 45.83% | 11 |
| It is a nice addition, but I will only use periodically. | 33.33% | 8 |
| It will be good for others to use. | 8.33% | 2 |
| Seems like a waste of money, don't do it. | 4.17% | 1 |
| I want to learn more. | 8.33% | 2 |
| TOTAL | | 24 |

Q2 What do you think about the bike boulevard treatments along low volume streets in Mascoutah (like Church St, Poplar St, and/or Harnett St)?

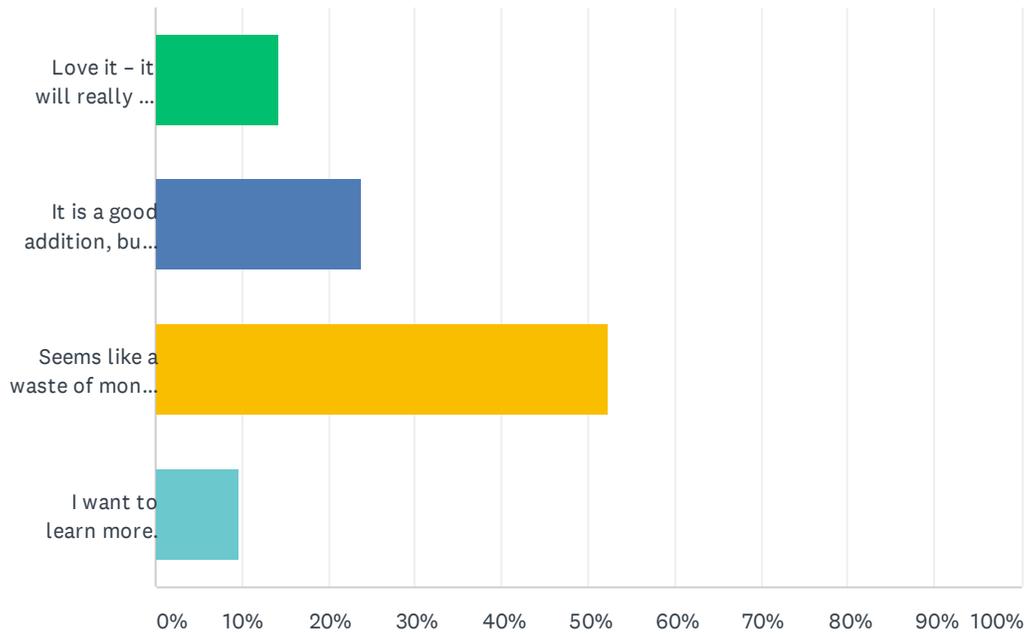
Answered: 24 Skipped: 0



| ANSWER CHOICES | PERCENTAGE | COUNT |
|--|------------|-----------|
| Love it – would use it all the time. | 50.00% | 12 |
| It is a nice addition, but I will only use periodically. | 12.50% | 3 |
| It will be good for others to use. | 8.33% | 2 |
| Seems like a waste of money, don't do it. | 12.50% | 3 |
| I want to learn more. | 16.67% | 4 |
| TOTAL | | 24 |

Q1 What do you think about using entry signage (monument strategies) as a transition to Main Street from the rural area east of Mine Road?

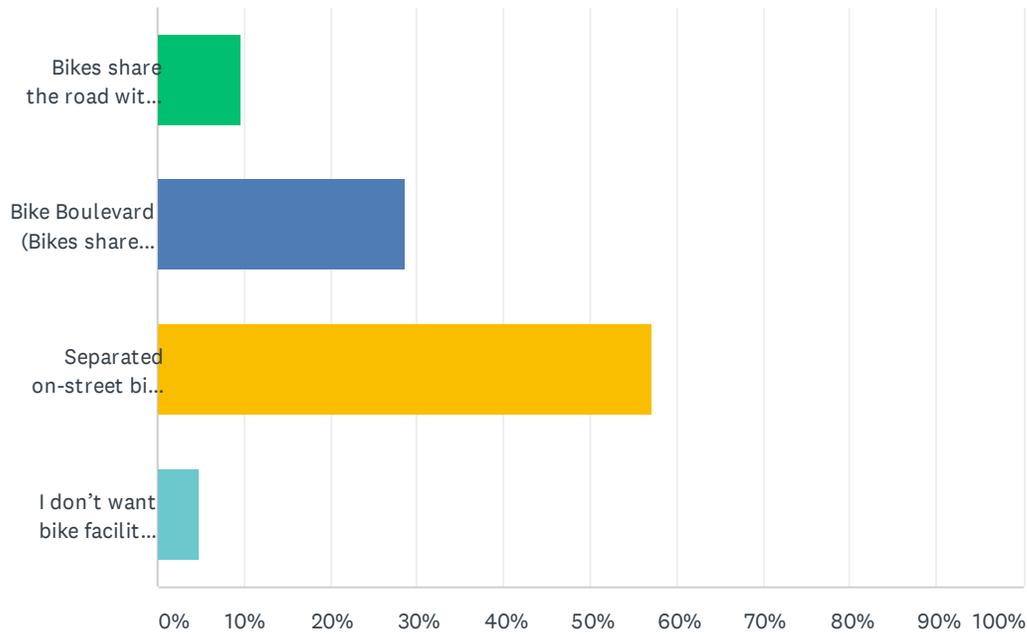
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Love it – it will really add to Mascoutah. | 14.29% | 3 |
| It is a good addition, but I am indifferent. | 23.81% | 5 |
| Seems like a waste of money, don't do it. | 52.38% | 11 |
| I want to learn more. | 9.52% | 2 |
| TOTAL | | 21 |

Q6 Which of the following options would you prefer for new bike facilities along Church Street?

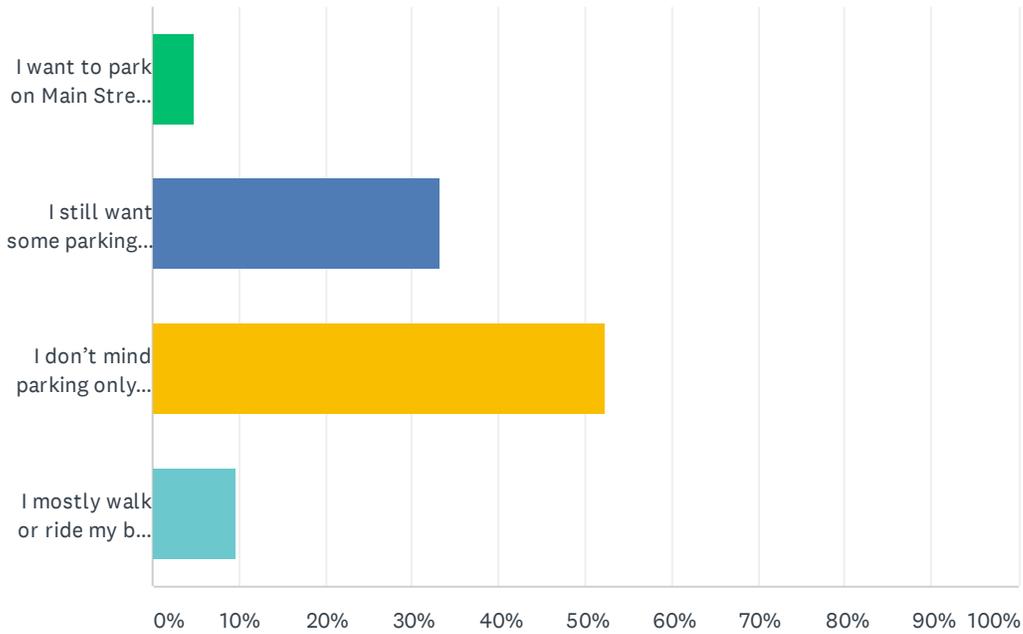
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Bikes share the road with cars as existing. | 9.52% | 2 |
| Bike Boulevard (Bikes share road with cars with more traffic calming elements.) | 28.57% | 6 |
| Separated on-street bike lanes. | 57.14% | 12 |
| I don't want bike facilities on Church Street. | 4.76% | 1 |
| TOTAL | | 21 |

Q7 How would you prefer to park in downtown Mascoutah?

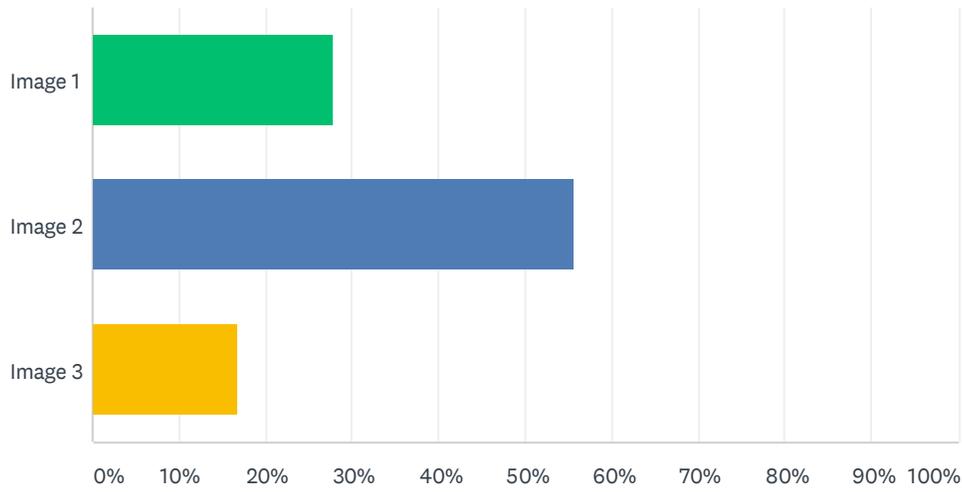
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| I want to park on Main Street right outside my destination. | 4.76% | 1 |
| I still want some parking on Main Street, but I don't mind walking a few blocks to my destination. | 33.33% | 7 |
| I don't mind parking only on side streets or back lots off Main Street. | 52.38% | 11 |
| I mostly walk or ride my bike to Main Street. | 9.52% | 2 |
| TOTAL | | 21 |

Q1 Which option do you prefer for the west segment (Main Street from 6th to 2nd Street)?

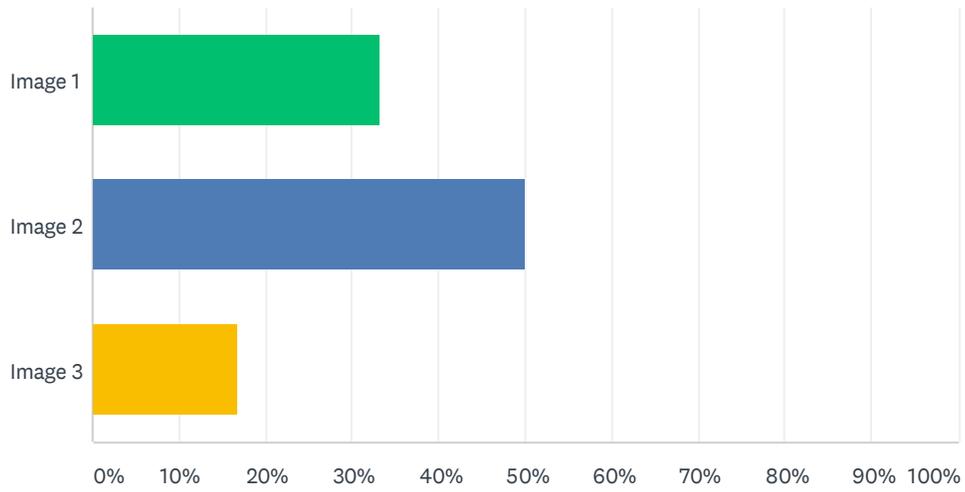
Answered: 18 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|----|
| Image 1 | 27.78% | 5 |
| Image 2 | 55.56% | 10 |
| Image 3 | 16.67% | 3 |
| TOTAL | | 18 |

Q2 Which option do you prefer for the central segment (Main Street from Independence Street to Mine Street)?

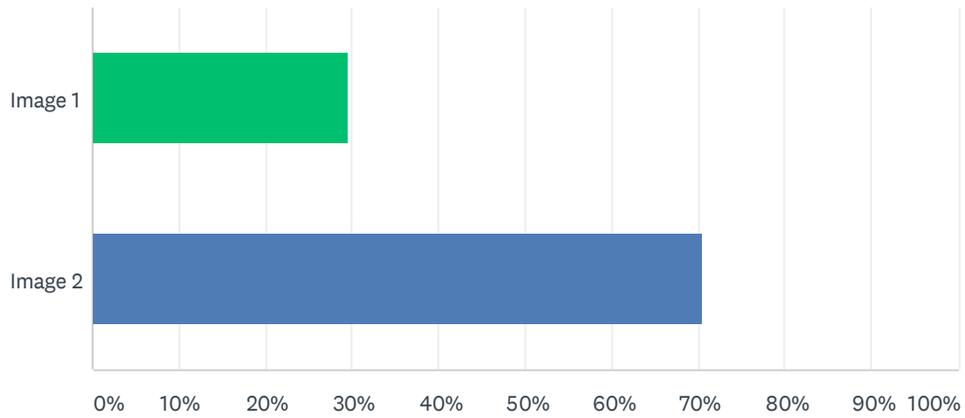
Answered: 18 Skipped: 0



| ANSWER CHOICES | | RESPONSES | |
|----------------|--|-----------|----|
| Image 1 | | 33.33% | 6 |
| Image 2 | | 50.00% | 9 |
| Image 3 | | 16.67% | 3 |
| TOTAL | | | 18 |

Q3 Which option do you prefer for the east segment (Main Street from Independence Street to Mine Street)?

Answered: 17 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|----|
| Image 1 | 29.41% | 5 |
| Image 2 | 70.59% | 12 |
| TOTAL | | 17 |

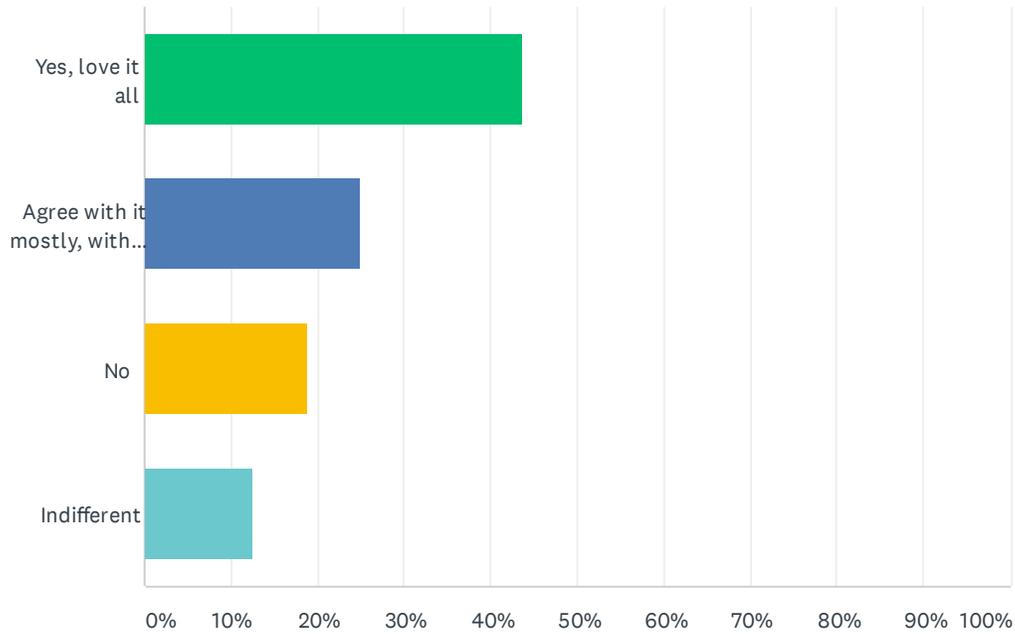
Q4 Do you have any specific comments on the proposed configurations within any or all of the particular segments (i.e. thoughts on removing on-street parking or recommendations for particular uses of the pedestrian space)?

Answered: 6 Skipped: 12

| # | RESPONSES | DATE |
|---|--|-------------------|
| 1 | The set up of main street is fine. The side walks need to be repaired but that's about it. Major design changes would be a waste of money. | 9/2/2020 12:56 AM |
| 2 | We'd like to see a focus on aesthetics and bringing in more restaurants and small business. | 8/31/2020 9:25 PM |
| 3 | Do not get rid of on street parking, add to it. | 8/26/2020 5:49 PM |
| 4 | Would like to keep street parking or add parking lots behind the buildings | 8/26/2020 4:12 PM |
| 5 | Get rid of on Main Street parking | 8/26/2020 4:11 PM |
| 6 | Be really nice to have extra wide sidewalks downtown to allow for outdoor dining like Belleville. Create a more fun downtown | 8/26/2020 2:41 PM |

Q1 Do you agree with the recommended regional connection plan?

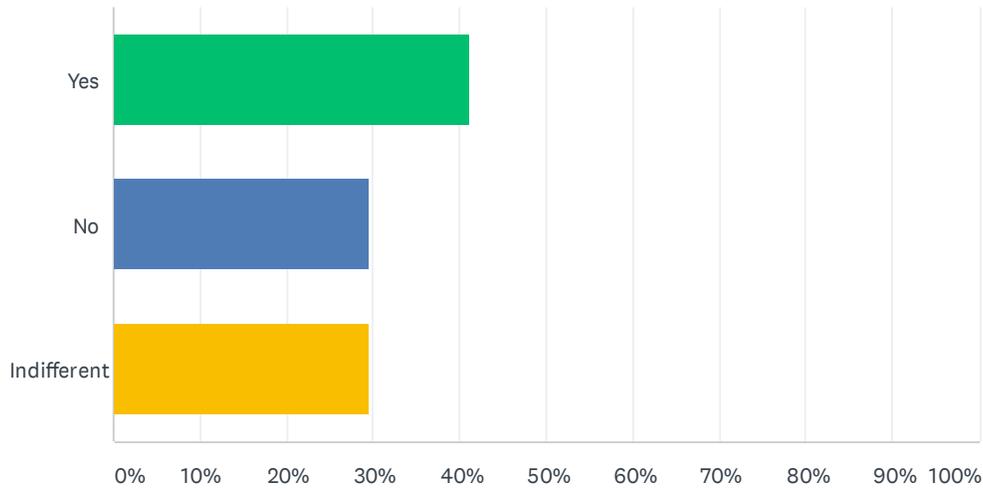
Answered: 16 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, love it all | 43.75% | 7 |
| Agree with it mostly, with some changes | 25.00% | 4 |
| No | 18.75% | 3 |
| Indifferent | 12.50% | 2 |
| TOTAL | | 16 |

Q2 Do you agree with prioritizing the connection to St. Clair County BikeLink trail over the connection to Scott Airforce Base and the Multi-Modal Hub at the MidAmerica Airport?

Answered: 17 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----------|
| Yes | 41.18% | 7 |
| No | 29.41% | 5 |
| Indifferent | 29.41% | 5 |
| TOTAL | | 17 |

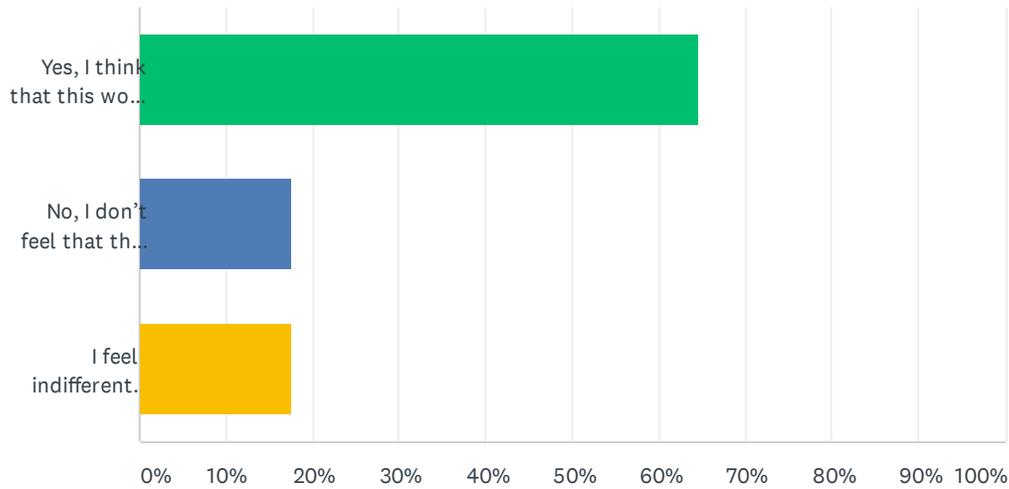
Q3 What changes would you make to the regional connection plan?

Answered: 9 Skipped: 8

| # | RESPONSES | DATE |
|---|--|--------------------|
| 1 | None | 9/21/2020 11:37 PM |
| 2 | Would there be a way long term if the focus was initially on connecting with the st Clair county bike trail, but to eventually create a shortcut to also connect with SAFB? I was also wondering if somehow we could obtain the rights to the L &N railway track into Belleville? I also would like consideration of a bike path around the perimeter of Mascoutah. I can't remember if that was in the plan | 9/21/2020 12:54 PM |
| 3 | More accessible entry | 9/21/2020 10:09 AM |
| 4 | None | 9/16/2020 9:43 AM |
| 5 | eliminate it | 9/16/2020 7:19 AM |
| 6 | Connect to safb | 9/15/2020 7:12 PM |
| 7 | Always more trees, more gardens, more green space. | 9/15/2020 6:56 PM |
| 8 | None | 9/15/2020 6:27 PM |
| 9 | Not do it | 9/15/2020 5:06 PM |

Q4 Do you agree with the proposed regional highway wayfinding directional signage, located on Hwy 64 and Rte. 4 and at Rte. 177 and 158?

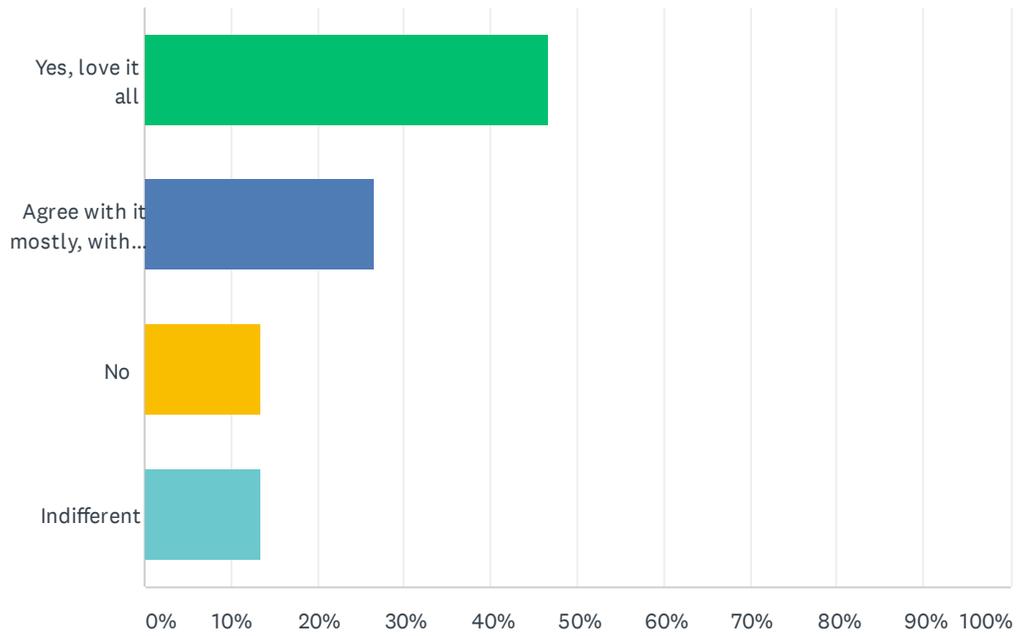
Answered: 17 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, I think that this would be beneficial. | 64.71% | 11 |
| No, I don't feel that they are needed. | 17.65% | 3 |
| I feel indifferent. | 17.65% | 3 |
| TOTAL | | 17 |

Q1 Do you agree with the recommended pedestrian and bike network plan?

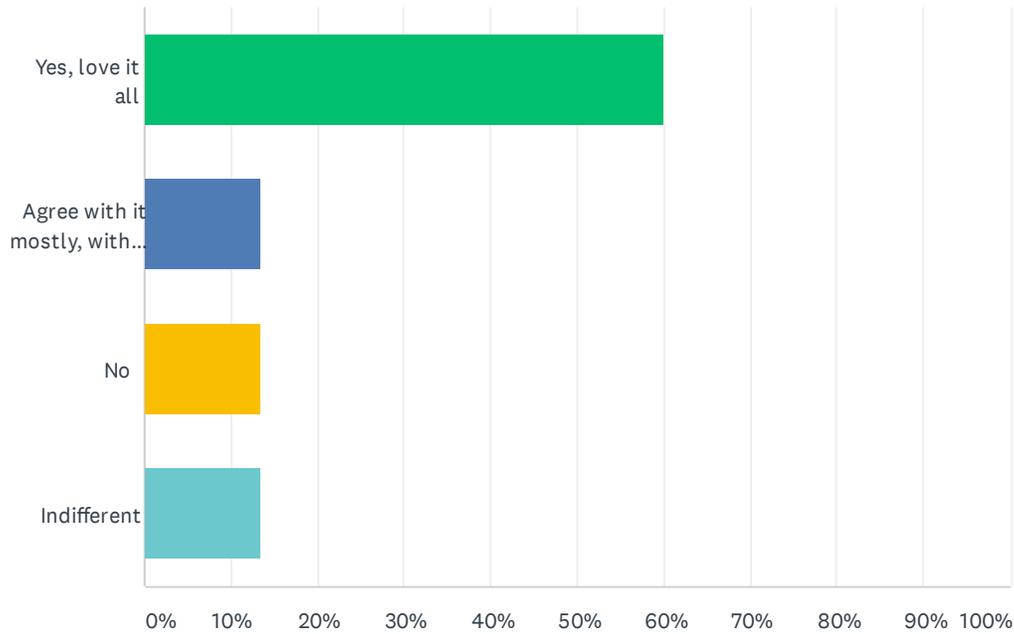
Answered: 15 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, love it all | 46.67% | 7 |
| Agree with it mostly, with some changes | 26.67% | 4 |
| No | 13.33% | 2 |
| Indifferent | 13.33% | 2 |
| TOTAL | | 15 |

Q2 Do you agree with the recommended first phase implementation of bike boulevard loop on 10th Street, Railway Street, Independence Street, Poplar Street, South Street, and temporarily on Church Street?

Answered: 15 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|----|
| Yes, love it all | 60.00% | 9 |
| Agree with it mostly, with some changes | 13.33% | 2 |
| No | 13.33% | 2 |
| Indifferent | 13.33% | 2 |
| TOTAL | | 15 |

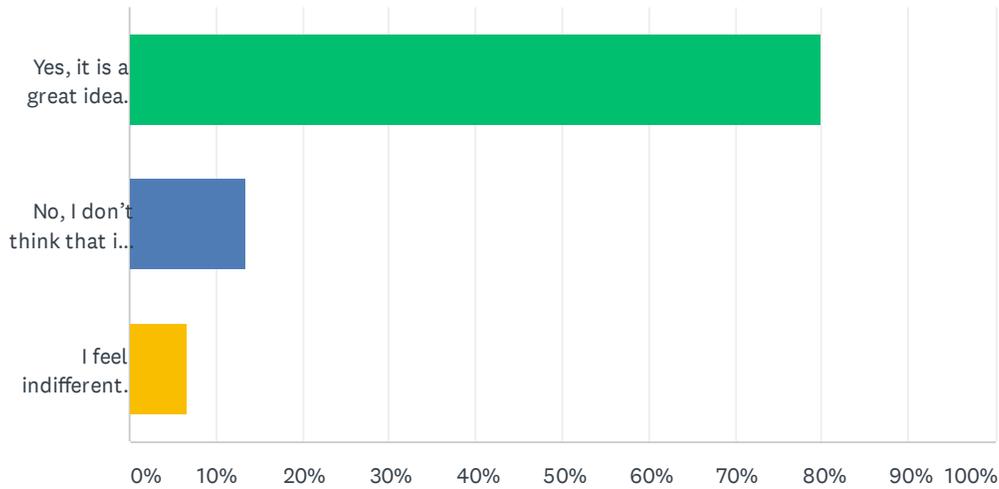
Q3 Would you make any changes to the pedestrian and bike network plan?

Answered: 7 Skipped: 8

| # | RESPONSES | DATE |
|---|---|--------------------|
| 1 | No | 9/21/2020 11:38 PM |
| 2 | no changes | 9/21/2020 2:59 PM |
| 3 | I would hesitate using Harnett and Sixth Streets as main routes for shared bike routes. I would suggest Poplar St. for the East/west route (with some re-routing on the eastern edge of town) and 10th Street as the north/south route. Harnett and Sixth st are too heavily traveled, and already too many accidents occurring at Harnett and Sixth St. Intersection | 9/21/2020 1:00 PM |
| 4 | none | 9/16/2020 9:44 AM |
| 5 | eliminate it | 9/16/2020 7:20 AM |
| 6 | No | 9/15/2020 6:59 PM |
| 7 | Unsure | 9/15/2020 6:58 PM |

Q4 Do you agree with the following recommendation for the bike trail wayfinding network?: Add informational and directional signage as the proposed trails are implemented.

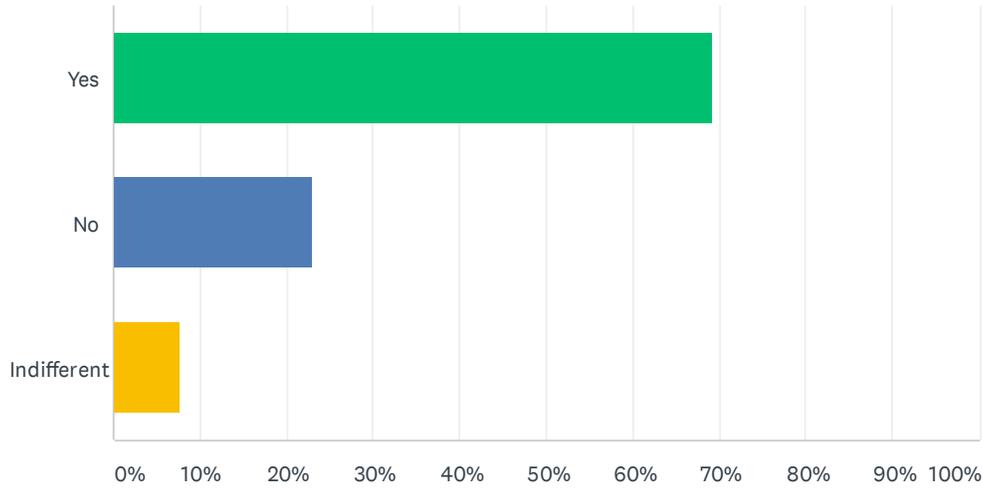
Answered: 15 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, it is a great idea. | 80.00% | 12 |
| No, I don't think that it is necessary. | 13.33% | 2 |
| I feel indifferent. | 6.67% | 1 |
| TOTAL | | 15 |

Q1 Do you agree with the recommendation for a Shared Use Path on the north side of Church Street instead of a Bike Boulevard?

Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|----------------|-----------|
| Yes | 69.23% 9 |
| No | 23.08% 3 |
| Indifferent | 7.69% 1 |
| TOTAL | 13 |

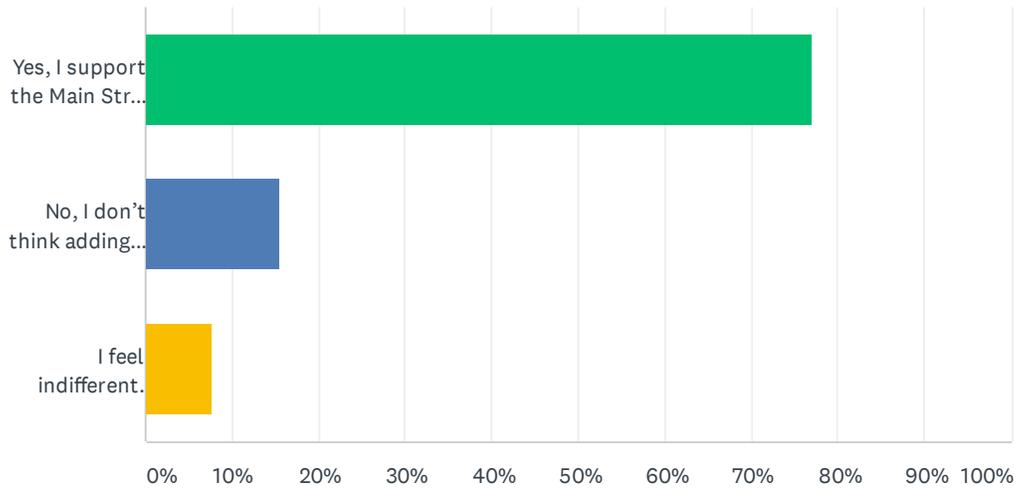
Q2 For your answer to the previous question, why or why not?

Answered: 6 Skipped: 7

| # | RESPONSES | DATE |
|---|---|--------------------|
| 1 | The street is wide enough to support a shared use path | 9/21/2020 11:42 PM |
| 2 | I don't think church street is heavily traveled enough to warrant a bike boulevard. It's already fairly bike friendly | 9/21/2020 1:12 PM |
| 3 | The street is busy enough without a bike lane! | 9/21/2020 10:10 AM |
| 4 | Bikes and pedestrians sharing the same path is not a good idea IMO... Fix the sidewalks so they are useable and add a separate bike lane on Church Street | 9/21/2020 8:53 AM |
| 5 | I still have concerns for traffic. I've seen bikes in sidewalks in town, so bicyclists might not feel comfortable sharing the road with cars. | 9/16/2020 9:48 AM |
| 6 | no need for this in residential areas | 9/16/2020 7:23 AM |

Q3 Do you agree with the recommendations of using pedestrian and vehicular wayfinding signage along the Main Street corridor to direct, inform, and guide visitors?

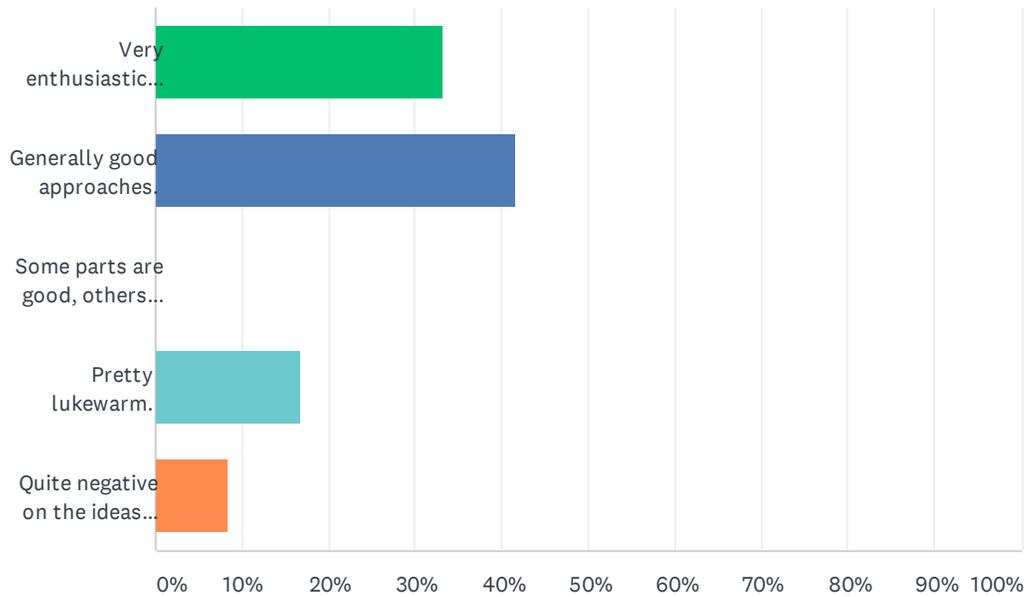
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Yes, I support the Main Street wayfinding signage recommendations. | 76.92% | 10 |
| No, I don't think adding wayfinding signage on Main Street is necessary. | 15.38% | 2 |
| I feel indifferent. | 7.69% | 1 |
| TOTAL | | 13 |

Q5 What is your reaction to the overall ideas discussed for the Main Street corridor?

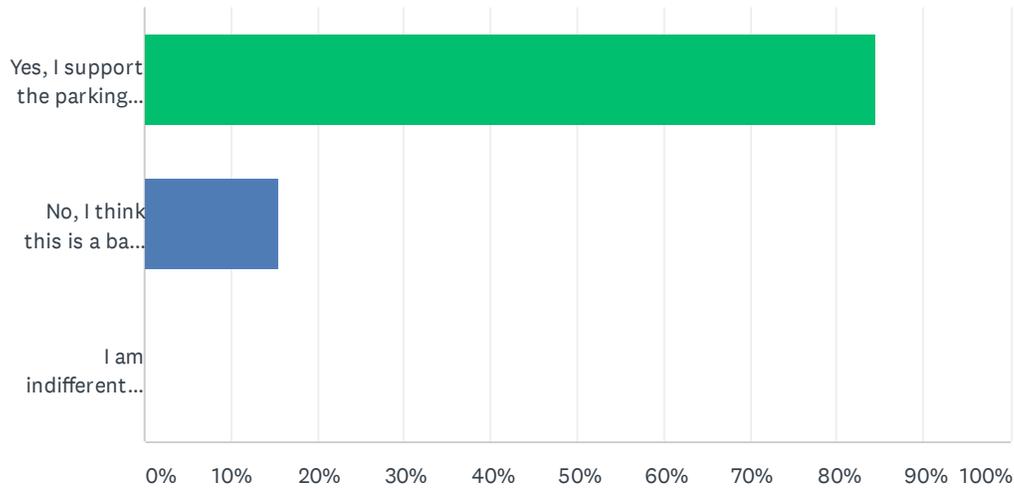
Answered: 12 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 33.33% | 4 |
| Generally good approaches. | 41.67% | 5 |
| Some parts are good, others are doubtful. | 0.00% | 0 |
| Pretty lukewarm. | 16.67% | 2 |
| Quite negative on the ideas – off the mark. | 8.33% | 1 |
| TOTAL | | 12 |

Q6 Do you agree with the recommendations to better organize and coordinate parking behind the buildings along Main Street with primary vehicular access off of Church Street?

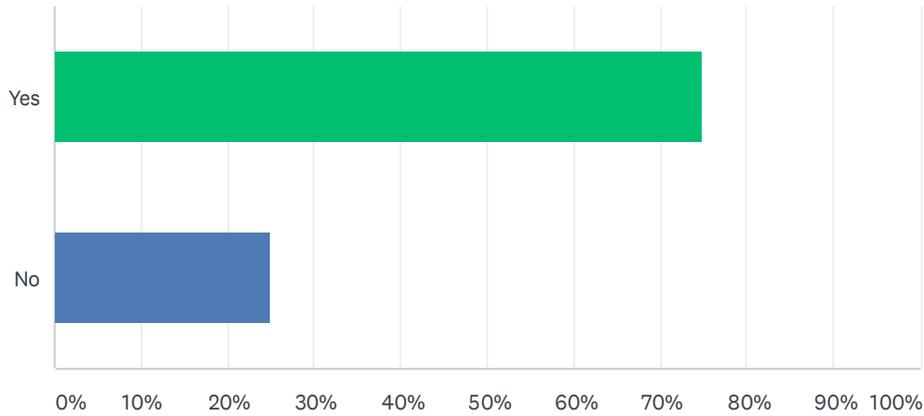
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, I support the parking recommendations. | 84.62% | 11 |
| No, I think this is a bad idea. | 15.38% | 2 |
| I am indifferent about this. | 0.00% | 0 |
| TOTAL | | 13 |

Q1 Do you place a greater value on the additional 4 feet of new pedestrian space, on both sides of Main Street (in the center segment of our Main Street study area), as a better use than the current one-sided parallel parking? The additional 4 feet of sidewalk increases the total from 8 to 12 feet for pedestrians and includes lighting, benches, signs, and trash receptacles.

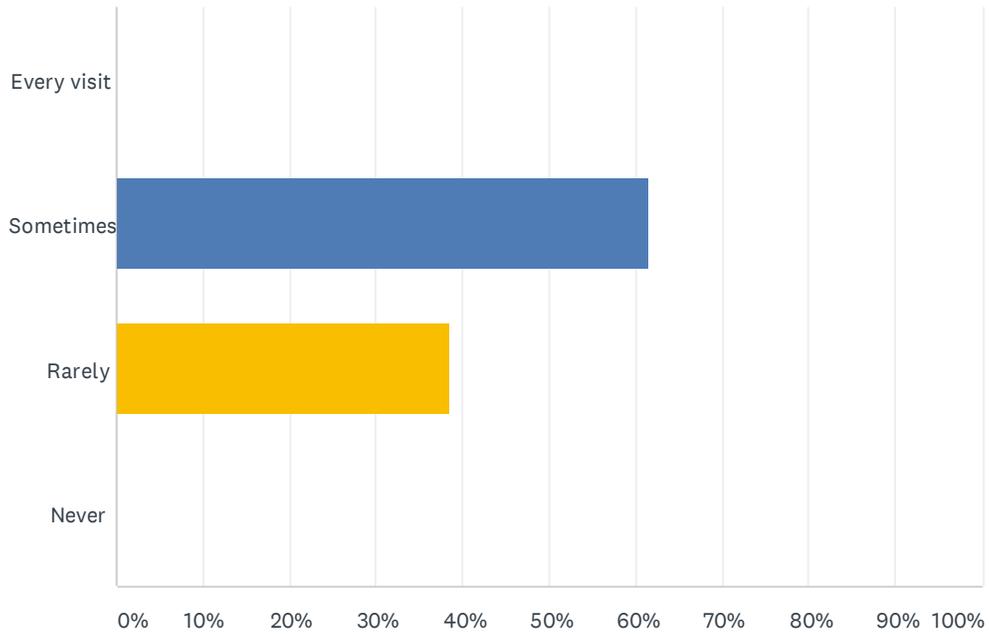
Answered: 12 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|----|
| Yes | 75.00% | 9 |
| No | 25.00% | 3 |
| TOTAL | | 12 |

Q4 How often do you parallel park your vehicle downtown on Main Street?

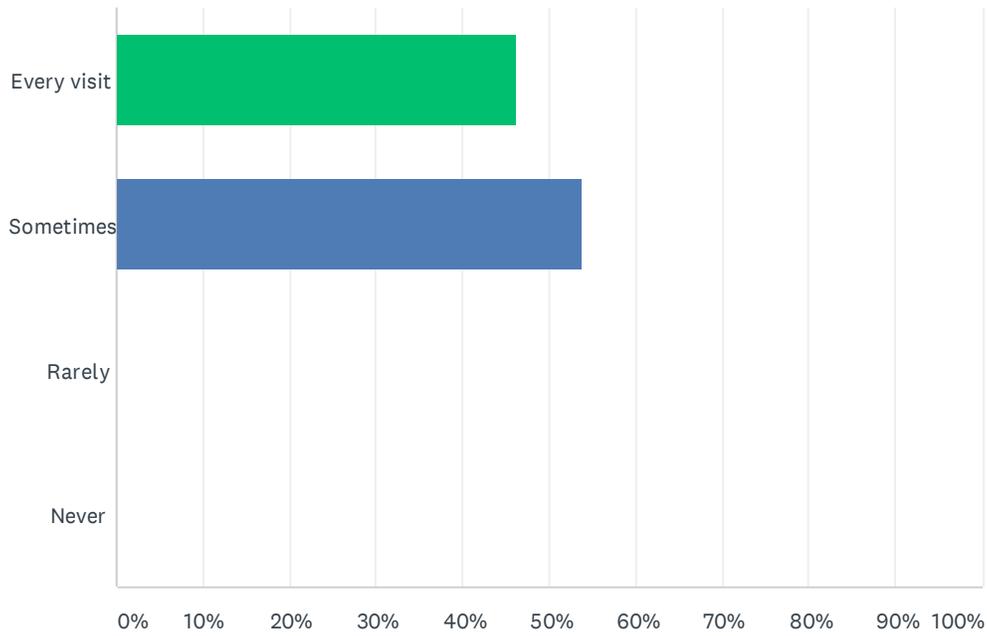
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|----------------|-----------|
| Every visit | 0.00% 0 |
| Sometimes | 61.54% 8 |
| Rarely | 38.46% 5 |
| Never | 0.00% 0 |
| TOTAL | 13 |

Q5 How often do you find places to park on traditional lots or side streets when visiting downtown?

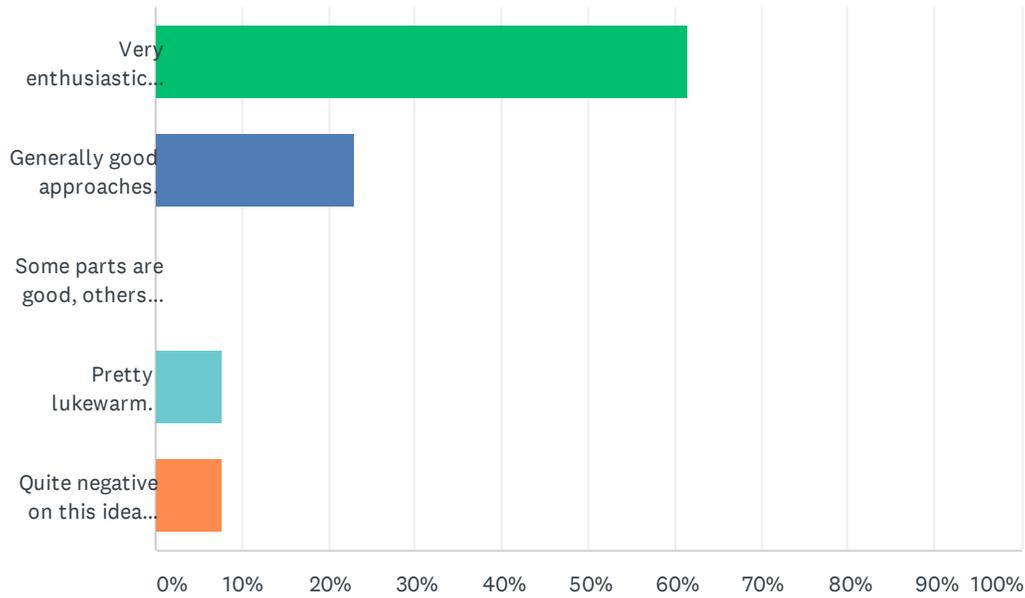
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|----------------|-----------|
| Every visit | 46.15% 6 |
| Sometimes | 53.85% 7 |
| Rarely | 0.00% 0 |
| Never | 0.00% 0 |
| TOTAL | 13 |

Q6 What is your reaction to the overall idea discussed of adding curb extensions to the western segment (6th Street - 2nd Street) of Main street?

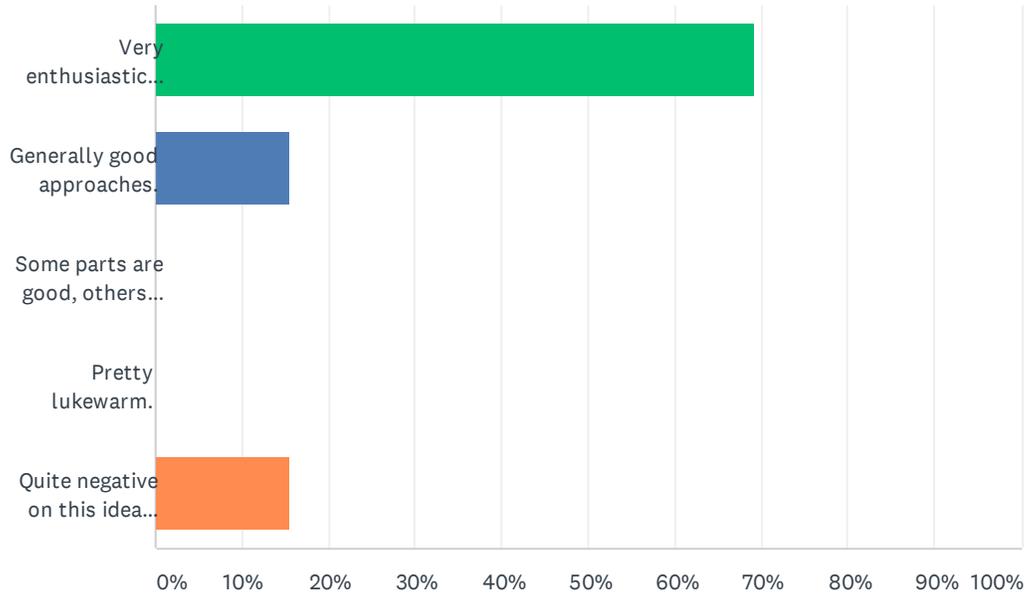
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 61.54% | 8 |
| Generally good approaches. | 23.08% | 3 |
| Some parts are good, others are doubtful. | 0.00% | 0 |
| Pretty lukewarm. | 7.69% | 1 |
| Quite negative on this idea – off the mark. | 7.69% | 1 |
| TOTAL | | 13 |

Q7 What is your reaction to the overall idea discussed for removing parallel parking and increasing the sidewalk width in the central segment of the corridor? (1st Street - Independence)

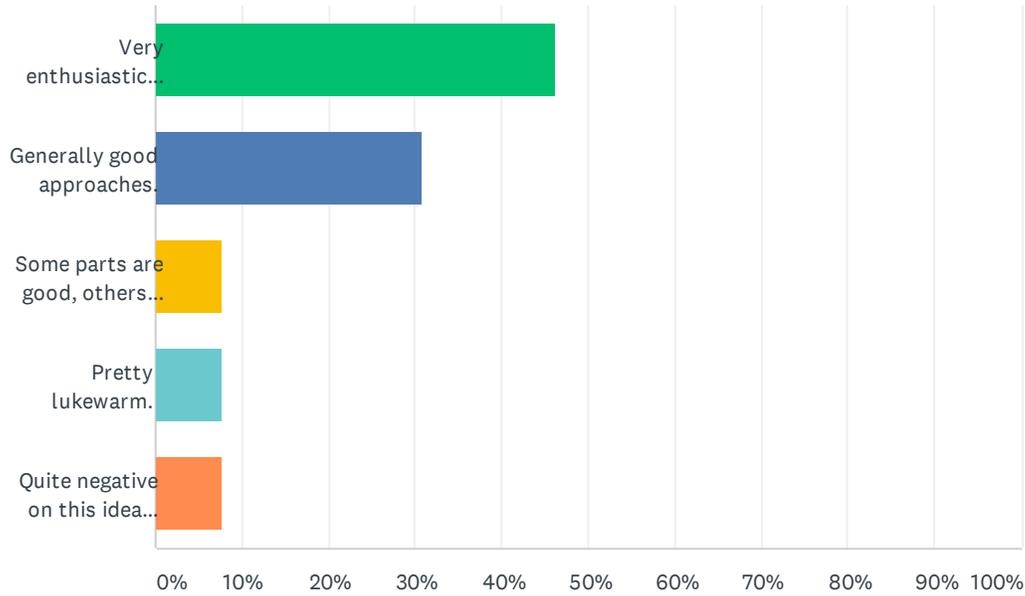
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 69.23% 9 |
| Generally good approaches. | 15.38% 2 |
| Some parts are good, others are doubtful. | 0.00% 0 |
| Pretty lukewarm. | 0.00% 0 |
| Quite negative on this idea – off the mark. | 15.38% 2 |
| TOTAL | 13 |

Q8 What is your reaction to the overall idea discussed for adding curb extensions and increasing sidewalk width to the eastern segment (6th Street - 2nd Street) of Main Street?

Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 46.15% | 6 |
| Generally good approaches. | 30.77% | 4 |
| Some parts are good, others are doubtful. | 7.69% | 1 |
| Pretty lukewarm. | 7.69% | 1 |
| Quite negative on this idea – off the mark. | 7.69% | 1 |
| TOTAL | | 13 |

Q9 Do you have any additional comments about the Main Street corridor?

Answered: 5 Skipped: 8

| # | RESPONSES | DATE |
|---|--|-------------------|
| 1 | Taking parking off of Main Street is a bad idea IMO. Repair the existing sidewalks and curbs | 9/21/2020 8:57 AM |
| 2 | No | 9/16/2020 9:50 AM |
| 3 | leave it alone | 9/16/2020 7:25 AM |
| 4 | Add more handicapped spaces | 9/16/2020 7:03 AM |
| 5 | If trees/branches are a concern due to traffic/ visual safety, why not try trellises, arbors, shrubs, bulbs as well as colorful historically correct awnings or overhangs. | 9/15/2020 7:13 PM |

Q1 Of all the recommendations and ideas we have presented, please list the three you like the most.

Answered: 9 Skipped: 0

| ANSWER CHOICES | RESPONSES |
|----------------|-----------|
| 1. | 100.00% 9 |
| 2. | 100.00% 9 |
| 3. | 100.00% 9 |

| # | 1. | DATE |
|---|--|-------------------|
| 1 | A network of bicycle paths that connect with the St Clair County trails/bike paths | 9/21/2020 6:05 PM |
| 2 | reorganization of parking off Main St | 9/21/2020 3:10 PM |
| 3 | Sidewalk expansion on Main St | 9/21/2020 2:18 PM |
| 4 | Developing bike paths and connecting to st Clair co system | 9/21/2020 1:36 PM |
| 5 | Reduce main st parking | 9/16/2020 5:00 PM |
| 6 | Railway Nucleus | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Creative parking ideas | 9/15/2020 7:32 PM |
| 9 | Railway park with stage and brew pub | 9/15/2020 7:18 PM |

| # | 2. | DATE |
|---|--|-------------------|
| 1 | Developing the Main Street- Railway Avenue nucleus | 9/21/2020 6:05 PM |
| 2 | additional sidewalks on Main & Church | 9/21/2020 3:10 PM |
| 3 | Historical Society creek project | 9/21/2020 2:18 PM |
| 4 | Focusing on the Railway nucleus to make that a gathering center with cafes, performance areas, etc. There is so much potential for that to become an attraction/ destination to our city | 9/21/2020 1:36 PM |
| 5 | Route 4/Jefferson Parking lot changes | 9/16/2020 5:00 PM |
| 6 | Old gas station converted to ice cream shop | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Embracing the historical aspects of the buildings & areas | 9/15/2020 7:32 PM |
| 9 | New and Wider sidewalks downtown with opportunity to eat | 9/15/2020 7:18 PM |

| # | 3. | DATE |
|---|--|-------------------|
| 1 | Developing a historic walking path along the Hog River. | 9/21/2020 6:05 PM |
| 2 | shared use paths & bike blvds | 9/21/2020 3:10 PM |
| 3 | Railway and Main redesign | 9/21/2020 2:18 PM |
| 4 | The pig creek historic path | 9/21/2020 1:36 PM |
| 5 | Proposed Ice Cream shop | 9/16/2020 5:00 PM |
| 6 | Bike path connection on west end of town to St Clair County network. (Would be great to be able to bike to Eckert's) | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | The incorporation of trees, flowers & green space. | 9/15/2020 7:32 PM |
| 9 | Historical district to help with more redevelopment | 9/15/2020 7:18 PM |

Q2 Which three address the most practical needs of Mascoutah?

Answered: 9 Skipped: 0

| ANSWER CHOICES | RESPONSES |
|----------------|-----------|
| 1. | 100.00% 9 |
| 2. | 100.00% 9 |
| 3. | 100.00% 9 |

| # | 1. | DATE |
|---|--|-------------------|
| 1 | Bike Path network starting with connecting Railway Avenue with South Tenth Street along the old railroad track. | 9/21/2020 6:05 PM |
| 2 | additional sidewalks | 9/21/2020 3:10 PM |
| 3 | Sidewalks along Main St | 9/21/2020 2:18 PM |
| 4 | Bike paths and connecting to St Clair county system to offer an alternate and sustainable mode of transportation | 9/21/2020 1:36 PM |
| 5 | Main st sidewalk improvements | 9/16/2020 5:00 PM |
| 6 | Railway Nucleus | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Parking issues | 9/15/2020 7:32 PM |
| 9 | Safer sidewalks downtown | 9/15/2020 7:18 PM |

| # | 2. | DATE |
|---|---|-------------------|
| 1 | Main Street -Railway Avenue core development | 9/21/2020 6:05 PM |
| 2 | more parking | 9/21/2020 3:10 PM |
| 3 | Refresh parking along Church St (more green space) | 9/21/2020 2:18 PM |
| 4 | Development of the Railway nucleus to revitalize the city's economy | 9/21/2020 1:36 PM |
| 5 | Making main st more ped friendly | 9/16/2020 5:00 PM |
| 6 | Convert old gas station into ice cream shop. | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Using Postal Mill as a viable space | 9/15/2020 7:32 PM |
| 9 | More dining/entertainment downtown | 9/15/2020 7:18 PM |

| # | 3. | DATE |
|---|--|-------------------|
| 1 | Hog River walking path | 9/21/2020 6:05 PM |
| 2 | bike / ped paths | 9/21/2020 3:10 PM |
| 3 | Revitalize areas south of Main St | 9/21/2020 2:18 PM |
| 4 | Widening the sidewalks and eliminating parallel parking on Main Street | 9/21/2020 1:36 PM |
| 5 | more bike paths | 9/16/2020 5:00 PM |
| 6 | Connect bike path on west end of town to St. Clair County network. | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Addressing the awful sidewalks (or lack of) along main street. | 9/15/2020 7:32 PM |
| 9 | More bike trails | 9/15/2020 7:18 PM |

Q3 What is the most important thing to do first?

Answered: 9 Skipped: 0

| # | RESPONSES | DATE |
|---|---|-------------------|
| 1 | Bike path network should be first and is the least expensive. It will be a catalyst to attract people to move around the City while embracing an active lifestyle. | 9/21/2020 6:05 PM |
| 2 | bike/ped paths | 9/21/2020 3:10 PM |
| 3 | Presentation to get support for new sidewalks on Main | 9/21/2020 2:18 PM |
| 4 | Bike path recommendations—I think it would attract future homeowners if we were more connected to the overall network in the area. Right now, I feel that we are isolated in that regard. | 9/21/2020 1:36 PM |
| 5 | Main street improvement from Route 4 to 2nd street | 9/16/2020 5:00 PM |
| 6 | Railway Nucleus | 9/16/2020 9:55 AM |
| 7 | nothing - leave it all alone | 9/16/2020 7:27 AM |
| 8 | Main street sidewalks. Then parking. | 9/15/2020 7:32 PM |
| 9 | Railway park with dining/nightlight | 9/15/2020 7:18 PM |

Q4 How would you describe success in this project?

Answered: 8 Skipped: 1

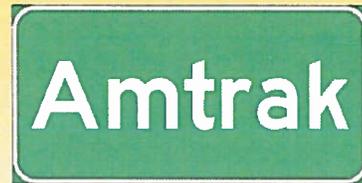
| # | RESPONSES | DATE |
|---|--|-------------------|
| 1 | Implementing the plan by connecting with existing paths and completing the connection between South Railway and South 10th Street will bring outdoor life to the city by pedestrians and bicycle riders. When people get out and move they will be ready to engage I. Use if the downtown are and the historic Hog River walkway too | 9/21/2020 6:05 PM |
| 2 | Completion of one project every 3 years to complete the 3 most practical projects | 9/21/2020 2:18 PM |
| 3 | I've already seen an increase in walkers and bike riders since the berm path was built. If we had a bigger network, I would foresee many more people out and about on these paths, increasing the overall health and well being of our residents, as well as also increasing commuter options by bike. It would result in a healthier community. | 9/21/2020 1:36 PM |
| 4 | If it actually happens | 9/16/2020 5:00 PM |
| 5 | Downtown becomes a vibrant district that attracts customers from the entire region and property taxes are not raised as a result of this. | 9/16/2020 9:55 AM |
| 6 | leave it all alone | 9/16/2020 7:27 AM |
| 7 | When we see more hometown citizens using the sidewalks, riding their bikes & patronizing our local businesses. | 9/15/2020 7:32 PM |
| 8 | More vibrant downtown | 9/15/2020 7:18 PM |

Appendix 3

Freeway Generator Signs Policy

Signing to Traffic Generators and Motorist Services

July 2014



Illinois Department of Transportation



Illinois Department of Transportation

Memorandum

To: All Operations Engineers
From: Justan Mann 
Subject: Signing to Traffic Generators and Motorist Services
Date: July 16, 2014

Guide signs for traffic generators and motorist services are important in providing valuable directional guidance information to motorists. However, it is necessary to develop criteria in determining which facilities are eligible for signing so that the facilities displayed are the ones generating the greatest need for information. This criteria is also important to limit the number of signs installed to minimize confusion that may occur when motorists are presented more information than they can comprehend.

Attached is an updated policy for signing to traffic generators and motorist services which supersedes the policy of May 1984. This policy incorporates a few new classifications of facilities as well as some criteria changes to existing eligible classifications.

For any freeway signing requests associated with recreational, cultural, or historical facilities, the district offices may inform the facility of the potential for signing through the Interstate Attractions Logo Signing Program, especially if the facility does not meet the criteria of the attached policy.

If you have any questions please contact Kyle Armstrong at 217-782-7414.

Attachment

cc: Aaron Weatherholt
All Regional Engineers

SIGNING TO TRAFFIC GENERATORS AND MOTORIST SERVICES ON THE STATE HIGHWAY SYSTEM

TRAFFIC GENERATORS

A. General

Criteria are necessary for determining which traffic generators and motorist services are displayed on supplemental guide signs so that the facilities shown are the ones generating the greatest need for information on the part of motorists unfamiliar with an area. Such criteria must also limit the number of signs installed to minimize the confusion that may occur when motorists are presented more information than they can rapidly comprehend. It also assures fair and consistent treatment of requests for signing to such facilities.

The establishment of criteria, however, does not mean that information concerning all facilities which meet the criteria will automatically be shown on supplemental guide signs. Signing for traffic generators is considered as secondary to basic regulatory, warning, and directional signing. Therefore, before a supplemental guide sign for a traffic generator or motorist service can be installed, sufficient longitudinal space along the highway must exist to accommodate the placement of the sign without interfering or conflicting with other necessary traffic control devices. Consideration must also be given to such factors as the space available along highways and the congestion of the area.

Signing for a traffic generator, which qualifies under the criteria in Table A, B, C, or D, should be installed only after a request is received from an official representative of the facility.

Where interchanges are numbered, the interchange number (and suffix) shall be used for the action line of all traffic generator signs. The message "NEXT RIGHT (LEFT)" shall be used where interchange numbers are not posted. When a sign for more than one traffic generator is installed in advance of a dual-exit interchange and the generators are in both directions at the interchange, the sign legend showing the generator(s) available and the exit information for the first exit shall be separated from the legend showing the generator(s) available and the exit information for the second exit by a horizontal border strip. The action line of the upper half shall show the exit number and suffix for the first exit, and the action line of the lower half shall show the exit number and suffix for the second exit. Where there are not exit numbers, the upper half shall have the message "NEXT RIGHT (LEFT)" and the lower half shall be "SECOND RIGHT (LEFT)."

Signing should be placed at the intersection or interchange providing the shortest and/or best route to the facility. For example, if the shortest route to a major facility leads through a residential area while another route to the facility is available which leads over a major arterial, the second route should be considered as the "best" route for signing. If the best route is longer in distance to the facility, the qualifying mileage for the shorter routing is to be used for any mileage criteria.

B. Definitions

The following words or phrases when used in this Part shall have the meanings ascribed to them below.

“Action Line” – That portion of the sign message giving directional information to the motorist.

“Advance” – A sign is ahead or in advance of another when it is at a greater distance than the other from the crossroad or intersection being signed for.

“Conventional Highway” – All State highways that do not have full access control or grade separated interchanges.

“Freeways” – Those State highways with full access control, together with those sections where the highway has grade separated interchanges.

“Guide Sign” – A sign that shows route designations, destinations, directions, distances, services, points of interest and other geographical, recreational, or cultural information.

“Highways” – Any public way for purposes of vehicular travel, including the entire area within the right of way.

“Hospital” - Any facility devoted to the diagnosis and treatment or care of 2 or more unrelated persons admitted for overnight stay or longer in order to obtain medical, including obstetric, psychiatric and nursing, care of illness, disease, injury, infirmity, or deformity care.

“Interchange” – A system of interconnecting roadways providing for the movement of traffic between two roadways on different levels.

“Motorist Service” – A facility, as classified in Table B or D, that offers information or a service directly related to the motorists’ needs.

“Ramp Terminal” – That point at which a ramp from a freeway ends at its connecting roadway.

“Traffic Generator” – Any land use development, as classified in Table A, C, or C-1, that serves as the destination for motor vehicle trips.

“Urban area” - a municipality with a population of 50,000 or more, and its contiguous urban fringe with a population density of 1,000 or more inhabitants per square mile; or a municipality with at least a population of 25,000 together with other contiguous places (incorporated or unincorporated) each with a population density of 1,000 or more inhabitants per square mile, which altogether constitutes for general socioeconomic purposes a single community with a combined population of at least 50,000 inhabitants.

C. Conventional Highways

The specific qualifying criteria for each type of traffic generator for which signing may be installed are shown in Table A. In addition, certain types of facilities may qualify for TODS signing under Part 541, TOURIST ORIENTED DIRECTIONAL SIGNING PROGRAM (92 Ill. Adm. Code 541) of the Illinois Department of Transportation Rules and Regulations.

Those values that vary annually, such as student enrollment or annual attendance, should be based upon the average value for the two previous years or, in the case of new or expanded facilities, on engineering judgment based on similar facilities.

Signing for a traffic generator should only be allowed at a given marked State highway intersection when the generator can be reached without crossing another marked State highway. Exceptions may be made where evidence of operational problems on the State highway would result without such signing. For the purposes of signing, unmarked State highways will be treated as local roads however, the mileage criteria is to be measured from the marked State highway. Any trailblazing necessary on an unmarked State highway to direct motorists from the marked State highway to the facility is to be placed by the department. Trailblazing on the local road system is to be placed by the appropriate local agency.

A traffic generator shall not be eligible for signing if the facility property abuts the marked State highway and the facility or a sign at the site of the facility is visible to the motorists in sufficient time to safely exit the highway. A facility may abut an unmarked State highway.

When the traffic generator does not abut the marked State highway, supplemental guide signs may be erected in advance of the street or road that provides the most logical and convenient route to the facility. The signing may be split with the sign in one direction being at a different intersection than the sign in the other direction. The distance from the intersection to the facility shall be no more than five road miles in rural areas or one road mile in urban areas.

Information relating to the traffic generator shall not be displayed on a supplemental guide sign until signing has been installed along the intersecting minor road and other roads as may be necessary to adequately direct the motorist from the State highway to the facility.

Signing on conventional highways may also be necessary to guide motorists from freeways to traffic generators located away from the freeway. Criteria for this type of signing are included under "Freeways" in this policy.

Additional Criteria

- BUS STATIONS, AIRPORTS and TRAIN STATIONS should be signed for using the standard symbol signs for such facilities (I-5 Airport, I-6 Bus Station, I-7 Train Depot) and the appropriate arrow plaque. The name of the airport (e.g. CAPITAL) and the name of the bus service (e.g. GREYHOUND) should be used on a plaque below the symbols. The I-7a Amtrak plaque should be used on a plaque below the I-7 sign.
- OFF-TRACK BETTING (OTB) FACILITIES and RIVERBOAT CASINOS should be signed for using the standard symbol signs for such facilities (D11-I101 Riverboat Casino, D11-I102 OTB) and the appropriate arrow plaque. LAND-BASED CASINOS should be signed for with the legend "CASINO" and the appropriate arrow plaque. The signs shall not include the actual names of the facilities.

D. Freeways

The specific criteria for each type of traffic generator for which signing may be installed on freeways are shown in Table C. In addition, some attractions which may not qualify for signing under this policy may qualify under the Attraction Signing program which is jointly administered by the Illinois Office of Tourism. This program falls under the Specific Service category of the MUTCD but includes categories of attractions which range from traffic generators to motorist services. Tourism Attraction LOGO signs are governed by Part 543, TOURISM ATTRACTION SIGNING PROGRAM (92 Ill. Adm. Code 543) of the Illinois Department of Transportation Rules and Regulations. Note that a traffic generator qualifying under both this policy and the Attraction policy cannot have both traffic generator signs and attraction signs on the same freeway.

Those values in the criteria that vary annually, such as student enrollment or annual attendance, should be based upon the average value for the two previous years or, in the case of new or expanded facilities, on engineering judgment based upon similar facilities.

A specific qualifying traffic generator shall only be signed for on one supplemental guide sign along a given freeway in each direction of travel. However, a specific qualifying traffic generator may be signed for from more than one freeway as long as the criteria are met for each freeway. The signing may be split with the sign in one direction being at a different interchange than the sign in the other direction.

Signing shall only be allowed when the generator can be reached without crossing another freeway or tollway. Exceptions may be made where evidence of operational problems on the freeway would result without such signing.

Not more than two traffic generators should be displayed on a single supplemental guide sign. The sign background may be divided into two colors where two differing types of generators are shown on the same sign (e.g. the portion of a sign directing to a state park shall be shown in brown and the portion of the same sign directing to a college shall be shown in green). When this is done, the sign should be designed as two panels, one above the other, with two action lines.

In no case shall information relating to traffic generators be displayed on a supplemental guide sign until signing has been installed along the interchanging road, as well as other roads where necessary, to adequately direct the motorist from the freeway to the facility which the signs are installed.

Signing for a traffic generator that is open for a limited time period each year and that meets the annual criteria within a shorter time (such as a county fair) should be displayed on a temporary supplemental guide sign installed near the beginning and removed at the end of the limited period. However, if the facility hosts frequent events throughout the year requiring temporary signing more than twice a year, permanent signing may be installed.

Where interchanges are numbered, the interchange number (and suffix) shall be used for the action line of all traffic generator signs. The message "NEXT RIGHT (LEFT)" shall be used where interchange numbers are not posted.

When a sign for more than one traffic generator is installed in advance of a dual-exit interchange and the generators are in both directions at the interchange, the sign legend showing the generator(s) available and the exit information for the first exit shall be separated from the legend showing the generator(s) available and the exit information for the second exit by a horizontal border strip. The action line of the upper half shall show the exit number and suffix for the first exit, and the action line of the lower half shall show the exit number and suffix for the second exit. Where there are not exit numbers, the upper half shall have the message "NEXT RIGHT (LEFT)" and the lower half shall be "SECOND RIGHT (LEFT)."

Additional Criteria

- AIRPORTS should be signed for on supplemental guide signs using the airport name along with the standard I-5 Airport symbol. The airport name should generally be limited to the name of the community or airfield and the word "Airport" unless doing so would cause confusion with other nearby facilities. For example, "Richardson Memorial Regional Airport" should be shortened to "Richardson Airport," "Memorial Airport," or "Memorial" as necessary. The I-5 symbol (without the airport name) may also be used similar to a route shield on the exit direction (take off) sign. Trail blazing should be done with standard I-5 signs.
- OFF-TRACK BETTING (OTB) FACILITIES and RIVERBOAT CASINOS should be signed for using the legend Off-Track Wagering Facility or Riverboat Casino with the appropriate symbol. LAND-BASED CASINOS should be signed for with the legend "CASINO". As an alternate, these facilities may be considered for signing under the TOURISM ATTRACTION SIGNING PROGRAM (92 Ill. Adm. Code 543). Trailblazing should use the standard symbol signs for such facilities (D11-I101 Riverboat Casino, D11-I102 OTB) or a sign with the legend "CASINO" (Land-Based Casinos) and the appropriate arrow plaque. The signs shall not include the actual names of the facilities.
- The full-time equivalent student (FTE) criterion for educational institutions may be obtained from the State Board of Higher Education at <http://www.ibhe.org/EnrollmentsDegrees/Search.aspx> and should not include students enrolled in online classes who are not present at the campus. For a satellite campus, the FTE shall come from those students having that campus as their primary campus and the request for signing should come from the primary campus administration rather than the satellite itself.
- AMTRAK STATIONS may be signed for on supplemental guide signs using the word "Amtrak" only in white on a green background without the Amtrak symbol. Trailblazing signing should include standard I-7 signs with I-7a Amtrak plaques.

MOTORIST SERVICES

A. General

Signing should be placed at the intersection or interchange providing the shortest and/or best route to the facility. For example, if the shortest route to a major facility leads through a residential area while another route to the facility is available which leads over a major arterial, the second route should be considered as the "best" route for signing. If the best route is longer in distance to the facility, the qualifying mileage for the shorter routing is to be used for any mileage criteria. In signing for hospitals or free-standing emergency centers, the best route would be the one normally providing the fastest route to the facility taking into account such factors as traffic congestion, directness, rail road crossings, lift bridges, etc.

B. Conventional Highways

The specific qualifying criteria for each type of motorist service are shown in Table B.

Gas, food, and lodging facilities shall not be signed for on conventional highways, except as provided for under Part 541 TOURIST ORIENTED DIRECTIONAL SIGNING (TODS) PROGRAM (92 Ill. Adm. Code 541), or as trailblazers under the BUSINESS LOGO SIGNING PROGRAM (92 Ill. Adm. Code 542) or the TOURISM ATTRACTION SIGNING PROGRAM (92 Ill. Adm. Code 543) of the Illinois Department of Transportation Rules and Regulations.

Qualifying hospitals should be signed with a Hospital symbol ("H") sign as opposed to the word message "HOSPITAL." The hospital name may be displayed on a plaque above the hospital symbol sign where two or more qualifying hospitals are served by the same route.

Tourist Information Centers should be signed with the message "TOURIST INFORMATION" (or TOURIST INFO or WELCOME CENTER) with appropriate directional/mileage information and may carry the name of the primary area/site for which the information is provided such as "NEWPORT TOURIST INFO."

Camping facilities shall carry only the message "CAMPING" with appropriate directional/mileage information. The name of the campground shall not be shown.

Except as may be provided for under the TODS, LOGO or Attraction program rules, signing for a motorist service shall only be allowed at a given marked State highway intersection when the service can be reached without crossing another marked State highway. Exceptions may be made where evidence of operational problems on the State highway would result without such signing. For the purposes of signing, unmarked State highways will be treated as local roads. However, the mileage criterion is to be measured from the marked State highway. Any trailblazing necessary on an unmarked State highway to direct motorists from the marked State highway to the facility is to be placed by the department. Trailblazing on the local road system is to be placed by the appropriate local agency.

C. Freeways

The specific criteria for each type motorist service for which signing may be installed on freeways are shown in Table D.

Business LOGO (Specific Service) signs (Gas, Food, Lodging, Camping and 24-hour Pharmacy) are governed by Part 542, BUSINESS LOGO SIGNING PROGRAM (92 Ill. Adm. Code 542) of the Illinois Department of Transportation Rules and Regulations. Tourism Attraction LOGO signs are governed by Part 543, TOURISM ATTRACTION SIGNING PROGRAM (92 Ill. Adm. Code 543) of the Illinois Department of Transportation Rules and Regulations.

A sign carrying the message "GAS, LODGING, FOOD (or ALL SERVICES) NEXT XX INTERCHANGES (or MILES)" may be installed in advance of urban areas where Business LOGO Signs are precluded under 92 Ill. Adm. Code 542.

Signs for Tourist Information Centers shall carry the message "TOURIST INFORMATION (or TOURIST INFO or WELCOME CENTER)" with appropriate directional information and may carry the name of the primary area/site for which the information is provided such as "NEWPORT TOURIST INFO."

Qualifying hospitals should be signed with a Hospital symbol ("H") sign as opposed to the word message "HOSPITAL." The hospital name may be displayed on a plaque above the hospital symbol sign where two or more qualifying hospitals are served by the same route. Hospital symbol signs should be installed separately from other motorist service signs.

**TABLE A
TRAFFIC GENERATORS - CONVENTIONAL HIGHWAY**

| CLASS | CRITERIA (Except as noted, must be within 5 miles of intersection in rural areas and 1 mile in urban areas) | EXAMPLES | SIGN COLOR |
|---|--|---|-------------------------------------|
| Transportation Facilities | Airports: Unrestricted airport identified on the Official State Highway Map Railroad Station: Served by AMTRAK Intercity Bus Terminal: 50 or more buses per week | | White on green (see text in policy) |
| Educational Institutions | 500 full-time equivalent students at a post-high-school or special needs institution | College/University/Grad School Junior or Community College Medical/Seminary/Trade School School for disabled | White on green |
| Public Library | HOURS OPEN PER WEEK BY POPULATION SERVED (Up to 1 mile in all areas) >25,000 5,000 - 24,999 1,500 - 4,999 <1,500 64 Hrs 43 Hrs 25 Hrs 18 Hrs | | White on green I-8 symbol |
| Correctional Institutions | Federal or State | Correctional Center Youth/Boot Camp State-wide DOC training facility | White on green |
| Health Care Facilities | Mental health residential care facility licensed by the State. Hospitals/medical facilities/clinics with 500 or more employees and any VA Hospital | Mental health Zone Center Hospital/Medical Facility/VA Hospital/Clinic | White on green |
| Miscellaneous Facilities | Local, State or Federal facility with 500 or more employees or assigned personnel. Air/Army National Guard Armories and Secretary of State Driver's License Facilities qualify with any number of employees or personnel. | Atomic Energy Commission Facility State/Federal Research Facility Military Base/National Guard Armory Driver's License Facility Ammunition Plant | White on green |
| Historical, Recreational, and Cultural Facilities | ANNUAL ATTENDANCE REQUIRED BASED ON POPULATION WITHIN 25 AIR MILES OF FACILITY Pop: >1,000,000 100,000 - 1,000,000 <100,000 Attend: <u>200,000</u> 100,000 50,000 State Parks, National Parks and staffed State Historic Sites in areas of under 100,000 population may be signed for to a maximum distance of 20 miles. National Cemeteries qualify without attendance criteria. Outdoor recreational facility provided by youth, church or social service organization having an annual attendance of at least 5,000 and open year-round. Historical markers erected by the Illinois State Historical Society may be signed for without attendance criteria. Special local historical attraction open year-round with no charge to the public may be signed without attendance criteria. | Amusement Park/Stadium/Fairground Arena/Auditorium/Convention Center Museum/Archeological Site Race Track/OTB/Casino Nature/Forest Preserve Arboretum/Aquarium/Zoo State/National Park State/National Historic Site National Cemetery Boy/Girl Scout Camp/Church Camp 4-H Camp/Special Needs Camp YMCA/YWCA Camp State historical marker Historic site Historic memorial | White on brown |

NOTE: Facilities of types not included in the "EXAMPLES" do not normally qualify for signing. Such facilities include, but are not limited to, shopping/discount centers or malls, auto repair facilities, auto/truck dealerships/malls, antique malls, health clubs, golf courses, bowling alleys, retirement/nursing homes, "members only" facilities of any kind, theaters, and municipal/county governmental facilities.

**TABLE B
MOTORIST SERVICES - CONVENTIONAL HIGHWAY**

| CLASS | CRITERIA | SIGN COLOR | | | | | | |
|--|--|----------------|---------------------|---------------|--------|---------|---------|-------------------------------|
| State Police and County Sheriff Headquarters | Must be staffed and open to the public 24 hours a day. Shall not be more than 1 mile from the intersection in urban areas and 5 miles in rural areas. | White on blue | | | | | | |
| Hospitals | <p>Must be listed on the Illinois Dept. of Public Health website http://www.hfsrb.illinois.gov/HospProf_ABR.htm and must be located within the following distances from the intersection:</p> <p align="center">POPULATION OF METROPOLITAN AREA</p> <table border="0" data-bbox="516 751 1159 810"> <tr> <td align="center">Over 1,000,000</td> <td align="center">100,000 - 1,000,000</td> <td align="center">Under 100,000</td> </tr> <tr> <td align="center">1 mile</td> <td align="center">2 miles</td> <td align="center">5 miles</td> </tr> </table> | Over 1,000,000 | 100,000 - 1,000,000 | Under 100,000 | 1 mile | 2 miles | 5 miles | White on blue D9-2 "H" symbol |
| Over 1,000,000 | 100,000 - 1,000,000 | Under 100,000 | | | | | | |
| 1 mile | 2 miles | 5 miles | | | | | | |
| Tourist Information Centers | <p>The facility must be within 1 1/2 miles from the intersection in urban areas over 1,000,000 in population and 3 miles in other areas. It may be operated by chambers of commerce, civic groups, local agencies, or private enterprises. The operators of the facility shall make no attempt to promote their organization to groups seeking information, although those operating the facility may be identified.</p> <p>Information must be available at least 8 hours per day, 7 days per week, and for a minimum of 4 continuous months out of the year. Signs must be removed or covered during the off-season.</p> <p>Adequate off-street parking must be provided and the facility must be accessible to persons with disabilities.</p> <p>The information must be provided free of charge, with no attempt to promote any one tourist service/site over another. Souvenirs and other items of interest to tourists may be offered for sale.</p> <p>The center shall not be on the premises of a commercial business supplying motorist services such as gas, food, lodging, or camping. However, it may be located on grounds occupied by a number of businesses such as a shopping mall either as a separate facility or as a separate store within the mall. Access to the center must be available without passing through a commercial business.</p> | White on blue | | | | | | |
| Camping Facilities | Any campground open to the general public which is in an area not qualifying for TODS signing and having a minimum of 20 spaces, toilet, telephone, and adequate supply of drinking water, and which is open at least 6 months of the year may be signed providing it is less than 10 road miles from the intersection in rural areas and 1 mile in urban areas. | White on blue | | | | | | |

**TABLE C
TRAFFIC GENERATORS - FREEWAY**

| CLASS | CRITERIA | DISTANCE FROM INTERCHANGE BY POPULATION OF AREA WITHIN 25 AIR MILES OF FACILITY | | | EXAMPLES | SIGN COLOR |
|--|--|---|-------------------------|----------|--|--|
| | | >1,000,000 | 100,000 to 1,000,000 | <100,000 | | |
| Transportation Facilities | Any municipal airport shown on official State map having regularly scheduled commercial flights or acting as a major freight terminus | 2 miles | 3 miles | 5 miles | Airports | White on green with I-5 Airport symbol |
| | Communities with Amtrak station | See www.amtrak.com for list of communities with stations | | | Amtrak | White on green |
| Educational Institutions | 500 full-time equivalent students at a post-high- school institution. | 2 miles | 3 miles | 5 miles | Col./Univ./Grad School Junior/Community Col. Seminary/Medical School Trade School School for disabled | White on green |
| Military Facilities | 5,000 employees or assigned personnel | 5 miles | 7.5 miles | 10 miles | Ammunition Plant, Military Base | White on green |
| Miscellaneous Facilities (Includes hospitals or medical facilities/clinics which do not qualify as motorist services) | 2,000 employees at facility, clinic, or complex VA Hospitals qualify without employee criteria | 2 miles | 3 miles | 5 miles | Atomic Energy Commission/State or Federal Research Facility VA Hospital Medical Facility Clinic | White on green |
| Historical, Recreational, and Cultural Facilities <i>other than State/National Parks and State/National Historic Sites.</i> | Annual Attendance | 200,000 attendance up to 1 mile of distance from the interchange plus 20,000 for each additional mile up to 5 miles plus 30,000 for each additional mile over 5 miles with a maximum distance of 10 miles. | | | Amusement Park Arena/Auditorium/ Convention Center/ Stadium, Fairground, Museum, Nature Preserve, Race Track/OTB/Casino | White on brown |
| State/National Parks and State/National Historic Sites. | Annual attendance/distance | (See Table C-1) | | | Parks, Recreation Areas and Historic Sites which are State or Federally operated. | White on brown |

NOTE: Facilities of types not included in the "EXAMPLES" do not normally qualify for traffic generator signing. Such facilities include, but are not limited to, shopping/discount centers or malls, auto repair facilities, auto/truck dealerships/malls, antique malls, health clubs, golf courses, bowling alleys, retirement/nursing homes, "members only" facilities of any kind, theaters, and municipal/county governmental facilities. Some facilities may be placed on a Tourism Attraction sign rather than a traffic generator sign if space is available and it meets the criteria for such signs.

**TABLE C-1
TRAFFIC GENERATORS - FREEWAY
State/National Parks and State/National Historic Sites**

| CLASS | CRITERIA | DISTANCE FROM INTERCHANGE BY COUNTY | | | EXAMPLES | SIGN COLOR | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|----------------------------|---|-------------------------|-------------------|------------------|------------|-------------------|----|--------------------|----|-----------------|----|------------------|----------|------------------|----|------------------|----|------------------|----|-----------------|----|-----------------|----|-----------------|----------|----------------|----|----------------|----|----------------|----|----------------|----|----------------|-----------|----------------|----|----------------|----|----------------|----|--|----------------|
| | | Cook 5 miles | DuPage/Lake 10 miles | Other 30 miles | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| State/National Parks and State/National Historic Sites. See Table C for Historical, Recreational, and Cultural Facilities other than State/National Parks and State/National Historic Sites | Annual attendance/distance | State/National Parks 15,000 min. attendance State/National Historic Sites 5,000 min. attendance. Staffed No more than 2 facilities may be signed for on a single sign at a given interchange. If priority must be established, it shall be based upon the following attendance and distance point criteria contained in the Tourism Attraction Signing Program rule (92 Ill. Adm. Code 543.400). <u>Annual Attendance:</u> <table> <tr><td>Less than 50,000</td><td>10 points</td></tr> <tr><td>50,000 to 149,999</td><td>20</td></tr> <tr><td>150,000 to 249,999</td><td>30</td></tr> <tr><td>250,000 or more</td><td>35</td></tr> </table> <u>Distance from interchange (Except Cook, DuPage and Lake Counties):</u> <table> <tr><td>25.1 to 30 miles</td><td>5 points</td></tr> <tr><td>20.1 to 25 miles</td><td>10</td></tr> <tr><td>15.1 to 20 miles</td><td>15</td></tr> <tr><td>10.1 to 15 miles</td><td>20</td></tr> <tr><td>5.1 to 10 miles</td><td>23</td></tr> <tr><td>5 miles or less</td><td>25</td></tr> </table> <u>Distance from interchange (DuPage and Lake Counties):</u> <table> <tr><td>9.1 to 10 miles</td><td>5 points</td></tr> <tr><td>7.1 to 9 miles</td><td>10</td></tr> <tr><td>5.1 to 7 miles</td><td>15</td></tr> <tr><td>1.1 to 5 miles</td><td>20</td></tr> <tr><td>1 mile or less</td><td>25</td></tr> </table> <u>Distance from interchange (Cook County):</u> <table> <tr><td>4.1 to 5 miles</td><td>10 points</td></tr> <tr><td>3.1 to 4 miles</td><td>15</td></tr> <tr><td>1.1 to 3 miles</td><td>20</td></tr> <tr><td>1 mile or less</td><td>25</td></tr> </table> | | | Less than 50,000 | 10 points | 50,000 to 149,999 | 20 | 150,000 to 249,999 | 30 | 250,000 or more | 35 | 25.1 to 30 miles | 5 points | 20.1 to 25 miles | 10 | 15.1 to 20 miles | 15 | 10.1 to 15 miles | 20 | 5.1 to 10 miles | 23 | 5 miles or less | 25 | 9.1 to 10 miles | 5 points | 7.1 to 9 miles | 10 | 5.1 to 7 miles | 15 | 1.1 to 5 miles | 20 | 1 mile or less | 25 | 4.1 to 5 miles | 10 points | 3.1 to 4 miles | 15 | 1.1 to 3 miles | 20 | 1 mile or less | 25 | State or Federally operated Parks, Recreation Areas and Historic Sites | White on brown |
| Less than 50,000 | 10 points | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 50,000 to 149,999 | 20 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 150,000 to 249,999 | 30 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 250,000 or more | 35 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25.1 to 30 miles | 5 points | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 20.1 to 25 miles | 10 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 15.1 to 20 miles | 15 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 10.1 to 15 miles | 20 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5.1 to 10 miles | 23 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 miles or less | 25 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 9.1 to 10 miles | 5 points | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7.1 to 9 miles | 10 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5.1 to 7 miles | 15 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1.1 to 5 miles | 20 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 mile or less | 25 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4.1 to 5 miles | 10 points | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3.1 to 4 miles | 15 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1.1 to 3 miles | 20 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 mile or less | 25 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

NOTE: Historical, Recreational, and Cultural Facilities *including State/National Parks and State/National Historic Sites shown in this table* fully meeting the attendance and distance criteria shown in Table C shall be considered as having 65 points for priority purposes.

**TABLE D
MOTORISTS SERVICES - FREEWAY**

| CLASS | CRITERIA | SIGN COLOR | | | | | | |
|-----------------------------|---|---|---------------------|---------------|--------|---------|---------|-------------------------------|
| State Police Headquarters | Must be within 1 1/2 miles from the interchange in urban areas over 1,000,000 population and 3 miles in other areas. | White on blue | | | | | | |
| Hospitals | <p>Must be the closest hospital listed as providing comprehensive or basic emergency medical service as listed on the Illinois Dept. of Public Health website http://www.hfsrb.illinois.gov/HospProf_ABR.htm and located within the following distances from the interchange. A hospital offering standby emergency medical services as listed on this website may be signed at an interchange where the next interchange with a hospital offering comprehensive or basic emergency service is more than 25 miles away.</p> <p align="center">POPULATION OF METROPOLITAN AREA</p> <table border="0" data-bbox="462 846 1057 898"> <tr> <td align="center">Over 1,000,000</td> <td align="center">100,000 - 1,000,000</td> <td align="center">Under 100,000</td> </tr> <tr> <td align="center">1 mile</td> <td align="center">2 miles</td> <td align="center">5 miles</td> </tr> </table> | Over 1,000,000 | 100,000 - 1,000,000 | Under 100,000 | 1 mile | 2 miles | 5 miles | White on blue D9-2 "H" symbol |
| Over 1,000,000 | 100,000 - 1,000,000 | Under 100,000 | | | | | | |
| 1 mile | 2 miles | 5 miles | | | | | | |
| Tourist Information Centers | <p>The facility must be within 1 1/2 miles from the interchange in urban areas over 1,000,000 in population and 3 miles in other areas. It may be operated by chambers of commerce, civic groups, local agencies, or private enterprises. The operators of the facility shall make no attempt to promote their organization to groups seeking information, although those operating the facility may be identified.</p> <p>Information must be available at least 8 hours per day, 7 days per week, and for a minimum of 4 continuous months out of the year. Signs must be removed or covered during the off-season.</p> <p>Adequate off-street parking must be provided and the facility must be accessible to persons with disabilities.</p> <p>The information must be provided free of charge, with no attempt to promote any one tourist service/site over another. Souvenirs and other items of interest to tourists may be offered for sale.</p> <p>The center shall not be on the premises of a commercial business supplying motorist services such as gas, food, lodging, or camping. However, it may be located on grounds occupied by a number of businesses such as a shopping mall either as a separate facility or as a separate store within the mall. Access to the center must be available without passing through a commercial business.</p> | White on blue (If it is operated in conjunction with another facility which qualifies for signing, such as a museum, a white on blue plaque may be mounted below the sign for the other facility.) | | | | | | |



This white paper addresses land use and urban design issues in the Main Street study area in Mascoutah, Illinois. The study area extends one block north and south of Main Street between Mine Road, the east corporate limits of the community and 6th Street. This paper is part of an effort led by East-West Gateway and the City of Mascoutah that includes four specialized studies addressing markets, transportation, environment, and land use/urban design. The overall effort included three separate phases: 1) existing conditions analysis, 2) overall principles, and 3) concepts with recommendations. Each phase

concluded with a narrated presentation posted on the city's website with opportunities for public viewers to provide reactions and comments through on-line polling.

This paper is organized around this three-part structure. The first section considers existing conditions, beginning with factors that have influenced the land use and design pattern that we observe today. It continues by examining those patterns and the issues and opportunities that they present. The second section presents unifying principles, developed with the help of

community input through key informant consultation and public polling. The third part presents detailed concepts and recommendations, along with planning level opinions of costs of key recommendations and implementation directions.

Within each of these sections, this concept plan, in line with the overall process, considers increasing levels of focus:

- *City*, the role of the study area within the fabric of the entire Mascoutah community.
- *Corridor*, overall issues and patterns that affect the study corridor as a whole, from Mine Road to 6th Street.
- *Segment*, understanding that the study corridor is not uniform from end to end, but has segments with individual features and opportunities. This consideration applies the general principles to the segments and specific sites within them.
- *Site*, providing detailed concepts for strategic sites within the character segments.



PART ONE: EXISTING CONDITIONS

Main Street Today: Characteristics and Issues

HISTORY AND URBAN DESIGN

Some important elements of the current design and function of the study area have their roots in its early history. As settlement expanded into eastern Illinois during the early 19th Century, Mascoutah Township, containing the current city, was surveyed and platted in 1814. By the 1830s, a major road and mail route had been established between Saint Louis and Shawneetown, Illinois on the Ohio River. The Shawneetown to Saint Louis route was particularly strategic, as it cut a direct diagonal route between the Ohio and Mississippi Rivers. A lack of an incorporated town in St. Clair County east of Belleville led to the platting of the Town of Mechanicsburg in 1837, with its “main street” naturally platted along the mail road. Demand for better mail service from surrounding rural areas led to the establishment of a post office in the new town. However, another Illinois post office had previously used the name Mechanicsburg, and the town was renamed “Mascoutah” after the Mascouten Native American tribe who had moved from Michigan to this area of Illinois. F.B Marshall built a post office and general store on the southwest corner of Lebanon and State in a small structure that still exists at that location.

The land purchased by August F. Conradi, one of the original underfunded businesspeople who put the town in motion, included a sawmill located on the “Mill Block,” the block between today’s Main, State, Market, and Railway Streets. A lack of demand for lumber induced the owners to add a grist mill, but limited farming in the area at the

time also meant little business. After a couple of ownership transactions, a group of relatives that included Philip Postel bought the unsuccessful mill, enlarged the facility, and became exporters of flour. By 1850, the original mill was obsolete and the property was sold. But Postel and partners built a new mill across the street, on the west side of Railway Street. The new mill, including both sawmilling and flour milling operations, had both a frame section and a brick addition and became the primary economic, and in some ways, social engine of the community. The frame structure, currently the site of a city parking lot, was demolished in 1961. The remaining brick building dominates Main Street and is sometimes referred to as the “mural building” for the mural of local history painted on its north wall. Disassembly of third mill structure, a unique wood building clad in corrugated metal, was completed in 2020.

Interestingly, the successful mill had a significant impact on the urban design and function of the town. The original developers of the town believed that Market Street would be the main civic and commercial street of the town, and so it was platted at 60 feet, as opposed to 50 feet for Main Street and other principal avenues, and 40 feet for many of the local streets in the original town. However, the economic and financial centrality of the mill made east-west Main Street, connecting north-south approach routes like Lebanon and Jefferson Streets to the mill area more important, and major commercial development gravitated to our current study corridor.

As the St. Louis and Southeastern Railway (later



History. From top: Main (Mill) Street looking east from 1st Street in the early 1900s; remnants of the Postel Mill being deconstructed in 2020.

the Louisville & Nashville or L&N), developed its line between St. Louis and Evansville, Indiana, it built a station in Mascoutah in 1870. The railroad corridor, now abandoned and owned by the city, follows the south edge of the original town, and the station was located at the foot of Schmahl (now Railway) Street. Schmahl Street, the dividing line between east and west addresses and the direct connection from the depot to the



Mascoutah brick. From top: 315 West Main, built in 1857; brick home in study area.

mill and central district, was lined with the now historic houses and mansions owned by the city's wealthiest residents, a pattern fairly typical when the railroad was the ceremonial gateway to a community. Photographs show that the mill built a spur to the railroad, and for a time, freight cars were pulled by oxen to the Postel Mill. These living "locomotives" were replaced by mechanical ones around 1908.

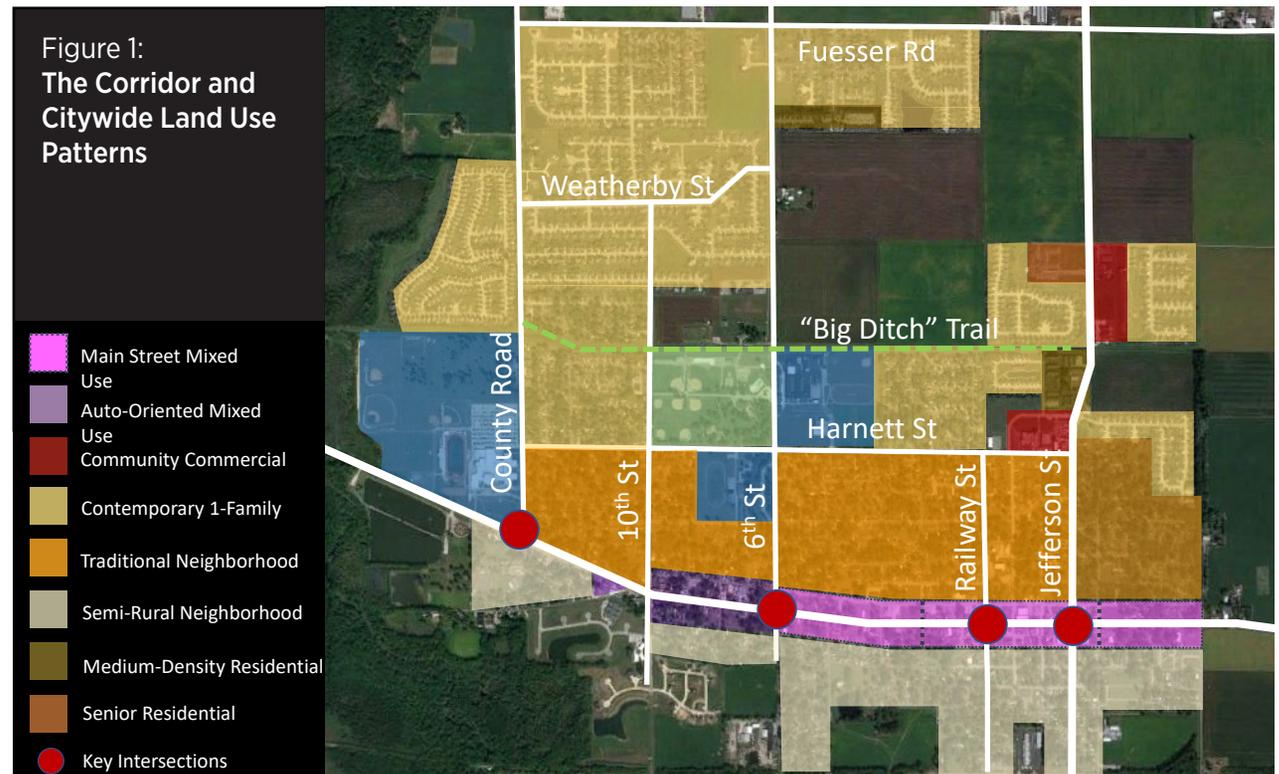
Another influence on the character of Mascoutah's built environment, especially but not only in the study area, was its local brickmaking industry. This industry was established in the 1850s west of today's County Road. Leonhardt Heiligenstein, the force behind the development of this industry, went into the brewery business and made the bricks for that building. The coming of the railroad expanded the brick business, and a substantial amount of brick were shipped by train for construction in Saint Louis. In 1902, the Mascoutah Brick Company was established on the south end on 10th Street. This company produced high-quality high density brick, used both locally in structures such as City Hall, and extensively in Saint Louis.

This discussion considers several factors that

have influenced the overall character of the Main Street study area. For more information, two excellent and engaging sources on local history are *Remembering Mascoutah* by Marilyn Welch (Mascoutah Society, 2016) and *Mascoutah* by Thomas S. Snyder (Arcadia Publishing, 2010).

THE STUDY AREA IN THE CITY

The original "Mechanicsburg" was extremely small, bounded by Lebanon Street, South Street, Railway Street, and the alley between Main (formerly Mill) and Church Streets. In a sense, though, the traditional town is contained in a sector between Harnett Street and the former L&N Railway corridor, roughly bisected by the Main Street study corridor. The regular grid of equal blocks of approximately 300 by 220 foot blocks



is bounded by Railway, Independence, Green, and Phillips Street. Main Street (Illinois Highway 177) remains the primary connection between the traditional town and the surrounding region, from Belleville to the west and smaller towns and villages to the east. The hard urban/rural divide that occurs at Mine Road reinforces the sense of Mascoutah as the eastern edge of the Saint Louis metropolitan area.

While the city has grown beyond the core study area, the original village and surrounding blocks remain the civic and spiritual (in both the religious and secular senses of the word) center of Mascoutah. It includes City Hall, the public library, and many of the city's longest-established churches. The Postel Mill also had an function as a social center for the city, and the Railway and Main intersection appears to retain that hold even though the mill itself is history. It also includes significant commercial and service businesses, although Mascoutah has a relatively small retail and commercial base for a city of its size – largely because of its proximity to the giant commercial complexes of the I-64 corridor.

The city's other commercial cluster at Mascoutah Plaza, includes Tom's Supermarket, Casey's convenience store, McDonald's, and some other local offices and services. This commercial group complements but does not compete with the center of town. In addition, the study area has good connections to major destinations like Scheve Park and school campuses, and the possibility of a trail loop will further increase that connectivity. As a result, people who live in Mascoutah's "traditional sector" are likely to maintain a strong business and emotional relationship to the Main Street corridor.

A larger challenge may be retaining that connec-

tion as the city continues to grow to the north. The "Big Ditch Trail" in some ways separates the areas of current growth from the traditional city and growth trends continue to the north. The trail corridor itself is not a barrier and should be seen as a unifying community space that transcends its current role as largely a recreational path. But continued north-south connectivity to the trail and beyond will be necessary to ensure that the city grows together rather than apart – and that this study area continues to maintain its role as the heart of the city.

CHARACTER AND SCALE

The study area has overall distinguishing characteristics, one of which is the different nature of its individual streets. One of the most significant of these is its east-west orientation, largely created by several factors:

- The initial regional pattern of east-west movement. Mascoutah developed along an east-west route between Shawneetown, then a major port and trading center on the Ohio River, and Saint Louis. This expressed itself with an east-west orientation of what eventually became Main Street. The railroad followed the same east-west pattern when it began service to the city in 1870.
- The nature of the initial platting of the city, with longer block frontages in the east-west direction and relatively shallow 100-foot deep lots. This was reinforced with later east-west platting of alleys. This pattern ensures building orientation to the longer block and narrower lot frontage.
- The attraction of the Postel Mill to people doing business in the community. The region-



Three corridors. From top: Main Street, Church Street, State Street.

al approach routes to town from surrounding agricultural areas required travel along Main Street to reach the mill. And the mill had different functions beyond grinding flour and sawing wood. It included a bank for farmers that evolved into the Bank of Mascoutah. Historic photographs show that the frame building of the mill featured a colonnade along its Main (Mill) Street elevation that not only “provided protection for farmers unloading their grain, but (also) benches (that) were also popular as a place where men could spin yarns.”

Within this east-west framework, each of these individual streets have distinct characters.

- Main Street’s relatively narrow right-of-way, the result of the original plat that de-emphasized it in favor of Market Street, gives it a distinctive intimacy that has some of the sense of older New England towns. This is reinforced by close-to-the-street two story commercial buildings, some with pitched or hip roofs, and a 50-foot distance between opposing building walls on the street. Some of this enclosure has been lost over the years with contemporary development with setbacks, driveways, and parking lots, but the remaining street quality is still significant. Other parts of the corridor have free-standing buildings and even single-family residential, but generally the sense of the mixed use street is retained. Individual parts of the Main Street corridor have different land use characteristics, but the variety of the street also has significant value.
- Church Street has two landmark examples of its namesake use – Holy Childhood Catholic Church and St. John’s United Church of



West Main. 40-foot segment of Main Street, as corridor widens outside the commercial core.

Christ. It also has quality civic uses, including the present front entrance of City Hall and the Public Library. However, much of its character has been compromised by storage and loading areas, unimproved parking lots, and a general “back of house” look along the south side of the street.

- State Street, the southern of the three primary corridors, has a primarily residential character on its south side, punctuated by a greenway where the Hog River daylight. The north side of the street is defined by free-standing buildings with a mix of offices, light industry, storage, and residential struc-

tures. This side of this street lacks a cohesive character but the building frontage masks the loading and service areas of Main Street buildings. Zion Lutheran Church and the south facade of the historic Postel Mill buildings establish a potential activity node at Railway Street, and provides an entrance to a potential Railway Street residential historic district.

Figure 2: Study Area Land Use



SEGMENTS: LAND USE AND URBAN DESIGN ISSUES

Within this overall character framework, the primary Main Street corridor divides into three logical segments. Each of these segments has an individual character with a different mix of land uses and include the west segment between 1st Street and 6th Street; central segment between 1st and Independence; and east segment between Jefferson and Mine Road. At the segment level, character is defined in this discussion by two factors: land and building use and urban design.

Land and building use factors:

- Consider the distribution of different uses on the ground.
- Identify retail and service clusters.
- Display patterns of activity.
- Help quantify the building and land area for different uses.
- Identify underused land and development opportunities.

Urban design factors:

- Consider the relationships and character of buildings to each other, the street, and surrounding neighborhoods.
- Address issues like building size and character, the character of the street, possible connections, and special features.
- Identify opportunities to create a more connected, attractive, and efficient corridor.

WEST SEGMENT: 1st to 6th Streets

Land Use: The Pig Creek greenway, which roughly bisects this segment, is a potential defining feature. The western section between Pig Creek and 6th Street is predominately residential in scale and character. Most residential buildings are single-family, with the notable exception of the historic 1857 multifamily building at 315 West Main. The Mascoutah Heritage Museum on the west side of Pig Creek is a significant anchor in this area. A convenience store is located on the northeast quadrant of the 6th Street intersection.

The eastern section between the creek and 1st Street displays a mix of uses along Main Street that includes significant office and financial uses, a restaurant in a re-purposed fast-food structure, a church, and scattered single-family homes. A largely vacant commercial site on the south side of Main between 2nd and Pig Creek provides an important opportunity for re-occupancy.

Development is more intermittent along both the Church and State Street frontages. Parking, circulation, and storage yards dominate the south side of Church, with more continuous residential development on the north side. The north blockfaces also include a large church parking lot without buffering or internal landscaping, a telephone utility building, and a civic club. The State Street frontage is a largely residential on both sides, with the exception of significant light industrial use at 4th Street.

Urban Design: Main Street widens west of 2nd Street and while many buildings are close to the street, the combination of the increased street width and primarily free-standing buildings creates an urban environment more typical of the edge rather than the core of a central dis-

Figure 3: West Segment Land Use



trict. Sidewalks and sidewalk setbacks also are a significant design determinant. Sidewalks are at or very close to the back of curb along the north side and typically set back about five feet on the south side. Most residential structures have small front yards, and some on the north side of Main are accessed from long driveways to Church Street. In fact, Church and the properties on its south side largely serve the Main Street frontages.

Other significant urban design features and opportunities include:

- *The 6th and Main intersection.* Sixth Street is a major community corridor to Mascoutah Elementary School, Scheve Park, and Illinois Route 161, a main route to the center



of Belleville. The 6th and Main intersection transitions to the town center environment of the study area. A district defining feature is possible at this point.

- *Brick structures.* This segment includes several important examples of 19th-Century buildings built of local brick, and offer real illustrations of this part of the city’s economic history.
- *The Heritage Museum and adjacent Pig Creek.* This small watercourse and the adjacent museum are centerpieces to this segment of the street, and offer an opportunity to connect Main Street to residential areas north and south through greenway development.
- *Rear Parking and Service Areas.* The disordered development pattern found behind Main Street buildings and especially along Church Street provides an opportunity for redesign and areawide cooperation that will produce better efficiency and appearance and some development opportunities.
- *Street Grid shift at 2nd Street.* The orientation of Mascoutah’s street grid, including Main and parallel streets, shifts to a north-west-southeast alignment, presumably to follow the route of the original mail route. This slight change in alignment attracts attention to the now underused southwest corner of the street, that also adjoins Pig Creek.



Pig Creek Views. From left, looking south toward State Street; Main Street creek crossing.

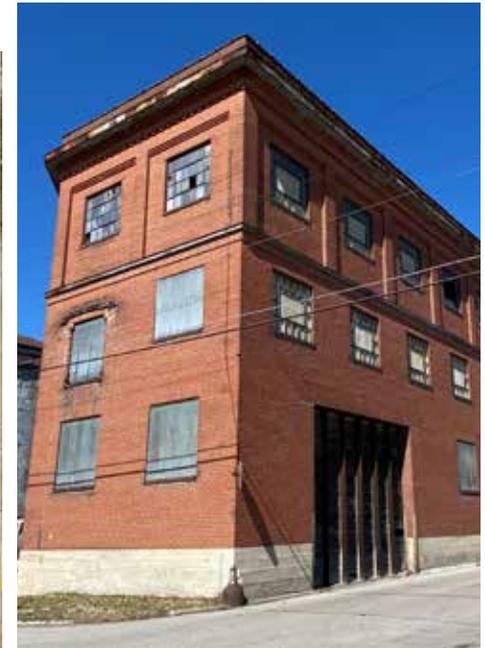
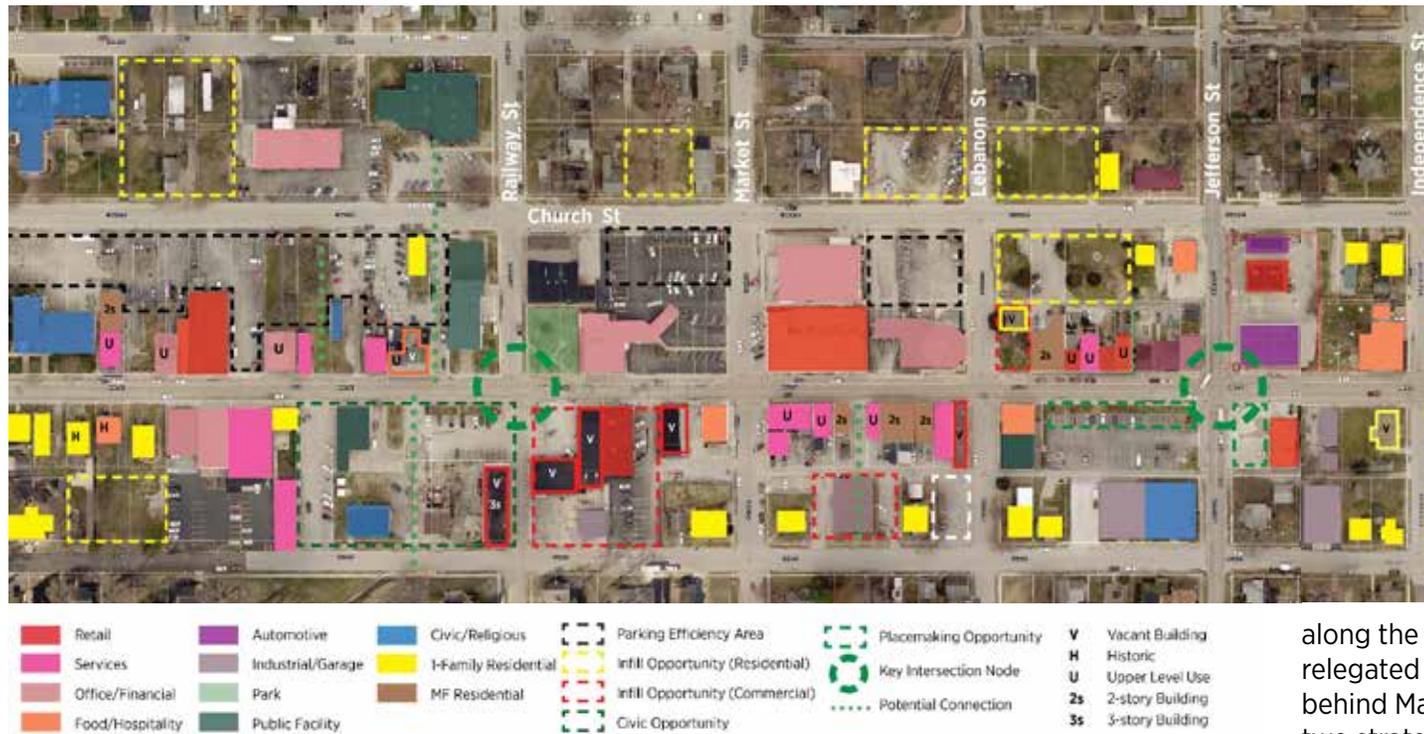


Commercial development opportunities. Vacancies on the south side of Main.



Street environment. From left, the “knuckle” at 2nd Street; 6th and Main intersection.

Figure 4: Central Segment Land Use



CENTRAL SEGMENT: 1st to Independence Street

Land Use: This segment is the commercial and civic core of the study area, with its largest continuous concentration of retail, service, and office uses and eating/drinking businesses. It includes the best preserved commercial blocks the Ace Hardware block east of Market Street; the north block of Main between Jefferson and Lebanon; the south block of Main between Lebanon and Market; and parts of the south block between Market and Railway and the north block west of Railway. The two largest private commercial properties along Main are Ace Hardware and the Moll Funeral Home. This Main Street segment also includes the largest concentration of civic

uses, including City Hall, Post Office, the small City Hall Park, and the Public Library north of Church Street. An unusual land use component are residential units behind Main Street structures, with entrances and parking off the parallel alleys. These are most common along the north side alley from Jefferson to Lebanon and the south alley between Lebanon and Market. These suggest the possibility of a populated alley environment that complements Main Street.

Two major contemporary banks have changed that landscape as they've moved from traditional Main Street buildings to more suburban-scaled development that accommodates drive-through banking and parking. But these later 20th century projects still maintain an urban presence

along the street. Surface parking lots are mostly relegated to the parcels along Church Street behind Main Street structures, but occur along two strategic main street locations – the south side of the Jefferson to Lebanon block and the former Postel Mill site at Railway Street. Both are important parking facilities, but both occupy very valuable real estate. The Postel Mill site includes the remaining, vacant three-story building and the now demolished mill building, and is a critical development opportunity for a district that largely lacks public space.

In common with the west segment, most land on the south side of Church Street provides parking, loading, and support for Main Street buildings. The north blockface along Church accommodate quality non-residential uses – a church, library, and office building – but have a number of vacant sites. Service areas along the north side of State Street support main street buildings but these blocks also have a variety of residential and

office uses oriented to the street. The south side of State is largely in single-family residential use with the exception of Zion Lutheran Church.

Urban Design: The quality of the urban environment presents special opportunities and distinction in this segment. Design issues focus on two key intersections – the Jefferson (Route 4) and Railway Street intersection. As we have seen, the original plat concept for Mascoutah envisioned Market Street as the main commercial street. However the key intersections over time became Jefferson, the main highway junction of Highways 177 and 4 and primary route from I-64 to the center of town; and Railway Street, with the Postel Mill. Mascoutah’s economic heart. These two nodes have significant placemaking potential, but fail to take advantage of their strategic locations.

The now disjointed corridor between Church Street and the Main/Church alley also presents significant opportunities. With the exception of four major buildings (Subway, the Ace Hardware warehouse, City Hall, and Crosspointe Church), most of this land serves Main Street buildings with parking and loading area. Of major buildings, only City Hall has a front entrance from Church Street, created by the 1990s vintage north addition to the original 1938 building. A sidewalk on the south face of Church is at best discontinuous and parking areas themselves are inefficient and fragmented. Redesign and cooperative action to create a unified parking and service area with pedestrian access could both increase parking and substantially improve the appearance of this corridor. Opportunities also exist for connecting this service corridor directly to the Main Street frontage through existing or possible passages.

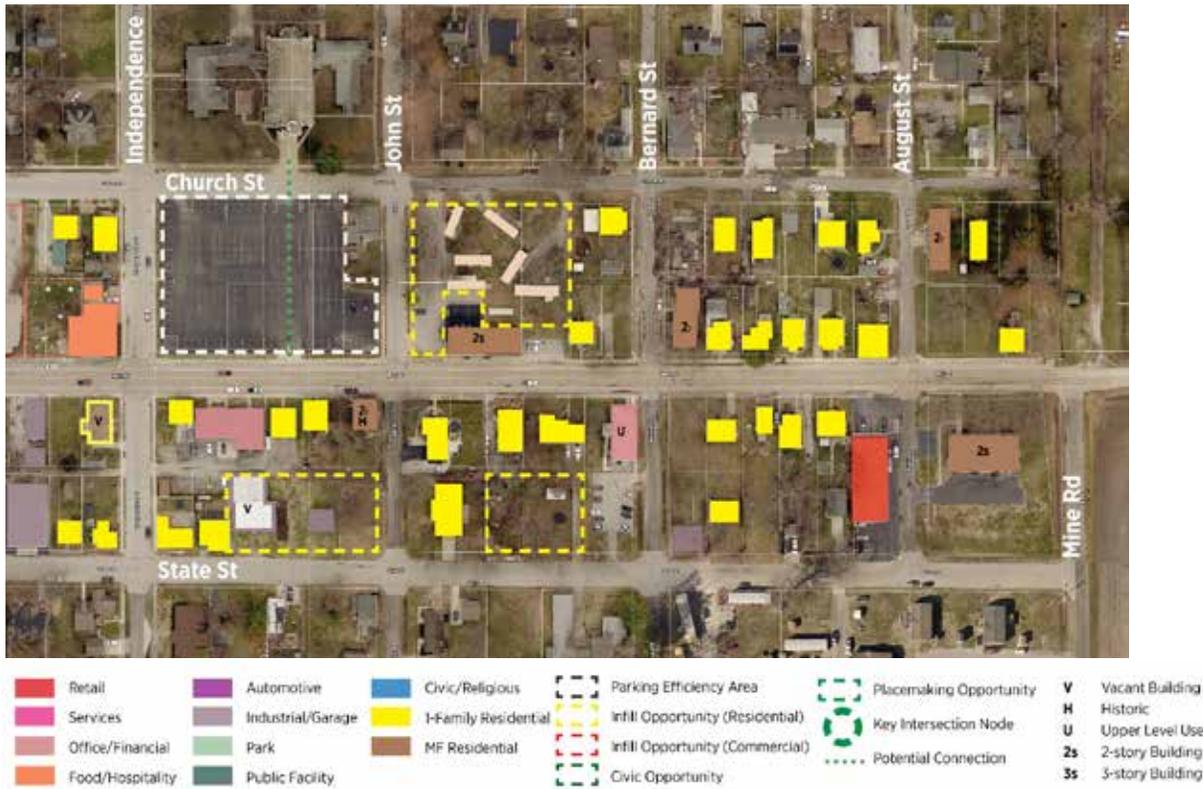
Other significant urban design resources include:

- *The intimate scale of Main Street cited above.* The 50-foot building line separation creates a distinctive linear room, but also restricts parking along Main Street to one side and sidewalk width to less than 10 feet.
- *National Register quality buildings,* notably but not limited to the Hagist Building (Ace Hardware) and the Postel Mill Building, along with effective adaptive reuse of the 1914-vintage First National Bank Building at Market and Main.
- *A row of significant and finely scaled homes* (including the Doll House bed and breakfast) east of 1st Street.
- *The study area’s main existing public space,* the gazebo park on the northeast corner of Railway and Main.
- *The 2020 reconstruction of the Jefferson (Route 4) and Main intersection by the Illinois Department of Transportation,* a thoughtful design that includes curb extensions, colored concrete and brick texture mats along the curb line, and teardrop lighting. This establishes a basis for future enhancement of the Main Street environment as part of a future DOT reconstruction of Illinois Rout 177.
- The alignment of major public facilities and possible redevelopment sites along Railway Street, including the Postel site, City Hall, and the Public Library.
- Orientation and visibility of the building housing the Mascoutah Visitors Center and Saint Louis Coffee World toward the Jefferson Street intersection.



Opportunity features. From top, intersection improvement at Jefferson Street intersection looking west; gazebo park across from City Hall; residential units behind Main Street storefronts

Figure 5: East Segment Land Use



EAST SEGMENT: Independence Street to Mill Road

Land Use: The eastern segment of the study area transitions from the commercial nature of the central segment to the rural environment of western Illinois, and the land use character changes accordingly. The overall character of the segment is residential, with single-family homes close to Main Street. Office, retail, and multi-family uses are also present along the street, including adaptive multi-family reuse of the brick structure between Mine and August.

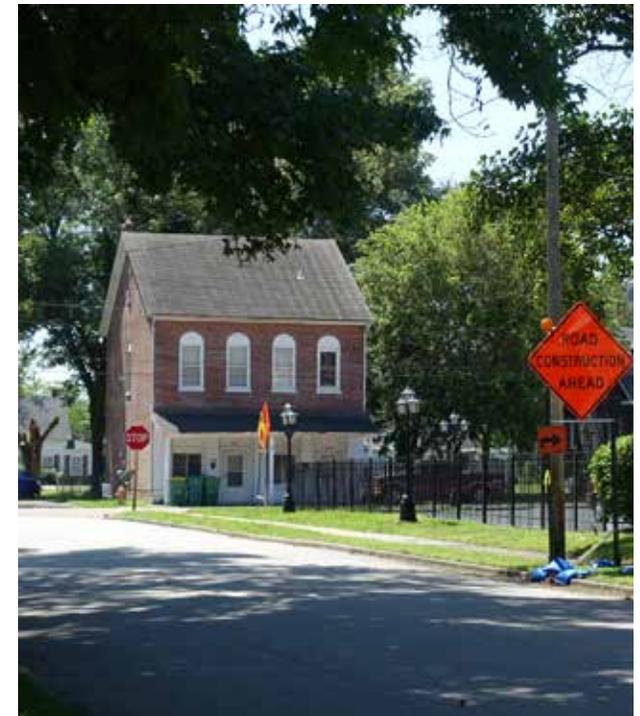
Somewhat characteristic of an urban/rural transition area, some mobile home clusters also

occur here, including a small group of older mobile homes along Church between John and Bernard, and some small industrial and storage occupancies along State between Independence and John. Additionally, the relatively diffuse land use pattern in this transitional area affords some significant infill development opportunities, most notably on the easternmost block and the possible redevelopment of the mobile home group.

The largest single user of land is the nearly full block surface parking lot of the landmark Holy Childhood Catholic Church, which also provides auxiliary parking for some Main Street commercial uses, notably Skooter’s Restaurant.

Urban Design: In some ways, the dominant urban design feature of the eastern segment is Holy Childhood Catholic Church. This landmark, with its highly visible steeple, marks the center of Mascoutah from the entire vicinity. The dramatic front of the church is disengaged from Main, separated by its large and undifferentiated asphalt parking lot.

East of John Street, Church Street takes on a decidedly more rural character, narrowing and using a rural section without curbs and with surface drainage. Main Street itself widens east of Jefferson Street to a 40-foot section, with a shoulder available for parking on both sides. The urban character of the street through the town center dissipates as the highway leaves town, and, as noted earlier, Mine Road marks a clear division between town and rural settings.



PART TWO: ORGANIZING PRINCIPLES

Major Plan Ideas for the Corridor

OVERALL LAND USE AND URBAN DESIGN GOALS

Given the role of the Main Street study area in the city and the perceptions and opinions of people who responded to the polling questions during this process, we can define the primary land use and urban design goals as follows:

Maintain the role of the linear “town center” as common ground for the community, a place that continues to function as a civic destination for Mascoutah. This can be an increasing challenge as Mascoutah continues to grow and, like many communities, grows away from its traditional core. Today, most of the city’s banks and churches are located in and near the Main Street study area. Main Street is also the focus of civic life (City Hall), cultural life (the Library and Museum), a variety of key services that cover the entire cycle of life, and distinctive retail and food establishments. The combination of these features define Mascoutah and distinguish it from every other place. Something significant and fundamentally important is lost if this community role erodes over time.

Increase the number of features that attract people to Main Street. This is a necessary corollary to the first goal. A district that remains static will inevitably lose energy and decline in importance. A strategy for the area should provide both places and spaces that diversify its offerings. It should provide opportunities for new activities, using existing buildings effectively and providing some new spaces, consistent with market potential. It

is also important to note that more features that generate more human activity strengthen existing businesses.

Offer a great experience to customers, business-people, workers, and residents. To paraphrase the physicist and urban thinker Geoffrey West, a goal of good planning is increasing positive interaction while minimizing distress. The measures of the range from the functional to the subjective and even emotional. On the functional side, people arriving at the district by car should be able to find a parking space easily and have a pleasant route from their parking place to their Main Street destination. On the subjective side, a well-ordered and attractively designed environment provides a sense of a security and satisfaction not found in disorder and poor maintenance. When customers become pedestrians, they should feel comfortable and welcomed.

Increase the number of people living in and around the district. Residential development has been a staple of revitalization efforts in many cities and towns. Residents in places like the Main Street study area create a walking distance environment that brings security and is active throughout the day. They also create an environment that makes visitors feel better about a place. Gaps such as vacant lots, on the other hand, decrease comfort and overall quality.

Link features together. Linkage is very important to the nature and use of a district. These linkages often involve circulation and connections to other community features. For example a clear path that accommodates all modes of travel from



Common Ground for the Community
Mascoutah City Hall



Increase Attractions
Performance Area in Bayliss Park, Council Bluffs, IA



Great Experience
69th Street Plaza, Wauwatosa, WI



Increase Residents
Townhomes, Clear Lake, IA



Link Features
Roslindale, MA



Promote Cooperation
Kearney, NE

Scheve Park to the Main Street district benefits both destinations. An active transportation network that organically takes people to and through the study area also provides this kind of linkage. But linkage within the district is equally important – parking space to Main Street, destination to destination.

Promote cooperative action. Districts that think cooperatively invariably do better than those where businesses act alone or in isolation from one another. There are examples of informal cooperative action. For example, some privately owned parking lots are informally available to customers other than the lot owners or employees. But there is a mutual benefit to the businesses and stakeholders working together and even developing an organizational structure that manages the overall process.

CORRIDOR CONCEPTS

Given the role of the Main Street study area in the city and the perceptions and opinions of people who responded to the polling questions during this process, we can define the primary land use and urban design goals as follows:

- **Three Distinctive Character Segments.** This formalizes the analysis presented above – viewing the corridor as three interconnected segments that have somewhat different personalities. Each segment has at least one catalytic project area that contribute to the health of the entire linear study area. Each of these segments could include a thematic element, expressed through environmental graphics and future streetscape elements. Possible thematic associations are History (West, related to the Historic Musuem), Commerce (Central, related to the segment’s sta-

RELEVANT POLLING RESULTS

- About half visit Downtown businesses at least weekly, mostly for shopping or eating
- Most people see City Hall/railway and Main as the “center” of Downtown.
- Greatest business needs are more restaurants, entertainment, and retailing; favor outdoor or patio seating at restaurants
- Greatest measures of success are improved aesthetics, better pedestrian features, increased retailing
- Majority favors better bicycle facilities and connections to trail
- Support for public space with flexibility for events and performance, space for picnics and eating area
- Mill site is a likely candidate site.
- Initially, most people do not support housing in the study area
- Majority views Main Street as a negative environment
- Improved building facades and sidewalks are top priorities
- Parking supply is not a huge issue, and most people will walk to destinations
- 60% support economic development incentives and technical assistance to businesses
- 80% agree with some form of lodging on Main Street
- Top priority buildings for redevelopment are in the Railway and Main area
- Housing is preferred off or adjacent to Main Street itself, including townhouses, attached housing, and small scaled apartments

Figure 6: Overall Corridor Concept



tus as the commercial and economic core), and Living (East, related to Holy Childhood and surrounding neighborhoods).

- **Civic Anchors.** The corridor includes four areas of special importance because of their character, history, or location. They form the nodes along the street that help seed other private and public investments and include:
 - The Heritage Museum and Pig Creek Greenway, associated with historic interpretation to tell the story of Mascoutah.
 - The Railway and Main Nucleus, incorporating the Postel Mill that helped solidify

the struggling community of Mascoutah in the mid 19th Century and could become the primary public square with related private development.

- The Jefferson Node, the primary highway junction and entrance from Interstate 64.
- Holy Childhood Promenade, making the city's most visible landmark a more integral part of the Main Street environment.
- **Railway and Main.** Of the four anchors identified above, Main and Railway emerges as the nucleus that the study area currently lacks. The combination of vacant commercial

- Civic anchors
- Enhanced Main Street
- Church Street Complete Corridor
- Cross Connections
- ⋯ Parking Improvement Areas
- Railway Nucleus
- Business Core
- Entrepreneurial Opportunity Areas
- Residential Opportunity Areas



Four Civic Anchors. Clockwise from top left: Museum and Pig Creek greenway, Main and Railway, Jefferson and Main gateway, Holy Childhood

spaces, a three-story vacant historic building, available open space, City Hall, and solid surrounding commercial development provides this unusual opportunity.

• **Two Key Corridors with Cross-Connections.**

The two primary east-west corridors, Main and Church, have distinct roles. Main Street is the primary east-west street, providing smooth movement of motor vehicles at appropriate speeds and an improved pedestrian environment. Church Street will accommodate local movements, service and primary parking access, and upgraded bicycle and

pedestrian accommodation. While transportation is addressed in another white paper, active transportation is also very much an urban design consideration. Bicycle (conventional and e-bikes) should be accommodated in the district and pedestrian access improved. The corridor-wide program:

- Identifies Church Street as the primary east-west bike route though the study area. Bicycle boulevard adaptation of the street may be a short-term solution, with a future sidepath possible on the north side of the street. A continuous 6-foot

sidewalk should be developed on the south side.

- Incorporates bicycle parking into improved parking lots.
- Adapts Railway Street as a north-south bicycle boulevard, a major central area connection between the Big Ditch Trail and a new shared use path using the city-owned former L&N corridor.
- Widens Main Street sidewalks, created by removing on-street parking in most locations except in the currently improved area between Independence and Lebanon Streets. This assumes parking redesign behind Main Street buildings to increase parking inventory. Sidewalk width would increase by four feet on each side in the central segment, with a 6- to 8-foot parkway strip with a 6-foot sidewalk in the existing 40-foot sections in the east and west segments. On-street parking bays protected by curb extensions may be used as an alternative where demand presents itself.
- Provides passages and connections in strategic locations wherever feasible between peripheral parking lots and Main Street frontage.
- Where possible, provides walking areas along alleys as part of parking lot redesign between Jefferson and Railway and City Hall to 1st Street.
- **Cooperative and Efficient Parking and Service Corridor.** This unifying idea proposes a comprehensive redesign of the fragment-



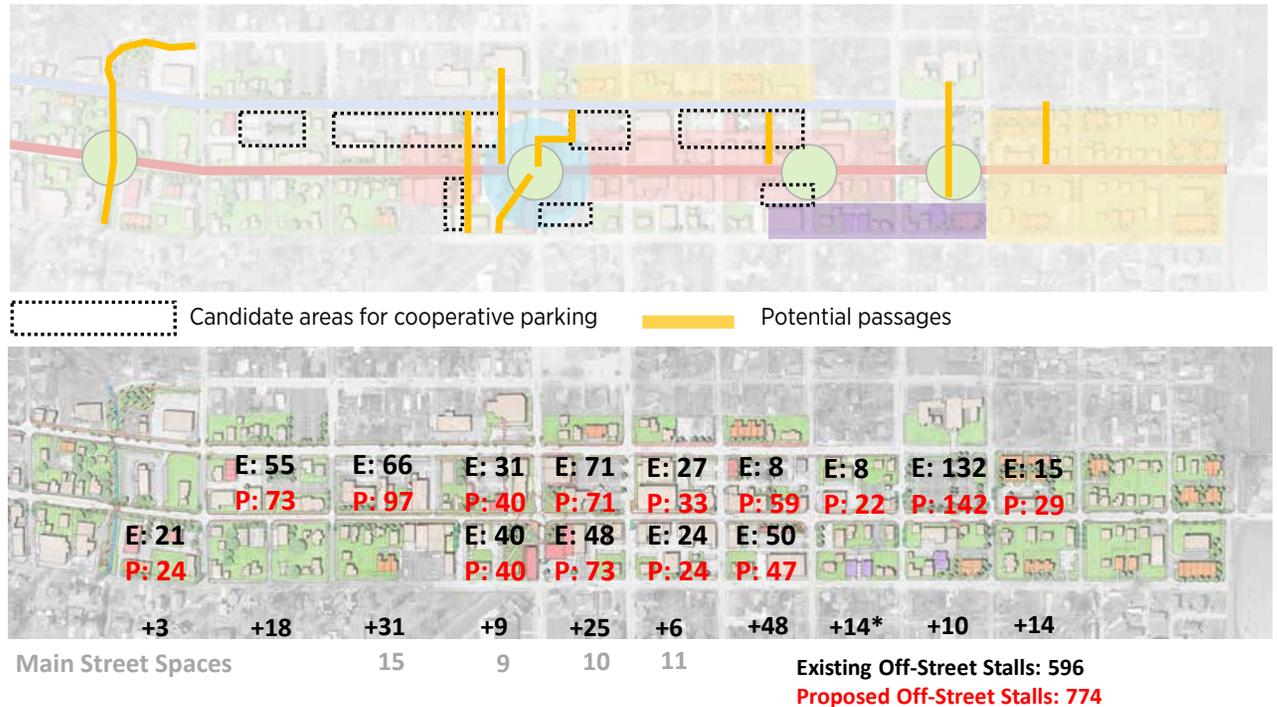
Parking lot link. Midblock pedestrian link between a downtown parking lot and main street storefronts in Council Bluffs, Iowa. Ceramic inlays are adaptations of historic postcards.



Main Street section. Street reconstruction of a street similar to Main Street, using curb extensions and residential parking bays. (Osawatomie, KS)

ed and often unimproved parking lots and service areas north of Main Street buildings between Jefferson and 1st Street in the central segment and 1st and 2nd Street in the west segment. This principle proposes coordinated and unified redesign and improvement of these parking areas, with connected circulation, pedestrian linkages to Main Street, and maintaining all functional access to businesses. This approach increases the

Figure 7: Coordinated Parking Areas and Projected Parking Inventory Change



parking inventory through greater efficiency, provides some infill possibilities, improves the visual quality of the district, and reduces the demand for on-street parking along Main Street. As shown in Figure 7, the net parking inventory increases by up to 170 stalls with implementation of this concept.

- **Mix of Uses.** Where possible and depending on context, the study area should incorporate new uses that support the overall goals for the study area are consistent with market demand and preferences. These development focuses include:
 - Residential development on potential redevelopment sites, with major opportunities on the north side of Church between

Jefferson and Railway and between John Street and Mine Road. Favored development forms are medium-density townhomes and single-family attached units.

- Office and commercial uses with a focus on re-occupancy of vacant or underused buildings and sites created by parking redesign.
- Entrepreneurial/start-up possibilities in new or existing buildings along State Street between Lebanon and John Streets and as part of redevelopment of the Postel Mill Building.



Parking lots. Cooperative redesign of large parking areas can substantially increase the district's parking inventory and take the pressure off on-street parking along Main Street.



Sidewalk width. Increasing the amount and convenience of off-street parking provides the flexibility to widen Main Street sidewalks with space for outdoor use, as well as add green parkway strips.

- **Improved sidewalk and public environment.**

Participants in the various polling exercises stated a decided preference for wider Main Street sidewalks. In the Main Street environment, existing narrow sidewalks may feel uncomfortable because of adjacent traffic and lack of separation from other pedestrians. Wider sidewalks also provide space for outdoor activity, necessary in both the current COVID situation and in its likely aftermath. Other than sidewalks, the center of town is short on meaningful public space, and an improved public realm should maximize active public space, finding opportunities wherever possible and using the vacant and centrally located Mill site as a major opportunity to address this problem.



RELEVANT POLLING RESULTS

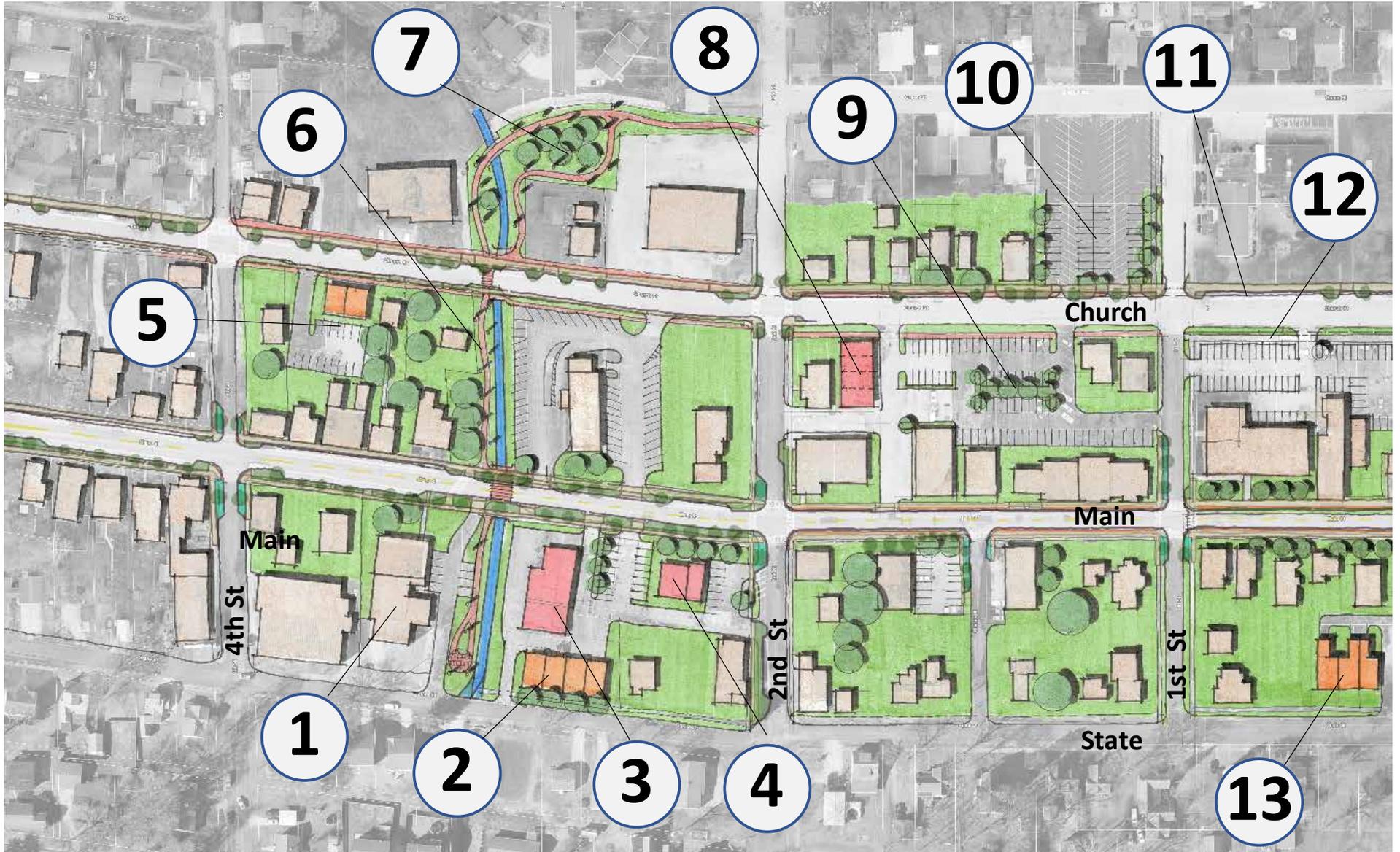
- Generally strong support for better bike connections to and through the study area.
- Shared use path is favored along Church Street north side.
- Of focus areas 65% prefer Main and Railway focus, 30% the Museum/Pig Creek area.
- 85% support coordinated/cooperative parking along Church Street as proposed
- General support for reducing on-street parking in favor of wider sidewalks
- 70% support National Register designation for Main Street
- Positive response to use of Postel Building for brew-pub and creative space
- 70% support a Main Street business and management organization to address parking and maintenance
- 54% are “very enthusiastic” about Pig Creek historic path concept; additional 24% consider it a “good” idea
- 69% are “very enthusiastic” about Railway nucleus concept; additional 15% consider it a “good” idea
- 54% are “very enthusiastic” about Jefferson Gateway concept; additional 31% consider it a “good” idea
- 46% are “very enthusiastic” about Holy Childhood focus concept; additional 23% consider it a “good” idea

PART THREE: DETAILED CONCEPTS FOR SEGMENTS AND SITES

This section applies the principles and concepts discussed above to each of the three segments. Each section has a table corresponding to a key map of the segment, with descriptions of the proposed concepts.

WEST SEGMENT PLAN

Figure 8: Detailed Concept Plan for West Segment



WEST SEGMENT PLAN

Figure 9: West Segment Components

| Map Key | Project | Notes |
|-----------|------------------------------------|--|
| 1 | Mascoutah Heritage Museum | The museum building itself has a long history that in its present form dates back to reconstruction after an 1896 tornado. The building was donated to the Society in 2003 and has been enlarged recently. |
| 2 | Infill townhomes | Medium density housing along State Street behind a former auto dealership |
| 3 | Commercial reuse | Reuse of a former auto dealership. Possible arcade proposed for Main Street frontage (showroom) with workshop or gallery in rear |
| 4 | Commercial reuse | Former gas station, with possible reuse as a restaurant, ice cream shop, or similar use. Landscaping and possible outdoor dining on site with parking off 2nd Street and shared parking with adjacent building |
| 5 | Parking and infill duplex | Improvement of parking area serving historic 315 W. Main multifamily building, with attached residential on Church Street |
| 6 | Pig Creek Path | Footpath with interpretive features beginning at the Museum and continuing along the west side of the Pig Creek Greenway to 2nd and Green. Includes a protected midblock crossing of Main Street. One of the four Civic Anchors. |
| 7 | Moose Park | Development of an unimproved portion of the Fraternal Order of Moose site as a neighborhood park along the Pig Creek Path, including a loop, and central green space with thematic play equipment. |
| 8 | New Commercial/Mixed Use | Office or mixed use development (residential over office or retail) on infill site adjacent to redesigned parking lot |
| 9 | Parking Lot Redesign | Redesign and consolidation of existing parking areas with internal landscaping, adding an estimated 18 spaces on the block. |
| 10 | Existing church parking lot | Peripheral landscaping to improve existing lot. Expansion of parking supply on surrounding lots could encourage church to develop a portion of lot with housing. |
| 11 | 10-foot shared use path | Path for pedestrians and bicyclists connecting to proposed path on 6th Street to Scheve Park and the Big Ditch Trail |
| 12 | 6-foot sidewalk | Continuous sidewalk on south side of Church Street with significant parkway setback, probably developed with improved parking lots |
| 13 | Residential infill | New rowhouses or attached units on vacant site |

WEST KEY SITE CONCEPT

Figure 10: Detailed Concept Plan for West Civic Anchor: Museum and Pig Creek Path



- 1 Historical Museum
- 2 Infill Attached Residential
- 3 Path Head Plaza
- 4 Protected Pedestrian Crossing
- 5 Path and Historic Columns
- 6 Footbridge
- 7 "Moose Park"
- 8 Commercial Reuse
- 9 Gas Station Reuse (Ice Cream with Outdoor Space)
- 10 New Commercial or Mixed Use Building



Ice cream and food. Reuse of a corner gas station as a popular ice cream shop and restaurant, complete with outdoor green space. This presents a possible model for development of the vacant building at 2nd and Main.



Historic markers on interpretive path. From left, monuments interpreting the history of South Omaha and its stockyards; vertical markers along San Francisco's Embarcadero

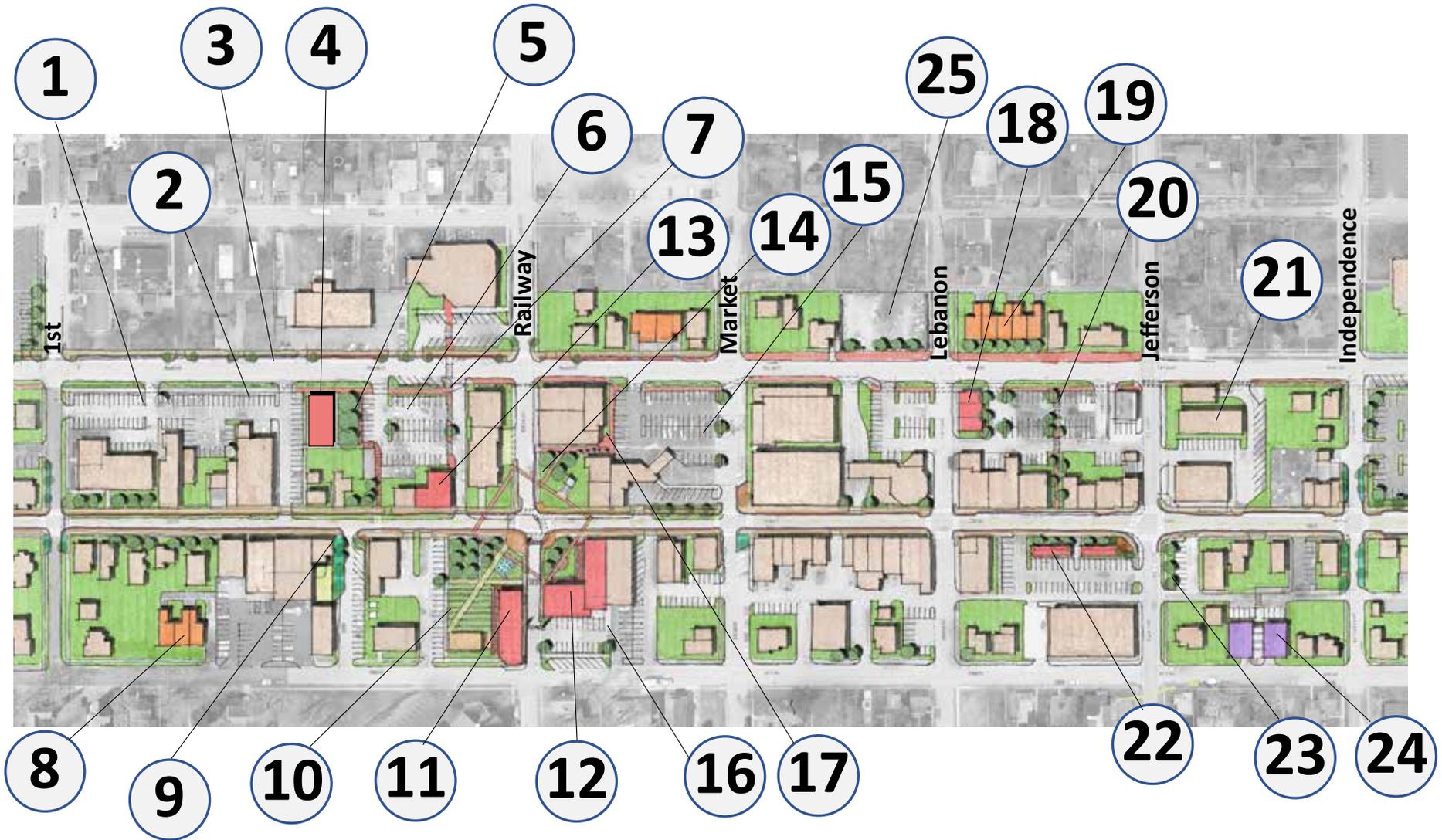
WEST KEY SITE CONCEPT



Pig Creek Path. *This pedestrian path would follow the west side of the creek from the Museum to 2nd and Green Streets. Vertical markers would describe Mascoutah's nearly two century history.*

CENTRAL SEGMENT PLAN

Figure 10: Detailed Concept Plan for Central Segment



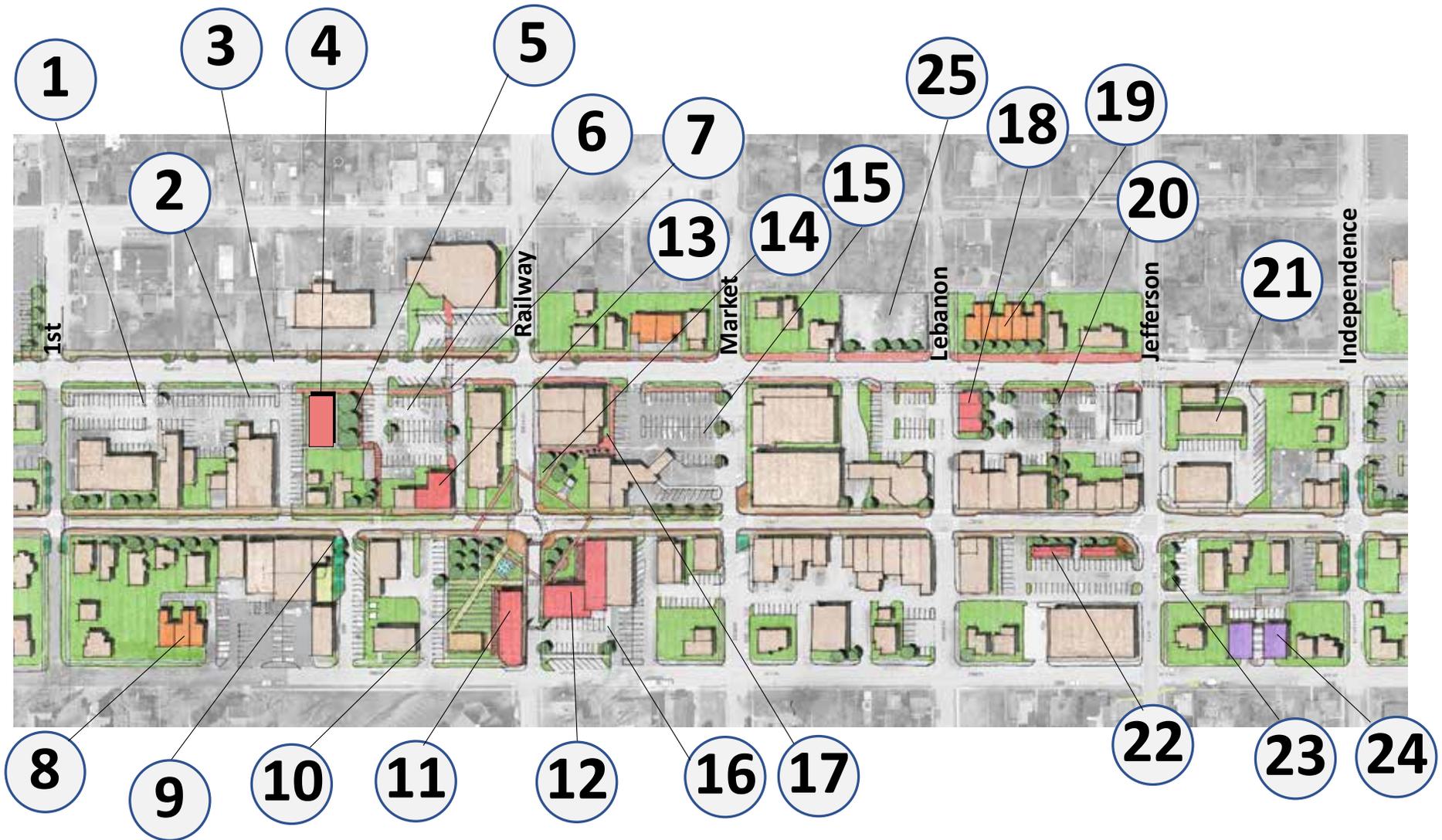
CENTRAL SEGMENT PLAN

Figure 11: Central Segment Components

| Map Key | Project | Notes |
|-----------|---|--|
| 1 | Parking Lot Redesign | Cooperative redesign and consolidation of parking lots serving Flowers, Balloons, Etc, Bethel United Methodist, and intervening businesses. Lot improvement adds up to 31 stalls to existing |
| 2 | 6-foot sidewalk | Continuous sidewalk on south side of Church Street with significant parkway setback, probably developed with improved parking lots |
| 3 | 10-foot shared use path | Path for pedestrians and bicyclists connecting to proposed path on 6th Street to Scheve Park and the Big Ditch Trail |
| 4 | New mixed use | New commercial or mixed use building adjacent to redesigned parking and frontage on “Big Tree” open space |
| 5 | “Big Tree Park” | Green space with preserved specimen trees, with landscaped passage to Main Street |
| 6 | Parking lot redesign | Improved and consolidated parking lot, serving City Hall, pantry, and Mascoutah Steakhouse building. Adds 9 stalls to existing. Involves demolition of vacant house |
| 7 | Library-Main Street Link | Pedestrian path from library front door along City Hall service drive west of building to Main. Includes mid-block crossing of Church Street |
| 8 | Infill townhomes | Three rowhouse units on vacant site next to Moll parking lot |
| 9 | Rain garden and parking modification | Rain garden for stormwater management, with minor modification of adjacent parking lot. |
| 10 | “Postel Mill Park” | Redevelopment of site of now-demolished mill building, proposed in park plan as a gathering space with performance stage, water feature, and green space. Major key to Main-Railway nucleus |
| 11 | Postel Mill Building | Redevelopment of historic Postel Mill building. Development concept proposes brew-pub at street level with innovation/enterprise space on upper levels. |
| 12 | Commercial reuse | Reuse of former laundromat and hardware store, potentially for food retail/specialty market and restaurant. Conversion of parking to green area with dining deck and redesign of Railway Street as a “festival street” that can be closed during special events. |
| 13 | Mascoutah Steakhouse reopening | Reopening of restaurant with residential or commercial rehabilitation on second level |

CENTRAL SEGMENT PLAN

Figure 10: Detailed Concept Plan for Central Segment



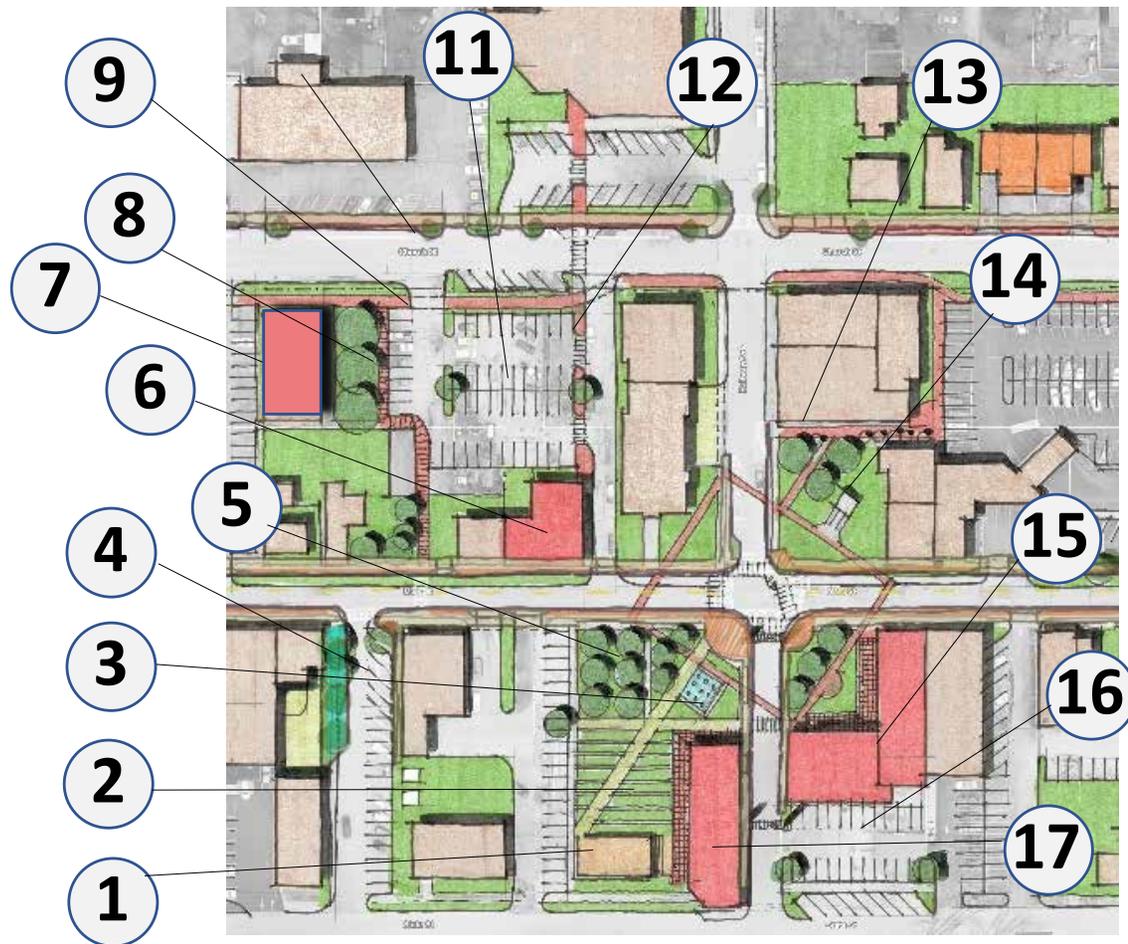
CENTRAL SEGMENT PLAN

Figure 11: Central Segment Components

| Map Key | Project | Notes |
|-----------|--|--|
| 14 | Gazebo Park | Existing open space with new path to tie into overall public space grouping |
| 15 | Parking lot redesign | Restriping and landscaping of existing bank lot, also providing space for sidewalk and park-way strip |
| 16 | New parking area | Reconfigured parking area to support Postel Building and surrounding development |
| 17 | Main Street Link | Connection from Church Street to Gazebo Park, greening a currently paved area not used for parking |
| 18 | New commercial building | New building with Lebanon Street frontage, made possible with parking lot redesign |
| 19 | Infill townhomes | 5 rowhouse unit on infill site on north side of Church Street |
| 20 | Parking lot redesign and Main Street link | Redesign of parking, adding up to 50 new parking stalls and providing an improved entrance to the central district. Includes a passage to Main Street from Church Street, through the parking lot, and to main Street using a site with a small vacant building. Structure could be demolished or reused as the covered passage with some interior vendor space. |
| 21 | Expanded Moto Mart | Project currently under construction |
| 22 | Jefferson Gateway | Market shelters along Main Street, defining the street edge with a multiuse shelter that connects intersection to Visitors Center and Saint Louis Coffee World. One of the Civic Anchors |
| 23 | Corner landscaping | Enhancement of Jefferson St frontage of All Mart |
| 24 | Innovation Village | New infill workshop or gallery buildings along State Street |
| 25 | Yard Screening | Landscaping and visibility screening around Ace Hardware storage yard |

CENTRAL KEY SITE CONCEPT: RAILWAY NUCLEUS

Figure 12: Detailed Concept Plan for Central Civic Anchor: Railway Nucleus



- | | | | |
|---|---------------------------------|----|-----------------------------------|
| 1 | Performance Stage | 10 | Shared Use Path |
| 2 | Seating Lawn | 11 | Redesigned Parking |
| 3 | Water Element | 12 | Library-Main Link |
| 4 | Parking Lot Redesign | 13 | Alley Connection |
| 5 | Tree Lawn | 14 | Gazebo Park |
| 6 | Steakhouse Revitalization | 15 | Commercial Reuse and Dining Plaza |
| 7 | New Commercial/Mixed Use | 16 | Expanded Parking |
| 8 | Specimen Trees and Main St Link | 17 | Postel Innovation Center/Brew-Pub |
| 9 | Sidewalk | | |



Program for the Postel Mill's upper levels. *Small business incubator at the Mill in Bloomington, IN*

Figure 13: Railway Nucleus



Railway Street Nucleus. *This central intersection can become the heart of the study area by re-purposing the Postel Mill site. This view looks southwest. The Mill site could include a performance and gathering space, multi-purpose lawn, and a water feature. Wood from the recently demolished mill building could form the backstop of a stage. The concept connects City hall and the gazebo park as part of a unified town square, and commercial redevelopment of the laundromat and hardware stores is also important to the evolution of this as a major activity center. The historic brick Postel Mill building should also be developed, and a suggested concept is described on the next page.*

CENTRAL KEY SITE CONCEPT: RAILWAY NUCLEUS



Postel Mill. This building, now vacant, is a dominant building in the study area, partially because of its size and partially that it is free-standing, the last vestige of the business that helped secure Mascoutah's future. Attempts have been made to reuse it, but they have fallen short. A natural reuse program is residential, but that kind of adaptive reuse can be prohibitively expensive outside of high-rent markets. Another, more affordable use would be to return the building to its original role – a center for enterprise. This concept suggests a retail use like a brewer-pub on the lower level with conversion of the upper levels to flexible space for innovative and start-up businesses. Examples of this program are illustrated on this page. Left column: The Mill, a conversion of a downtown mill to innovation space in Bloomington., Indiana. This building was developed and is owned by the city of Bloomington. Middle column: Banbury Place, a reuse of a formerly vacant US Rubber plant in Eau Claire, Wisconsin. This very large project redeveloped this facility into a business center with occupants that range from heavy industries to T-shirt screen printers.



CENTRAL KEY SITE CONCEPT: JEFFERSON GATEWAY



Merriam (KS) Marketplace. This project is developed in the middle of a large municipal parking lot.

Gateway Market. The city parking lot on the southwest corner of Main and Jefferson is important for serving businesses at and around this key highway junction, including the Visitors Center. However, it also presents a weak arrival to the city at a very highly visible location. The problem becomes one of presenting activity and definition at the corner while retaining most of the parking at the site. This concept proposes a market shelter using the landscaped strip and the first bay of parking in the lot. A shelter would provide an edge along Main and a covered connection to the Visitors Center and coffee shop building. It would provide a place for vendors, a farmers' market, and other events. It could be designed as pedestrian space (as with the Marketplace in Merriam, Kansas shown at left, or over parking when not used for other purposes, as with Ogallala, Nebraska's Rendezvous Square below. Rendering at lower left illustrates a concept for such a shelter around a corner parking lot, a very similar setting to the Mascoutah site.

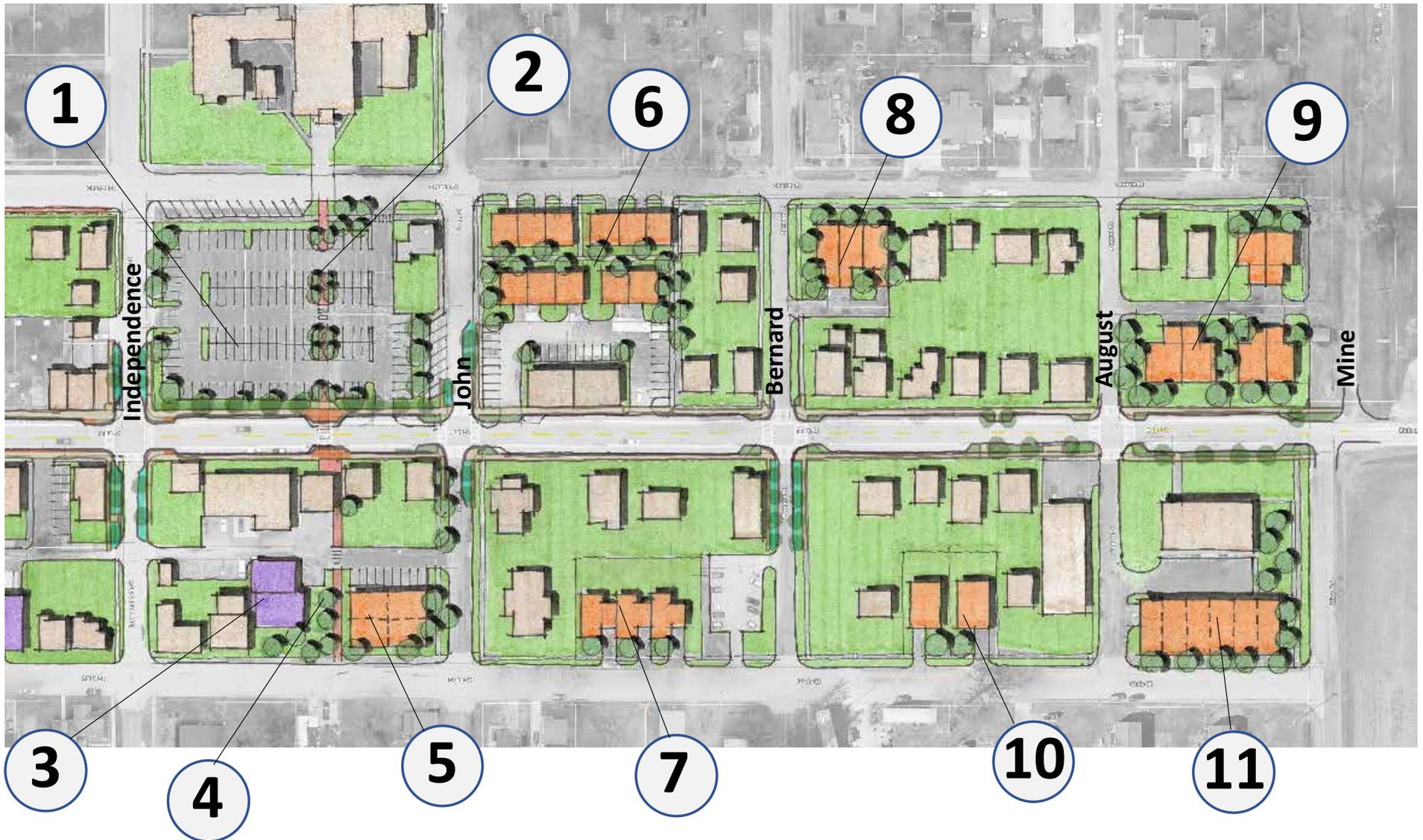
Figure 14: Market shelter concept for a corner parking lot



Rendezvous Square in Ogallala, NE. Site was a city parking lot and area under shelter is used for parking when not used for other purposes.

EAST SEGMENT PLAN

Figure 15: Detailed Concept Plan for East Segment



EAST SEGMENT PLAN

Figure 16: East Segment Components

| Map Key | Project | Notes |
|-----------|--|--|
| 1 | Holy Childhood parking redesign | One of Civic Anchors. Parking lot redesign to landscape lot and provide a promenade through the parking area that connects the historic front façade of the church to Main Street, and continuing on to State Street. Adds 10 parking stalls despite providing the promenade and buffer areas. |
| 2 | Holy Childhood Promenade | Landscaped promenade connecting church and Main Street |
| 3 | Innovation space | Workshop space along south extension of promenade |
| 4 | Promenade Extension | Continues walkway to State Street, opening up enterprise area |
| 5 | Live/work units | Four units with workshop/gallery space at street level and loft residential units above |
| 6 | Attached Cottage Homes | 11-unit redevelopment of small mobile home cluster with central greenway |
| 7 | Semi-attached units | Three infill units with garages buffering units from each other, along State Street |
| 8 | Infill townhomes | Three infill rowhouse units with rear access |
| 9 | Single-family attached infill | Six attached units with central drive, using largely vacant site on east edge of city |
| 10 | Small lot homes | Two small lot single-family homes |
| 11 | Rowhouses | Six rowhouse units behind existing historic multifamily conversion |



Potential development sites. From left: Extension of the proposed Holy Childhood Promenade reaches industrial buildings that could be used for a number of purposes or replaced with new flexible buildings; potential residential infill site.



Medium-Density Residential. *Medium-density housing is appropriate on infill or redevelopment sites that are relatively plentiful in the east segment. These sites can accommodate up to 40 new housing unit. In addition, polling participants clearly favored these housing forms in and around the Main Street study area. The photographs on this page illustrate some examples of urban family housing appropriate in the Main Street district. Clockwise from top left: small-lot single-family (Fayetteville, AR); semi-attached townhomes (Omaha, NE); rowhouse townhomes (Bloomington, IN); small footprint multi-family (Papillion, NE)*



Residential in the area. *Top: Mobile home cluster along Church Street between John and Bernard provides a future housing development opportunity. Above: Church Street with rural section should be improved to encourage new construction on vacant sites.*



Holy Childhood Promenade. *Modification of the church's asphalt parking lot maintains existing parking but both improves the appearance and stormwater performance of this lot and connects the landmark church to Main Street. Parking lot project should involve a partnership of the city and the parish for the benefit of this part of the study area.*

PART FOUR: IMPLEMENTATION

Cost Ranges and Implementation Strategies

PLANNING LEVEL COST OPINIONS

The concepts described on the previous pages of this paper have a number of both public and private components. The discussion in this section will concentrate on the four focus sites: the Museum/Pig Creek Path, Railway Nucleus, Jefferson Gateway, and Holy Childhood Promenade. It will also consider the recommendations for cooperative parking along Church Street and some general assumptions about potential private development. It does not include estimates for a future Main Street (Illinois Route 177) reconstruction project. This would be financed largely with federal and state transportation funding and the local share of a project that also includes sidewalk construction will ultimately be negotiated as that project is scheduled.

The opinions included here are planning level projections, designed to provide the community and decision-makers with a very general idea of costs. It will help the City consider feasibility, budgeting, and phasing issues, but actual cost estimates take place during the design process for individual projects.

Figure 18: Planning Level Cost Opinion: Museum/Pig Creek Path

| Component | Units | Unit Cost | Total | Notes |
|--|-----------|-----------|------------------|--|
| SITE PREPARATION | | | | |
| Path Construction | 7380 SF | \$7 | \$51,660 | |
| Main Street Crossing | 1 EA | \$25,000 | \$25,000 | May be integrated into a Route 177 project |
| Church Street Crossing | 1 EA | \$25,000 | \$25,000 | |
| Corridor landscaping and sitework | 31,700 SF | \$5 | \$158,500 | |
| Lighting | 16 EA | \$8,000 | \$128,000 | Pedestrian scaled lighting |
| Interpretive Monuments | 25 EA | \$2,000 | \$50,000 | |
| Moose Park Development | 11,400 SF | \$5 | \$57,000 | |
| Pig Creek Footbridge | 1 EA | \$75,000 | \$75,000 | |
| | | | | |
| SUBTOTAL | | | 570,160 | |
| 10% MOBILIZATION/GENERAL CONDITIONS | | | 57,016 | |
| 10% CONTINGENCY | | | 57,016 | |
| DESIGN FEES | | | 82,103 | |
| SURVEY | | | 15,000 | |
| | | | | |
| PROJECT TOTAL | | | \$781,295 | |

Figure 19: Railway Node

| Component | Units | Unit Cost | Total | Notes |
|----------------------------------|-----------|-------------|------------------|-------|
| SITE PREPARATION | | | | |
| Pavement Removals | 34,000 SF | \$1.50 | \$51,000 | |
| Tree Removal | 3 EA | \$400.00 | \$1,200 | |
| Grading | 4,000 CY | \$15.00 | \$60,000 | |
| Site Prep Subtotal | | | \$112,200 | |
| UTILITIES | | | | |
| Stormwater | Allowance | \$15,000.00 | \$15,000 | |
| Lighting - includes distribution | 10 | \$8,000.00 | \$80,000 | |
| Upgraded Water Service if needed | Allowance | \$25,000.00 | \$25,000 | |
| Wifi/Security | Allowance | \$12,000.00 | \$12,000 | |
| Sanitary Sewer | Allowance | \$15,000.00 | \$15,000 | |
| Utilities Subtotal | | | \$147,000 | |
| HARDSCAPE | | | | |
| 5" Concrete Sidewalks | 6,900 SF | \$7.00 | \$48,300 | |
| Retaining Wall behind stage | 360 SF | \$100.00 | \$36,000 | |
| Hardscape Subtotal | | | \$84,300 | |
| PARKING | | | | |
| Pavement | 8,120 SF | \$9.00 | \$73,080 | |
| Sidewalks | 1,110 SF | \$7.00 | \$7,770 | |
| Curbs | 60 | \$35.00 | \$2,100 | |
| Storm Sewer | Allowance | \$20,000.00 | \$20,000 | |
| Parking Subtotal | | | \$103,000 | |



Figure 19: Railway Node (Continued)

| Component | Units | Unit Cost | Total | Notes |
|---|-----------|--------------|------------------|--------------------------------|
| PERFORMANCE STAGE | | | | |
| Concrete Stage + Footings | 1,400 SF | \$30.00 | \$42,000 | |
| Stairs | 304 LF | \$35.00 | \$10,640 | |
| Ramp with railing | 260 SF | \$15.00 | \$3,900 | |
| Feature Wall | Allowance | \$100,000.00 | \$100,000 | Possible use of wood from mill |
| Electrical Allowance | Allowance | \$50,000.00 | \$50,000 | |
| Performance Stage Subtotal | | | \$206,000 | |
| WATER FEATURE | | | | |
| Feature Budget Subtotal | Allowance | \$200,000 | \$200,000 | |
| LANDSCAPING | | | | |
| Shade Trees | 15 EA | \$650.00 | \$9,750 | |
| Plant Beds (including mulch & soil and plantings) | 3,800 SF | \$15.00 | \$57,000 | |
| Lawn - Includes amended soil | 10,000 SF | \$1.00 | \$10,000 | |
| Irrigation for Landscape | 14,800 SF | \$2.00 | \$29,600 | |
| Landscaping Subtotal | | | \$106,400 | |
| OTHER COMPONENTS | | | | |
| Signage | Allowance | \$20,000.00 | \$20,000 | |
| Litter Receptacles | 3 EA | \$2,000.00 | \$6,000 | |
| Bike Racks | 4 EA | \$500.00 | \$2,000 | |
| Benches | 4 EA | \$2,250.00 | \$9,000 | |
| Other Subtotal | | | \$37,000 | |



Figure 19: Railway Node (Continued)

| Component | Units | Unit Cost | Total | Notes |
|-------------------------------------|-------|-----------|--------------------|----------------|
| | | | | |
| SUBTOTAL | | | \$893,500 | |
| 10% MOBILIZATION/GENERAL CONDITIONS | | | \$89,350 | |
| 10% CONTINGENCY | | | \$89,350 | |
| DESIGN FEES | | | \$128,664 | 12% for design |
| SURVEY | | | \$15,000 | |
| | | | | |
| PROJECT TOTAL | | | \$1,215,864 | |



Performance Stage. Performance stage at Thompson Park (formerly Santa Fe Commons) in Overland Park, KS. The stage shelter is designed to recall the important role that this city and site had in early aviation.

Figure 20: Jefferson Gateway

| Component | Units | Unit Cost | Total | Notes |
|--|-----------|-----------|------------------|------------------------|
| Pavement Removal | 5,400 SF | \$1.50 | \$8,100 | |
| Grading | 750 CY | \$15.00 | \$14,167 | |
| Lighting | 6 EA | \$8,000 | \$48,000 | |
| Water Service | Allowance | \$10,000 | \$10,000 | |
| Landscaping | 3,400 SF | \$5 | \$17,000 | |
| Plaza Paving | 4,800 SF | \$15 | \$72,000 | |
| Market Shelters | 2,800 SF | \$80 | \$224,000 | Two shelter structures |
| Parking Lot Repair | 4,250 SF | \$9 | \$38,250 | |
| Signage | Allowance | \$10,000 | \$10,000 | Entrance signage |
| Litter Receptacles | 3 EA | \$2,000 | \$6,000 | |
| Bike Racks | 2 EA | \$500 | \$1,000 | |
| Benches | 4 EA | \$2,250 | \$6,750 | |
| | | | | |
| SUBTOTAL | | | \$455,267 | |
| 10% MOBILIZATION/GENERAL CONDITIONS | | | \$45,527 | |
| 10% CONTINGENCY | | | \$45,527 | |
| DESIGN FEES | | | \$65,558 | |
| SURVEY | | | \$15,000 | |
| | | | | |
| PROJECT COST OPINION | | | \$626,878 | |



Figure 21: Holy Childhood Promenade

| Component | Units | Unit Cost | Total | Notes |
|--|-------------|-----------|------------------|---------------------------------|
| Pavement Removal | 8,000 SF | \$1.50 | \$12,000 | |
| Other Site Clearance | 3,600 SF | \$1.50 | \$5,400 | |
| Grading | 1,500 CY | \$15 | \$22,500 | |
| Lighting | 6 | \$8,000 | \$48,000 | |
| Water Service | 1 | \$10,000 | \$10,000 | |
| Curbs | 1,200 LF | \$22 | \$26,400 | |
| Promenade Path | 2200 SF | \$15 | \$33,000 | |
| Landscaping | 2880 SF | \$10 | \$28,800 | |
| Trees | 25 EA | \$650 | \$16,250 | |
| New Lot Paving | 14,600 SF | \$9 | \$131,400 | |
| Striping | 122 Stripes | \$11 | \$1,342 | 18-foot |
| Signage | Allowance | \$10,000 | \$10,000 | Sign replacement or move, panel |
| Benches | 4 | \$2,250 | \$9,000 | |
| | | | | |
| SUBTOTAL | | | \$354,092 | |
| 10% MOBILIZATION/GENERAL CONDITIONS | | | \$35,409 | |
| 10% CONTINGENCY | | | \$35,409 | |
| DESIGN FEES | | | \$50,989 | |
| SURVEY | | | \$15,000 | |
| | | | | |
| PROJECT COST OPINION | | | \$490,900 | |

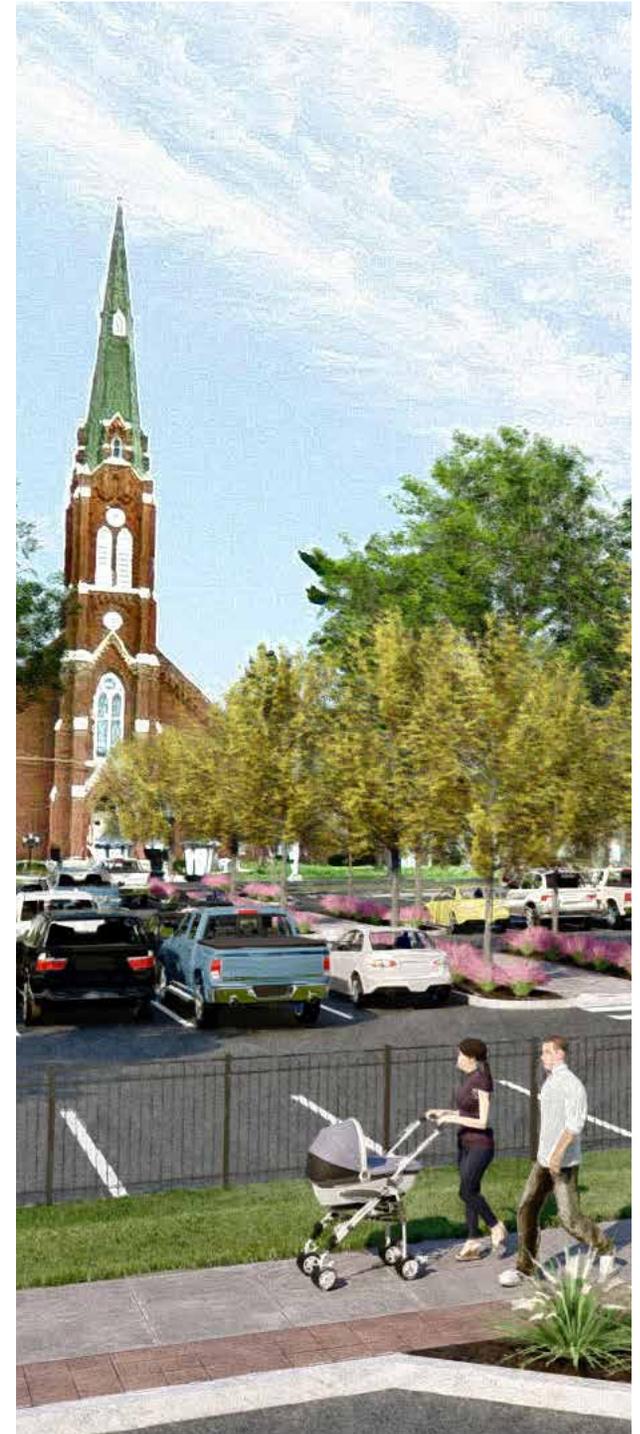


Figure 22: Parking Area Cost Opinions

| Component | Unit Cost | Jefferson to Lebanon | Lebanon to Market | Market to Railway | Railway to 1st East (City hall) | Railway to 1st West | 1st to 2nd | Notes |
|--|--------------------|----------------------|-------------------|-------------------|---------------------------------|---------------------|------------------|--|
| Pavement Removal | \$1.50/SF | \$36,000 | \$3,000 | \$3,600 | \$32,400 | \$10,260 | \$53,100 | |
| Grading | \$15/CY | \$20,000 | \$30,000 | \$2,000 | \$18,000 | \$11,400 | \$59,000 | |
| New Paving | \$9/SF | \$216,000 | \$18,000 | \$21,600 | \$194,400 | \$61,560 | \$318,600 | |
| Curbs | \$22/LF | \$11,968 | \$5,280 | \$6,600 | \$13,200 | \$12,320 | \$19,800 | |
| Striping | \$11/stripe | \$572 | \$308 | \$594 | \$495 | \$792 | \$781 | |
| Landscaping | \$10/SF | \$33,000 | \$24,000 | \$18,000 | \$1,500 | \$1,500 | \$15,000 | |
| Trees | \$650 EA | \$5,200 | \$0 | \$2,600 | \$1,300 | \$1,300 | \$4,550 | |
| Lighting | \$5,000 EA | \$30,000 | \$20,000 | \$20,000 | \$30,000 | \$30,000 | \$30,000 | |
| Pedestrian Paths | \$7-10/SF | \$21,000 | \$4,200 | \$18,000 | \$22,800 | \$22,800 | \$21,600 | Includes sidewalk along south side of Church |
| Furnishings | \$10,000 allowance | \$10,000 | \$10,000 | \$10,000 | \$10,000 | \$10,000 | \$10,000 | Bike racks, signage, receptacles |
| | | | | | | | | |
| SUBTOTAL | | \$383,740 | \$114,788 | \$102,994 | \$324,095 | \$161,932 | \$532,431 | |
| 10% MOBILIZATION/GENERAL CONDITIONS | | \$38,374 | \$11,479 | \$10,299 | \$32,410 | \$16,193 | \$53,243 | |
| 10% CONTINGENCY | | \$38,374 | \$11,479 | \$10,299 | \$32,410 | \$16,193 | \$53,243 | |
| DESIGN FEES | | \$55,259 | \$16,529 | \$14,831 | \$46,670 | \$23,318 | \$76,670 | |
| SURVEY | | \$15,000 | \$15,000 | \$15,000 | \$15,000 | \$15,000 | \$15,000 | |
| | | | | | | | | |
| PROJECT COST OPINION | | \$530,747 | \$169,275 | \$153,424 | \$450,584 | \$232,637 | \$730,587 | |

NOTES ON IMPLEMENTATION

This section is not intended to be a comprehensive implementation program for Main Street. However, we hope it provides some guidance for moving forward to help the district achieve its potential.

Our analysis and a review of the polling results indicate that the future this study area depends on 1) providing more reason for more people to visit Main Street and 2) offering those people a positive experience while there. Satisfying these outcomes will depend on catalytic projects that generate other positive private sector responses: more retailing and eating places, infill housing development on available sites largely around the corridor, and new business development. Of the various projects and initiatives that relate to land use and urban design, we believe the four projects are of special importance:

- The Railway Node, combining a major public common space, retail, restaurants, and historic preservation to restore the traditional center of Main Street Macoutah.
- The Pig Creek Path, connecting the study corridor to its history and its neighborhoods by combining information and recreation.
- The Church Street parking corridor, which improves function and the customer experience, providing the opportunity to widen Main Street sidewalks with a future Route 177 reconstruction project.
- The Jefferson Gateway project, providing a welcoming activity area at this key arrival point in the district, while retaining necessary parking. The first three of these initia-

tives received consistent positive responses throughout the public polling process. The fourth grew in support as the plan evolved and could provide a highly visible tactical first step, particularly in view of the intersection improvement project completed in 2020 at Jefferson and Main.

Two important initial steps can help create the conditions that will help implement these catalytic initiatives. The first of these is organizational: the creation (or re-creation) of a central district organization. In the past, Mascoutah had a Main Street program, which ended when the state discontinued funding. This planning effort will be successful if it sparks a realization of common interest among various stakeholders and provides a new and achievable agenda for such a re-formed group. Also, a central district organization will be necessary to manage the cooperative effort necessary to achieve the parking corridor concept, which transcends existing property lines and territories for the benefit of everyone in the district.

The second is seeking listing on the National Register of Historic Places for relevant parts of the central district. The primary focus of an NRHP district would be Main Street from Jefferson to 1st between the two alleys, while including the whole of the Postel block south to State Street. This opens the possibility of tax credits for key buildings in the area, most notably the Postel Mill building. It is also thematically consistent with the concept of the Pig Creek Path.

Several programs available to Mascoutah through local initiative or Illinois law appear to have special relevance to accomplishing these key projects:

- **Private philanthropy.** The role of the private



Private philanthropy. Thompson Park (once called Santa Fe Commons) in downtown Overland Park, KS was made possible largely through the gift of a private donor who wanted to leave a great park as a legacy to his town. The program for the park is very similar to that proposed for the Railway Node.

sector and individuals who would like to leave a legacy behind for their community should never be dismissed. This could be especially relevant at the Railway node, where a key property owner has a vision and a clear public interest.

- **A community bond issue.** A bond issue that provides substantial capital funding for catalytic projects could be money well-spent for “product enhancement” of a city in a growth

cycle, positioned to take advantage of its surrounding assets.

- **Transportation funding.** The future rebuilding of Illinois Route 177 provides enormous opportunities, and execution of the Church Street parking corridor allows that project to make major improvement in the customer environment along the Main Street. Funding through the Transportation Alternatives Program (TAP) will also be important in developing projects such as a Church Street “complete corridor” with shared use path on the north side and a continuous sidewalk with links to Main Street on the south side and other projects identified in the Transportation White Paper of this project.
- **Special Service Area Program (SSA).** SSA’s are the Illinois equivalent of business improvement districts, funding capital projects, promotions, and operations. An SSA may well be necessary to develop and manage the Church Street parking corridor. Currently, some parking lots are privately owned but provide informal parking for the entire district. An SSA effectively operating as a parking manager would more equitably distribute maintenance cost and provide a mechanism for finding functional and aesthetic improvements.
- **Tax Increment Financing (TIF).** TIF, using sales taxes and incremental property taxes generated in a district for funding improvements within that district. Typical uses in TIF include redevelopment of substandard, obsolete, or vacant buildings; public infrastructure; improvements to increase “the viability of downtown business districts;” and rehabilitation of historic properties. TIF is especially applicable to the reuse of the Postel Mill building, and other area improvements.
- **Business District Development and Redevelopment Sales Tax.** This allows the city to “impose a tax designed to fund the development or redevelopment of certain designated areas in a municipality.” The tax requires that the area is contiguous, includes only parcels that will directly benefit from the program, and is blighted as defined by the Illinois Municipal Code. This may be less immediately relevant because of the relatively small amount of retail existing in the study area, but may be a useful tool in the future.
- **Housing Programs.** The Illinois Housing Development Authority (IHDA) has a variety of both consumer and developer-oriented programs to finance housing. Many of these programs are relevant to infill development proposed for areas immediately adjacent to the study area. For economies of scale, a single developer could consolidate a number of small projects into a single package of what is referred to as “missing middle” housing – affordable, medium-density owner-occupied and rental development.

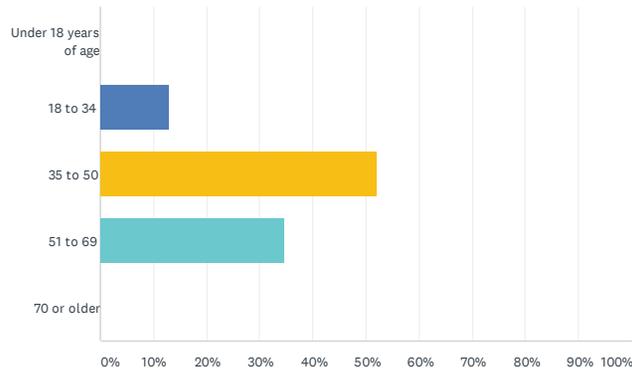


APPENDIX E-1

Survey Results - Round 1

Q1 How old are you?

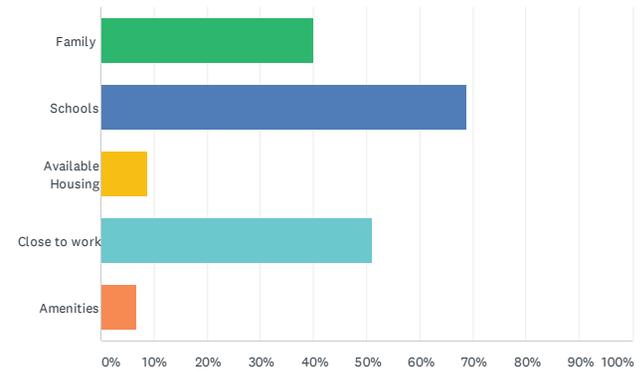
Answered: 46 Skipped: 0



| ANSWER CHOICES | RESPONSES | Count |
|-----------------------|-----------|-----------|
| Under 18 years of age | 0.00% | 0 |
| 18 to 34 | 13.04% | 6 |
| 35 to 50 | 52.17% | 24 |
| 51 to 69 | 34.78% | 16 |
| 70 or older | 0.00% | 0 |
| TOTAL | | 46 |

Q2 Why did you choose to live in Mascoutah? (Check all that apply)

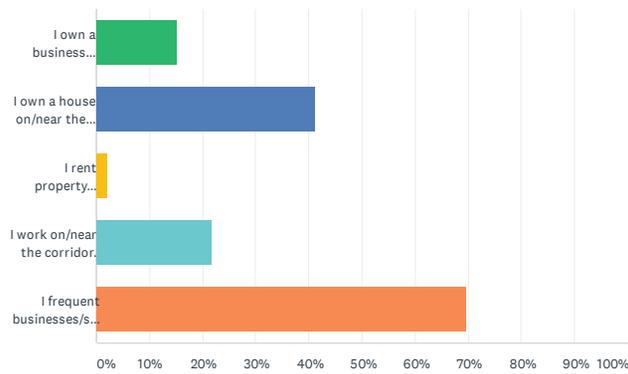
Answered: 45 Skipped: 1



| ANSWER CHOICES | RESPONSES | Count |
|------------------------------|-----------|-------|
| Family | 40.00% | 18 |
| Schools | 68.89% | 31 |
| Available Housing | 8.89% | 4 |
| Close to work | 51.11% | 23 |
| Amenities | 6.67% | 3 |
| Total Respondents: 45 | | |

Q3 What best describes your connection to Main Street in Mascoutah? (Check all that apply)

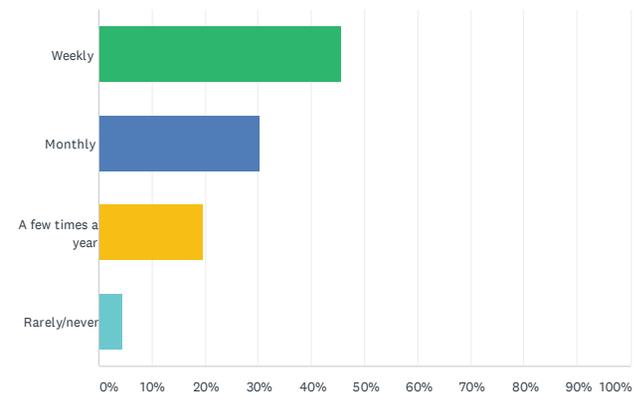
Answered: 46 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| I own a business on/near the corridor. | 15.22% 7 |
| I own a house on/near the corridor. | 41.30% 19 |
| I rent property on/near the corridor. | 2.17% 1 |
| I work on/near the corridor. | 21.74% 10 |
| I frequent businesses/shops on the corridor. | 69.57% 32 |
| Total Respondents: 46 | |

Q4 How frequently do you visit businesses/shops on Main Street?

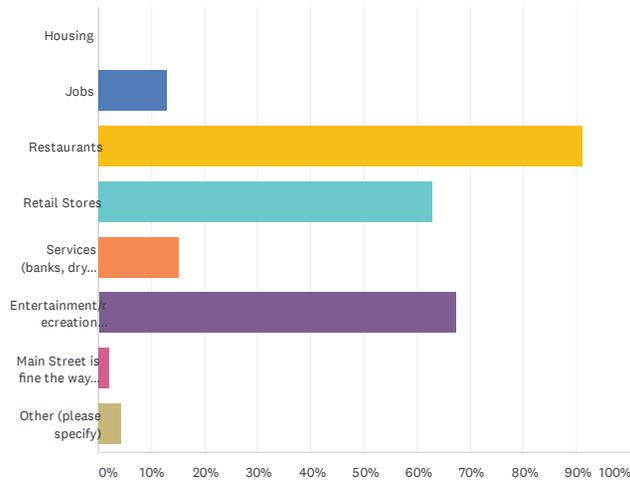
Answered: 46 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--------------------|-----------|
| Weekly | 45.65% 21 |
| Monthly | 30.43% 14 |
| A few times a year | 19.57% 9 |
| Rarely/never | 4.35% 2 |
| TOTAL | 46 |

Q5 What does Main Street need more of? (select all that apply)

Answered: 46 Skipped: 0

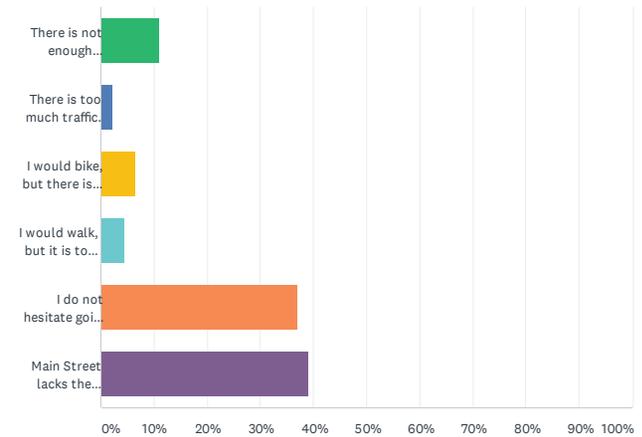


| ANSWER CHOICES | RESPONSES |
|---|-----------|
| Housing | 0.00% 0 |
| Jobs | 13.04% 6 |
| Restaurants | 91.30% 42 |
| Retail Stores | 63.04% 29 |
| Services (banks, dry cleaners, salons, fitness centers) | 15.22% 7 |
| Entertainment/recreation venues or events | 67.39% 31 |
| Main Street is fine the way it is | 2.17% 1 |
| Other (please specify) | 4.35% 2 |
| Total Respondents: 46 | |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|---|--------------------|
| 1 | Main street needs more restaurants, bars, small music venues, and shops. | 8/19/2020 7:51 AM |
| 2 | It wouldn't hurt to plant more trees and convert an unused space into a nice place to see nature/walk | 8/16/2020 11:34 PM |

Q6 What makes you hesitate coming to Main Street?

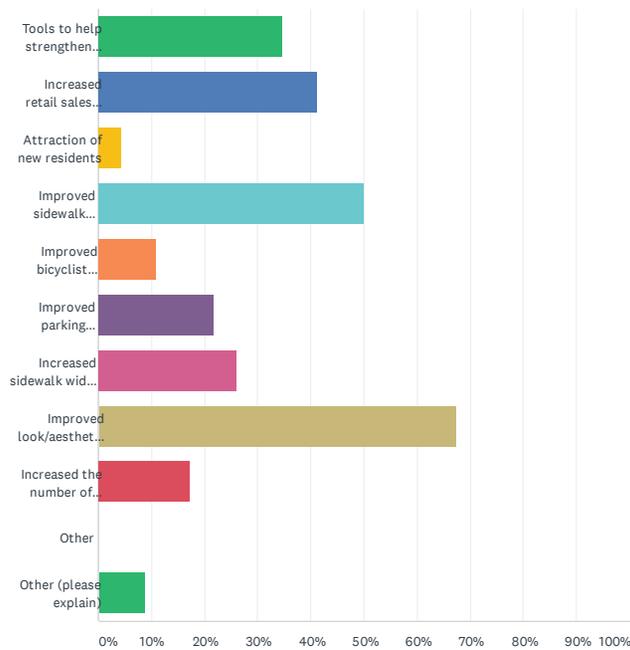
Answered: 46 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| There is not enough convenient parking. | 10.87% 5 |
| There is too much traffic. | 2.17% 1 |
| I would bike, but there is no good route. | 6.52% 3 |
| I would walk, but it is too difficult/unsafe. | 4.35% 2 |
| I do not hesitate going to Main Street. | 36.96% 17 |
| Main Street lacks the businesses or services I use. | 39.13% 18 |
| TOTAL | 46 |

Q7 I would consider the Mascoutah Great Streets project a success if it results in (select your top 3):

Answered: 46 Skipped: 0

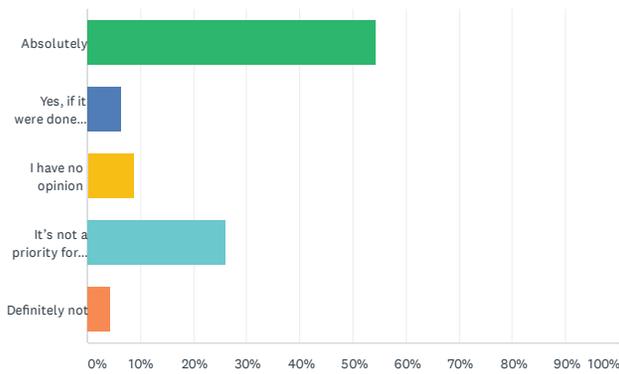


| ANSWER CHOICES | RESPONSES |
|---|-----------|
| Tools to help strengthen downtown businesses | 34.78% 16 |
| Increased retail sales for local retailers | 41.30% 19 |
| Attraction of new residents | 4.35% 2 |
| Improved sidewalk conditions and pedestrian safety | 50.00% 23 |
| Improved bicyclist safety | 10.87% 5 |
| Improved parking conditions | 21.74% 10 |
| Increased sidewalk width to allow for outdoor dining | 26.09% 12 |
| Improved look/aesthetics of corridor | 67.39% 31 |
| Increased the number of public spaces (plazas, gathering areas) | 17.39% 8 |
| Other | 0.00% 0 |
| Other (please explain) | 8.70% 4 |
| Total Respondents: 46 | |

| # | OTHER (PLEASE EXPLAIN) | DATE |
|---|---|--------------------|
| 1 | Shops that you can pop in/out of and visit. Boutiques, home decor, art/painting, wine and cigar bar, etc. There are too many "service type businesses" (hair salon, lawyer, bank, etc.). | 8/19/2020 5:18 PM |
| 2 | Main street lacks character. it is not scenic, nor reflective of our history. The Indian Mural is the bright spot, and now that we are losing the 'mill' one of the only buildings of character. Ace hardware is a beautiful building, as is the old movie theater and Museum, but we lack character. | 8/18/2020 2:15 PM |
| 3 | No parking on Main at would help. | 8/16/2020 8:54 PM |
| 4 | Feeling like you are not entering the Earth's atmosphere, when you are driving down main Street. | 8/13/2020 11:59 PM |

Q8 Would you like to see the recent bicycle/pedestrian path expanded to connect more Mascoutah neighborhoods and Main Street?

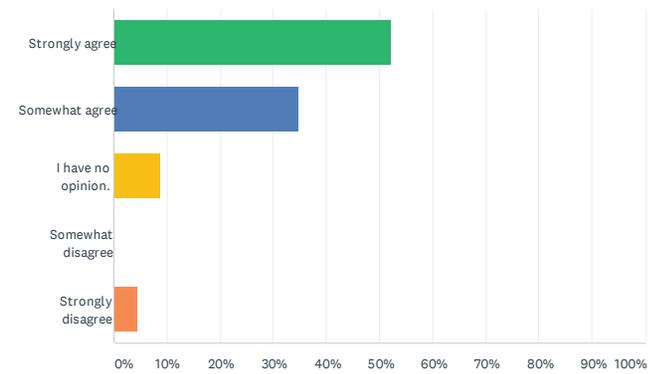
Answered: 46 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| Absolutely | 54.35% 25 |
| Yes, if it were done differently than the existing trail. | 6.52% 3 |
| I have no opinion | 8.70% 4 |
| It's not a priority for me, but I would not oppose the idea. | 26.09% 12 |
| Definitely not | 4.35% 2 |
| TOTAL | 46 |

Q9 What is your thought on the following statement: "Main Street Mascoutah should be a destination for regional diners and shoppers."?

Answered: 46 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--------------------|-----------|
| Strongly agree | 52.17% 24 |
| Somewhat agree | 34.78% 16 |
| I have no opinion. | 8.70% 4 |
| Somewhat disagree | 0.00% 0 |
| Strongly disagree | 4.35% 2 |
| TOTAL | 46 |

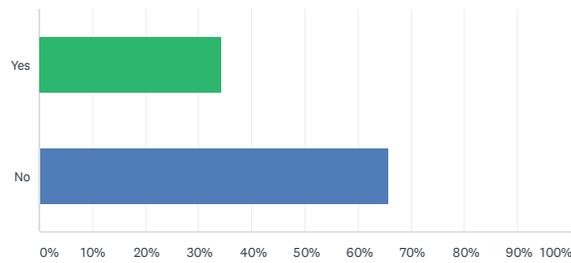
Q10 Do you have any additional comments on this project?

Answered: 21 Skipped: 25

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Do a First Friday of the month to close down Main St and allow future diners/shops to extend their business outdoors during non-winter months. | 8/19/2020 5:18 PM |
| 2 | This street has been neglected for years; crumbling sidewalks, places where the street is higher than the sidewalk with no curb left. It is in desperate need of a facelift! | 8/19/2020 9:24 AM |
| 3 | Mascoutah has so much potential! It can be a cute/hip area, it just needs some more local eateries, maybe a brewery, and some small music venues. Mix the old with the new and create a better pedestrian/biking environment. | 8/19/2020 7:51 AM |
| 4 | The current condition of Main Street appears run down, there is also a lack of restaurants or retail to entice residents and visitors. O'Fallon IL has recently revamped their downtown and made it a destination for people to visit. I would love to see something similar in Mascoutah. | 8/18/2020 6:45 PM |
| 5 | Improve curb appeal to existing buildings, include proper sidewalks and curbs. Expand the bike path to include residents South of Main Street. | 8/18/2020 5:35 PM |
| 6 | We have a wonderful small town, with history and character. Main street should reflect our history and character, and be classy and aesthetic. That does not mean it needs to be loaded with shops and restaurants, but a nice place for residents and visitors to walk, pruse shops, and enjoy social time. | 8/18/2020 2:15 PM |
| 7 | I think any development/improvement is a great idea and would support any ideas in furtherance of that. | 8/18/2020 11:50 AM |
| 8 | Add a turn lane and get rid of parking on main st. | 8/18/2020 10:04 AM |
| 9 | The city needs to start looking long term, we have missed multiple opportunities over the years because we don't have a Mayor, councilmen or City Manager with business sense. Way too much of "look at me and what I've done" rather than what's best for the overall city long term. I've lived here my entire life, and for the most part it's always been that way. The small business owners that have the knowledge to help the city on the council simply don't want to put up with the aggravation of it all. Which is a shame because we have some very bright and talented small business owners that would help this city immensely I'm not sure that the effort and cost of a project like this will reap any significant rewards other than a sign entering town saying that we have participated... | 8/18/2020 9:28 AM |
| 10 | I am excited to see how the town changes in the coming years, no matter what is eventually decided on. It's also amazing and a source of community pride that this project is taking place; I love it. | 8/16/2020 11:34 PM |
| 11 | Main St should feel like downtown Belleville, not a dump. | 8/16/2020 8:54 PM |
| 12 | I would like to see Main street stay with a historical theme. | 8/14/2020 12:14 PM |
| 13 | No, I made a statement in question 7 | 8/13/2020 11:59 PM |
| 14 | No | 8/13/2020 11:40 PM |
| 15 | main st. is not inviting for consumers. Many buildings are not maintained and very little parking. we need more economic growth | 8/13/2020 10:10 PM |
| 16 | No chain stores or restaurants. Encourage small business owners: bakery, book store, butcher shop, shoe store, clothing store, boutiques. | 8/13/2020 7:56 PM |
| 17 | Clean up sidewalks. They look awful. | 8/13/2020 6:39 PM |
| 18 | Utilities should be lowered to compete w other local rates. Limit the amount of bars. | 8/13/2020 6:12 PM |
| 19 | There needs to be an examination of WHY there isn't more business growth in Mascoutah? As a homeowner, taxpayer, and former renter I believe the taxes and high cost of rents (residential and commercial) greatly hinder growth | 8/13/2020 6:01 PM |
| 20 | The string lights over the street in Belleville looks beautiful and inviting. I'd love to see them here. | 8/13/2020 5:33 PM |
| 21 | You must also lower utilities for businesses for anything on Main Street to be successful! | 8/13/2020 5:09 PM |

Q1 Do you access Silver Creek or the surrounding floodplain for recreational activities?

Answered: 35 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----------|
| Yes | 34.29% | 12 |
| No | 65.71% | 23 |
| TOTAL | | 35 |

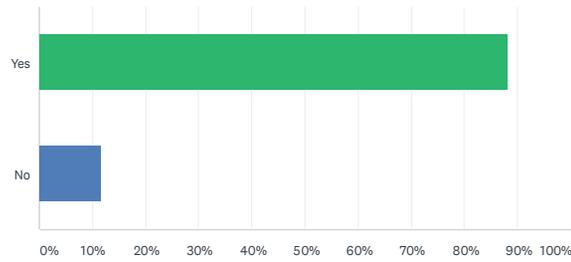
Q2 What do you use Silver Creek or the surrounding floodplain to do?

Answered: 11 Skipped: 24

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Walking and enjoying a bottle of wine. | 8/23/2020 9:05 PM |
| 2 | I walk at the preserve | 8/20/2020 11:58 AM |
| 3 | Hike | 8/19/2020 9:45 PM |
| 4 | Nice walking area for the dog and myself. | 8/19/2020 8:16 AM |
| 5 | hike | 8/18/2020 9:43 PM |
| 6 | Walk | 8/18/2020 12:29 PM |
| 7 | hiking | 8/13/2020 10:14 PM |
| 8 | Walking, bird watching, photography. | 8/13/2020 8:03 PM |
| 9 | Walk, spend time in nature | 8/13/2020 6:08 PM |
| 10 | Bike/walk preserve | 8/13/2020 5:41 PM |
| 11 | Walking and runn8ng. | 8/13/2020 5:14 PM |

Q3 Do you travel away from Mascoutah to experience the outdoors?

Answered: 34 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----------|
| Yes | 88.24% | 30 |
| No | 11.76% | 4 |
| TOTAL | | 34 |

Q4 Where do you go?

Answered: 27 Skipped: 8

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Wineries, fishing, hiking, boating | 8/23/2020 9:08 PM |
| 2 | St Louis | 8/20/2020 4:10 PM |
| 3 | Southern Illinois | 8/20/2020 11:59 AM |
| 4 | O'Fallon or Grafton area | 8/20/2020 7:34 AM |
| 5 | Shawnee National Forest in Southern IL | 8/19/2020 9:46 PM |
| 6 | O'fallon or Belleville | 8/19/2020 5:25 PM |
| 7 | Carlyle Lake | 8/19/2020 1:19 PM |
| 8 | Carlyle Lake | 8/19/2020 12:38 PM |
| 9 | Shawnee National Forest | 8/19/2020 9:32 AM |
| 10 | Usually over to MO. St Louis has some nice parks that host outdoor activities (farmers markets, food truck nights in the park, outdoor festivals). I also like to kayak and hike, which tends to take me to MO. | 8/19/2020 8:17 AM |
| 11 | Missouri | 8/18/2020 9:44 PM |
| 12 | Carlyle Lake | 8/18/2020 6:45 PM |
| 13 | Carlyle Lake and Rend lake mostly | 8/18/2020 4:51 PM |
| 14 | nearby parks in surrounding communities, and in stl. we like to camp and hike and take the kids to the park to play | 8/18/2020 2:31 PM |
| 15 | MCT and MetroBike Link Trails | 8/17/2020 6:42 PM |
| 16 | Missouri | 8/16/2020 11:46 PM |
| 17 | Anywhere with parks, trails, walking paths, nature. | 8/16/2020 9:01 PM |
| 18 | Carlyle Lake | 8/16/2020 7:04 PM |
| 19 | state parks and recreation areas. any place with boat or walking docks near lakes | 8/14/2020 12:53 PM |
| 20 | State parks that has fishing and hiking | 8/14/2020 12:25 PM |
| 21 | Delmar on the Loop, MO Botanical Gardens, Forest Park, Central Westend in St. Louis, Downtown Belleville | 8/14/2020 12:19 PM |
| 22 | Carlyle | 8/13/2020 11:41 PM |
| 23 | Southern Illinois | 8/13/2020 10:15 PM |
| 24 | Shawnee National Forest or State Parks. | 8/13/2020 8:04 PM |
| 25 | various state parks; trails to walk/hike | 8/13/2020 6:09 PM |
| 26 | Southern Illinois, or north to Grafton area | 8/13/2020 6:05 PM |
| 27 | Locally: Moody Park, Shawnee, Carlyle, Forest Park | 8/13/2020 5:44 PM |

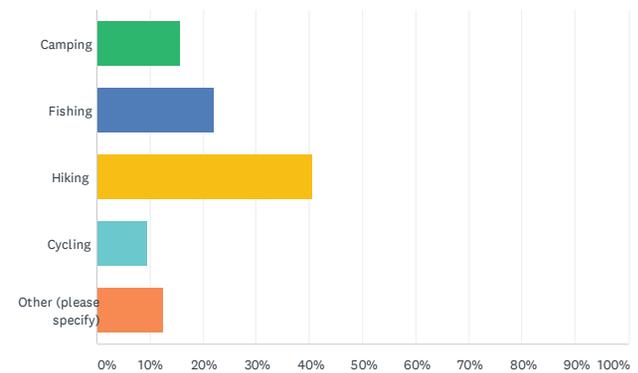
Q5 What do you do?

Answered: 26 Skipped: 9

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | See above. | 8/23/2020 9:08 PM |
| 2 | Bikeride, hike | 8/20/2020 4:10 PM |
| 3 | Hiking | 8/20/2020 11:59 AM |
| 4 | Walk, shop, | 8/20/2020 7:34 AM |
| 5 | Hike, swim, kayak, camp, ride bikes | 8/19/2020 9:46 PM |
| 6 | Dining and shops | 8/19/2020 5:25 PM |
| 7 | Walking and fishing | 8/19/2020 1:19 PM |
| 8 | Fishing and Boating | 8/19/2020 12:38 PM |
| 9 | Hike and bike | 8/19/2020 9:32 AM |
| 10 | St Louis has some nice parks that host outdoor activities (farmers markets, food truck nights in the park, outdoor festivals). I also like to kayak and hike, which tends to take me to MO. | 8/19/2020 8:17 AM |
| 11 | hike | 8/18/2020 9:44 PM |
| 12 | Fishing and swimming | 8/18/2020 6:45 PM |
| 13 | Boating and water recreation | 8/18/2020 4:51 PM |
| 14 | hike, take the kids to play, picnic, camp, play frisbee, recreation, etc. | 8/18/2020 2:31 PM |
| 15 | Cycle, walk | 8/17/2020 6:42 PM |
| 16 | Walk trails, things like Forest Park, visit streams, etc. | 8/16/2020 11:46 PM |
| 17 | Hike, walk, relax | 8/16/2020 9:01 PM |
| 18 | Boat | 8/16/2020 7:04 PM |
| 19 | fish, bike , hike | 8/14/2020 12:53 PM |
| 20 | Fish and hike | 8/14/2020 12:25 PM |
| 21 | Go out to eat, shop, hangout, go for walks, enjoy the outdoors | 8/14/2020 12:19 PM |
| 22 | Fish | 8/13/2020 11:41 PM |
| 23 | hike and kayak | 8/13/2020 10:15 PM |
| 24 | Hiking, fishing, photography, picnics, swimming. | 8/13/2020 8:04 PM |
| 25 | Hike, fish, boat | 8/13/2020 6:05 PM |
| 26 | Hike, playgrounds, swim, photography | 8/13/2020 5:44 PM |

Q6 Which of the following outdoor activities do you most often travel to experience?

Answered: 32 Skipped: 3



| ANSWER CHOICES | RESPONSES |
|------------------------|-----------|
| Camping | 15.63% 5 |
| Fishing | 21.88% 7 |
| Hiking | 40.63% 13 |
| Cycling | 9.38% 3 |
| Other (please specify) | 12.50% 4 |
| TOTAL | 32 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|---|--------------------|
| 1 | Motorcycle | 8/19/2020 1:26 PM |
| 2 | Walking | 8/19/2020 1:22 PM |
| 3 | basically everything except for Fishing. This community's fishing opportunity is 2nd to none. | 8/16/2020 11:51 PM |
| 4 | Walking trails | 8/14/2020 12:24 PM |

Q7 Where do you feel comfortable outdoors in downtown Mascoutah?

Answered: 28 Skipped: 7

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Scheve Park area. Definitely not downtown | 8/20/2020 4:10 PM |
| 2 | Yes | 8/20/2020 12:00 PM |
| 3 | Library | 8/20/2020 7:37 AM |
| 4 | all of it | 8/19/2020 9:48 PM |
| 5 | Every where | 8/19/2020 5:27 PM |
| 6 | yes | 8/19/2020 1:26 PM |
| 7 | Visiter's Center Porch | 8/19/2020 1:22 PM |
| 8 | Everywhere | 8/19/2020 12:40 PM |
| 9 | Anywhere | 8/19/2020 9:34 AM |
| 10 | Everywhere. | 8/19/2020 8:19 AM |
| 11 | During the day most places along main street | 8/18/2020 9:48 PM |
| 12 | Anywhere | 8/18/2020 6:46 PM |
| 13 | Everywhere, I feel very safe in this community | 8/18/2020 4:54 PM |
| 14 | walking along main street, and I like the benches in front of the Indian Mural | 8/18/2020 2:44 PM |
| 15 | Anywhere | 8/18/2020 12:30 PM |
| 16 | Anywhere and everywhere | 8/17/2020 6:44 PM |
| 17 | Between 6th st and Ace. | 8/16/2020 11:51 PM |
| 18 | Scheve park | 8/16/2020 7:06 PM |
| 19 | anyplace feels safe, it just looks like a city that doesn't care anymore | 8/14/2020 12:55 PM |
| 20 | Everywhere | 8/14/2020 12:32 PM |
| 21 | St. Louis Coffee World, there's outdoor seating, easily accessible for wheelchair users | 8/14/2020 12:24 PM |
| 22 | Nowhere | 8/13/2020 11:42 PM |
| 23 | walking on bike trail | 8/13/2020 10:16 PM |
| 24 | On sidewalks in lighted areas. | 8/13/2020 8:09 PM |
| 25 | bike trails, parks, preserve | 8/13/2020 6:11 PM |
| 26 | Nowhere...there aren't any real public outdoor spaces!!! | 8/13/2020 6:07 PM |
| 27 | Anywhere | 8/13/2020 5:49 PM |
| 28 | All over | 8/13/2020 5:16 PM |

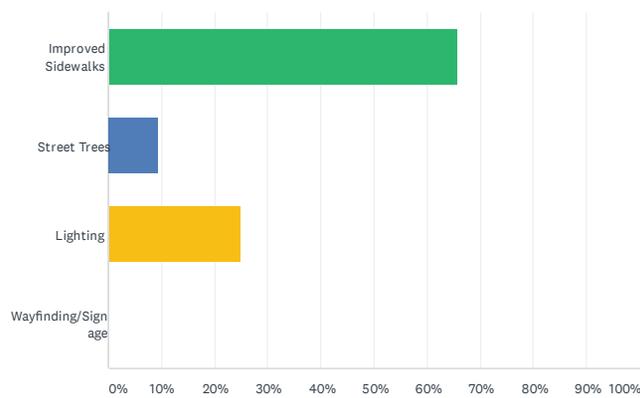
Q8 If you're running an errand in downtown Mascoutah, do you feel comfortable while walking between your car and the destination?

Answered: 30 Skipped: 5

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Yes | 8/23/2020 9:11 PM |
| 2 | yes | 8/23/2020 8:28 AM |
| 3 | Yes | 8/20/2020 12:00 PM |
| 4 | The streets are narrow and parking can be a nightmare. | 8/20/2020 7:37 AM |
| 5 | yes | 8/19/2020 9:48 PM |
| 6 | Yes | 8/19/2020 5:27 PM |
| 7 | yes | 8/19/2020 1:26 PM |
| 8 | Sometimes | 8/19/2020 1:22 PM |
| 9 | yes | 8/19/2020 12:40 PM |
| 10 | Absolutely | 8/19/2020 9:34 AM |
| 11 | 100% yes. | 8/19/2020 8:19 AM |
| 12 | yes | 8/18/2020 9:48 PM |
| 13 | Yes | 8/18/2020 6:46 PM |
| 14 | Yes, I've never had an issue | 8/18/2020 4:54 PM |
| 15 | of course | 8/18/2020 2:44 PM |
| 16 | Yes | 8/18/2020 12:30 PM |
| 17 | Yes | 8/17/2020 6:44 PM |
| 18 | yes, but more lighting is needed. | 8/16/2020 11:51 PM |
| 19 | Yes | 8/16/2020 9:06 PM |
| 20 | Yes | 8/16/2020 7:06 PM |
| 21 | yes, unless you mean falling or tripping over broken concrete | 8/14/2020 12:55 PM |
| 22 | Yes | 8/14/2020 12:32 PM |
| 23 | Yes | 8/14/2020 12:24 PM |
| 24 | Yes | 8/13/2020 11:42 PM |
| 25 | yes | 8/13/2020 10:16 PM |
| 26 | Yes | 8/13/2020 8:09 PM |
| 27 | yes | 8/13/2020 6:11 PM |
| 28 | Yes | 8/13/2020 6:07 PM |
| 29 | Yes | 8/13/2020 5:49 PM |
| 30 | Yes | 8/13/2020 5:16 PM |

Q9 Which improvement will make you feel most comfortable walking to your destination?

Answered: 32 Skipped: 3



| ANSWER CHOICES | RESPONSES | |
|--------------------|-----------|-----------|
| Improved Sidewalks | 65.63% | 21 |
| Street Trees | 9.38% | 3 |
| Lighting | 25.00% | 8 |
| Wayfinding/Signage | 0.00% | 0 |
| TOTAL | | 32 |

Q10 How would you describe Mascoutah to someone visiting?

Answered: 26 Skipped: 9

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | Small town, safe | 8/20/2020 12:00 PM |
| 2 | Great community some times, catty other times. Can be very non supportive of newcomers. | 8/20/2020 7:37 AM |
| 3 | great small town with a great park (Scheve) | 8/19/2020 9:48 PM |
| 4 | Small town. Not much to do. | 8/19/2020 5:27 PM |
| 5 | small town America | 8/19/2020 1:26 PM |
| 6 | Small town | 8/19/2020 1:22 PM |
| 7 | Friendly Residents | 8/19/2020 12:40 PM |
| 8 | Quiet bedroom community | 8/19/2020 9:34 AM |
| 9 | Very quiet town with friendly people, but needs some updating. | 8/19/2020 8:19 AM |
| 10 | Small town feel, good schools | 8/18/2020 9:48 PM |
| 11 | Home | 8/18/2020 6:46 PM |
| 12 | A small quiet bedroom community with many military and retired military families | 8/18/2020 4:54 PM |
| 13 | wonderful historic farming community with many long-time families | 8/18/2020 2:44 PM |
| 14 | Backwards | 8/17/2020 6:44 PM |
| 15 | Small town vibe, with pride. | 8/16/2020 11:51 PM |
| 16 | Rundown | 8/16/2020 9:06 PM |
| 17 | Family town, great place to raise kids, awesome schools. Civic pride. | 8/16/2020 7:06 PM |
| 18 | do not visit. Nothing but empty buildings and broken roads, sidewalks | 8/14/2020 12:55 PM |
| 19 | Small town and quiet and everyone helps everyone | 8/14/2020 12:32 PM |
| 20 | Old, small, needs a better up to date downtown area. No where to sit in the shade near restaurants | 8/14/2020 12:24 PM |
| 21 | Small town | 8/13/2020 11:42 PM |
| 22 | Small rural town | 8/13/2020 10:16 PM |
| 23 | Small Town quaintness without traffic and noise. | 8/13/2020 8:09 PM |
| 24 | Nice town, quiet, good place to raise a family, but not much recreation, shopping, or dining options. But close enough to places that DO have those | 8/13/2020 6:07 PM |
| 25 | Slow paced, growing, good schools, nice park, people don't wear masks at ACE hardware, the people are are more conservative than neighboring communities, rural, reasonably safe | 8/13/2020 5:49 PM |
| 26 | Small town, crooked politics. | 8/13/2020 5:16 PM |

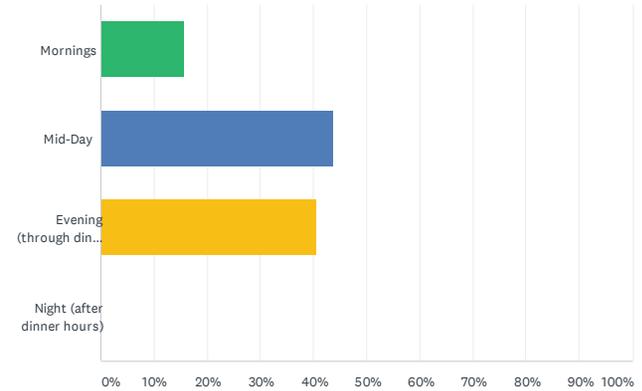
Q11 Where is the center of town?

Answered: 28 Skipped: 7

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | City Hall | 8/23/2020 9:11 PM |
| 2 | City hall | 8/20/2020 12:00 PM |
| 3 | cityhall area | 8/20/2020 7:37 AM |
| 4 | 6th Street and Harnett | 8/19/2020 9:48 PM |
| 5 | Near City Hall | 8/19/2020 5:27 PM |
| 6 | Hwy 4 and 177 | 8/19/2020 1:26 PM |
| 7 | City Hall | 8/19/2020 1:22 PM |
| 8 | Intersection of Main Street and Railway Avenue | 8/19/2020 12:40 PM |
| 9 | City Hall | 8/19/2020 9:34 AM |
| 10 | Main Street, over by Jefferson. | 8/19/2020 8:19 AM |
| 11 | Railway and Main | 8/18/2020 9:48 PM |
| 12 | City hall | 8/18/2020 6:46 PM |
| 13 | City Hall area | 8/18/2020 4:54 PM |
| 14 | I would estimate it to be City hall | 8/18/2020 2:44 PM |
| 15 | Post office to visitor's center | 8/17/2020 6:44 PM |
| 16 | Scheve | 8/16/2020 11:51 PM |
| 17 | Welcome center | 8/16/2020 9:06 PM |
| 18 | City Hall | 8/16/2020 7:06 PM |
| 19 | no where. maybe the mill that is half taken down | 8/14/2020 12:55 PM |
| 20 | City hall | 8/14/2020 12:32 PM |
| 21 | On Main street | 8/14/2020 12:24 PM |
| 22 | Idk | 8/13/2020 11:42 PM |
| 23 | around city hall | 8/13/2020 10:16 PM |
| 24 | City Hall/Mill/Post Office area | 8/13/2020 8:09 PM |
| 25 | city hall area | 8/13/2020 6:11 PM |
| 26 | ? | 8/13/2020 6:07 PM |
| 27 | 6th and Harnett | 8/13/2020 5:49 PM |
| 28 | City hall | 8/13/2020 5:16 PM |

Q12 What time of the day are you most likely to visit Main Street?

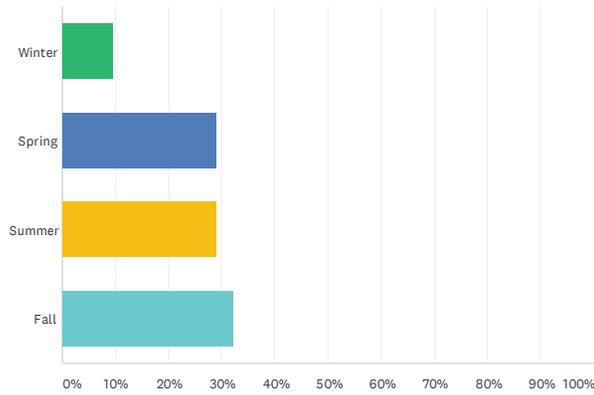
Answered: 32 Skipped: 3



| ANSWER CHOICES | RESPONSES | |
|--------------------------------|-----------|-----------|
| Mornings | 15.63% | 5 |
| Mid-Day | 43.75% | 14 |
| Evening (through dinner hours) | 40.63% | 13 |
| Night (after dinner hours) | 0.00% | 0 |
| TOTAL | | 32 |

Q13 What time of year are you most likely to visit Main Street?

Answered: 31 Skipped: 4



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----------|
| Winter | 9.68% | 3 |
| Spring | 29.03% | 9 |
| Summer | 29.03% | 9 |
| Fall | 32.26% | 10 |
| TOTAL | | 31 |

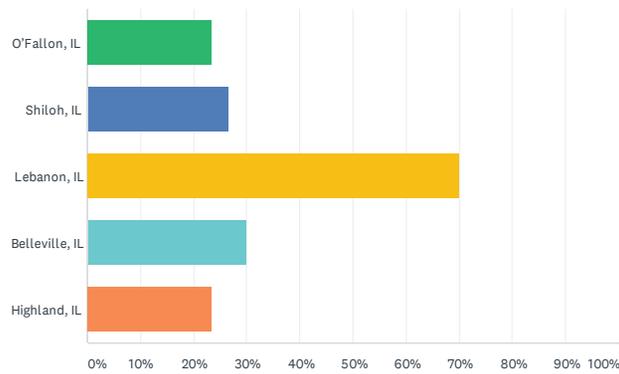
Q14 Do you have any additional comments on the environment in Mascoutah?

Answered: 7 Skipped: 28

| # | RESPONSES | DATE |
|---|---|--------------------|
| 1 | Love the parks in Mascoutah | 8/19/2020 8:19 AM |
| 2 | I think a beautification project is definitely in order for mainstreet. Think more like Scheve park and less like East St. Louis, from an environmental standpoint. North of Main Street's environment is prettier. | 8/16/2020 11:51 PM |
| 3 | Need more businesses like Highland has but feel like downtown Belleville. | 8/16/2020 9:06 PM |
| 4 | Na | 8/16/2020 7:06 PM |
| 5 | Bo | 8/13/2020 11:42 PM |
| 6 | Trees, flowers, maintaining original facades if historic buildings. Placards on historic buildings detailing history. | 8/13/2020 8:09 PM |
| 7 | You must lower utilities in this town. | 8/13/2020 5:16 PM |

Q1 Which of the following do you think are Mascoutah's peers? (Choose all that apply)

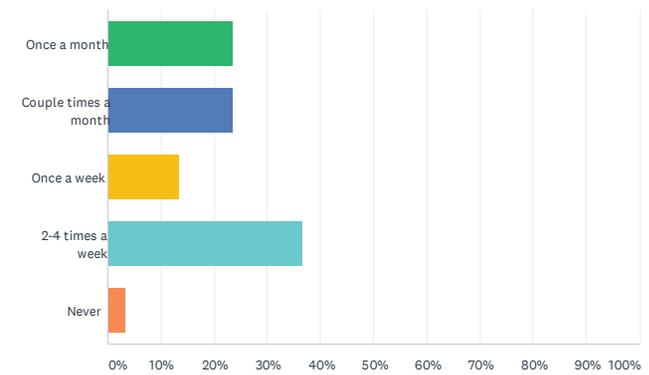
Answered: 30 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|-----------------------|-----------|----|
| O'Fallon, IL | 23.33% | 7 |
| Shiloh, IL | 26.67% | 8 |
| Lebanon, IL | 70.00% | 21 |
| Belleville, IL | 30.00% | 9 |
| Highland, IL | 23.33% | 7 |
| Total Respondents: 30 | | |

Q2 How often do you visit downtown Mascoutah?

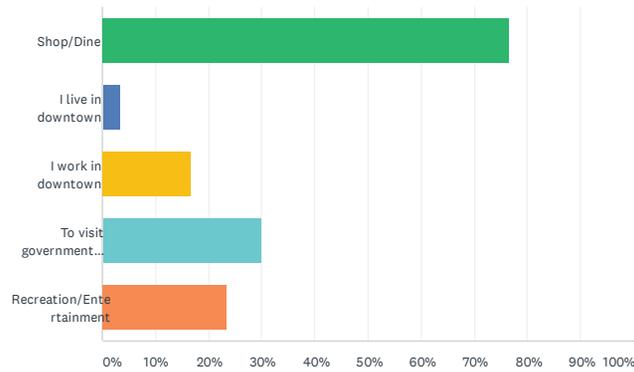
Answered: 30 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------------|-----------|----|
| Once a month | 23.33% | 7 |
| Couple times a month | 23.33% | 7 |
| Once a week | 13.33% | 4 |
| 2-4 times a week | 36.67% | 11 |
| Never | 3.33% | 1 |
| TOTAL | | 30 |

Q3 Why do you visit downtown Mascoutah? (select all that apply)

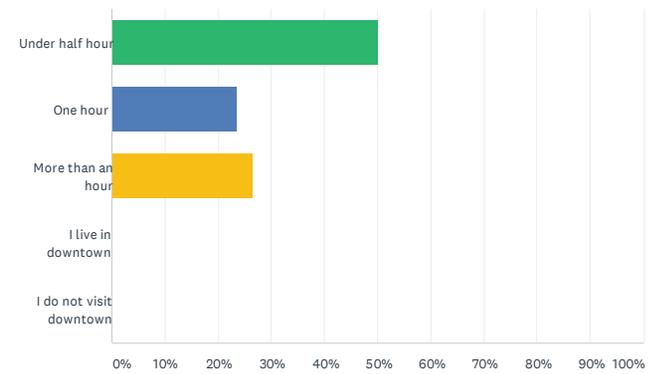
Answered: 30 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|-----------------------------|-----------|----|
| Shop/Dine | 76.67% | 23 |
| I live in downtown | 3.33% | 1 |
| I work in downtown | 16.67% | 5 |
| To visit government offices | 30.00% | 9 |
| Recreation/Entertainment | 23.33% | 7 |
| Total Respondents: 30 | | |

Q4 During your average visit, how much time do you spend in downtown Mascoutah?

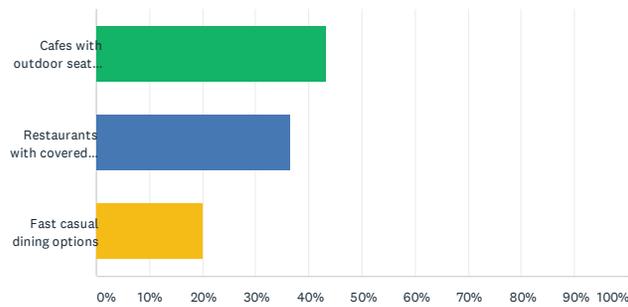
Answered: 30 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|-------------------------|-----------|----|
| Under half hour | 50.00% | 15 |
| One hour | 23.33% | 7 |
| More than an hour | 26.67% | 8 |
| I live in downtown | 0.00% | 0 |
| I do not visit downtown | 0.00% | 0 |
| TOTAL | | 30 |

Q5 Which of the following dining experiences would you prefer to see on Main Street?

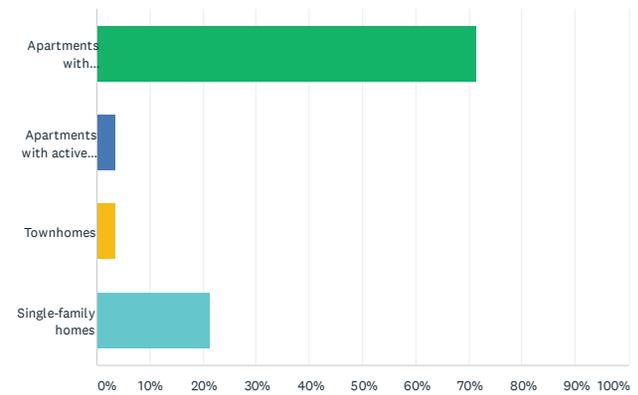
Answered: 30 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|----|
| Cafe Outdoor Seating Cafes with outdoor seating | 43.33% | 13 |
| Restaurants with covered patio seating | 36.67% | 11 |
| Fast casual dining options | 20.00% | 6 |
| TOTAL | | 30 |

Q6 Which of the following housing options would you prefer to see on Main Street?

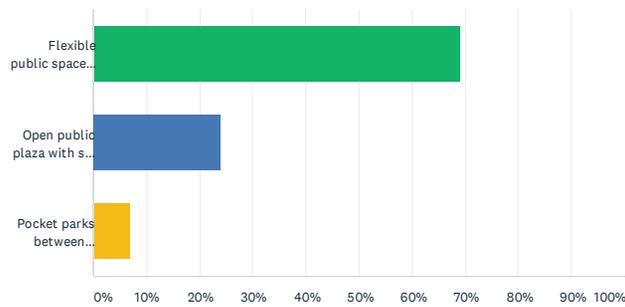
Answered: 28 Skipped: 2



| ANSWER CHOICES | RESPONSES | |
|---|-----------|----|
| Apartments with active street entrances | 71.43% | 20 |
| Townhomes | 3.57% | 1 |
| Single-family homes | 21.43% | 6 |
| Apartments with active street entrances | 3.57% | 1 |
| TOTAL | | 28 |

Q7 Which of the following public space experiences would you prefer to see on Main Street?

Answered: 29 Skipped: 1



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| Flexible public space that can be used for weekend activities such as a farmer's market. | 68.97% 20 |
| Open public plaza with some seating, landscaping and sculptures. | 24.14% 7 |
| Pocket parks between buildings. | 6.90% 2 |
| TOTAL | 29 |

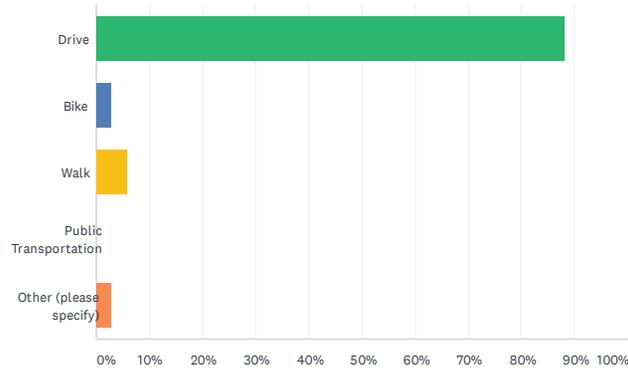
Q8 Do you have any additional comments on the market in Mascoutah?

Answered: 5 Skipped: 25

| # | RESPONSES | DATE |
|---|--|--------------------|
| 1 | Fast food | 8/18/2020 5:16 PM |
| 2 | We should aim to be peers with Shiloh, in my opinion. | 8/16/2020 11:45 PM |
| 3 | No | 8/13/2020 11:45 PM |
| 4 | Keep retail and restaurants small businesses with local ties and not chains. | 8/13/2020 8:02 PM |
| 5 | Apartments designed to attract younger adults would be awesome for creating a livelier vibe. Art Loft incentives | 8/13/2020 5:40 PM |

Q1 What is the primary way you get around Mascoutah?

Answered: 34 Skipped: 0

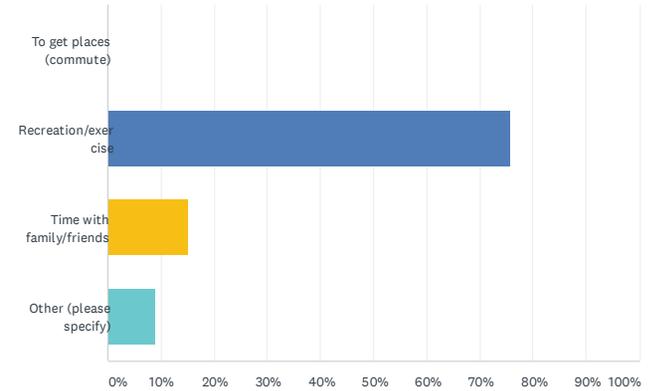


| ANSWER CHOICES | RESPONSES | |
|------------------------|-----------|-----------|
| Drive | 88.24% | 30 |
| Bike | 2.94% | 1 |
| Walk | 5.88% | 2 |
| Public Transportation | 0.00% | 0 |
| Other (please specify) | 2.94% | 1 |
| TOTAL | | 34 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|---------------------------|--------------------|
| 1 | Bike and Driving equally. | 8/16/2020 11:54 PM |

Q2 Why do you walk or ride your bike?

Answered: 33 Skipped: 1

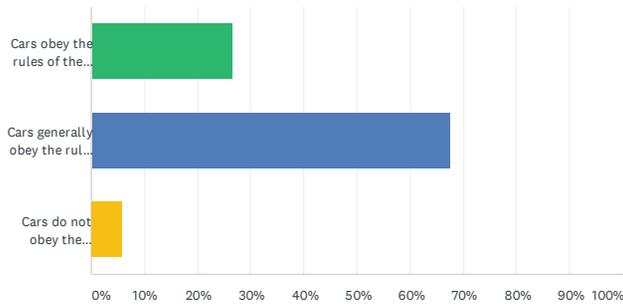


| ANSWER CHOICES | RESPONSES | |
|--------------------------|-----------|-----------|
| To get places (commute) | 0.00% | 0 |
| Recreation/exercise | 75.76% | 25 |
| Time with family/friends | 15.15% | 5 |
| Other (please specify) | 9.09% | 3 |
| TOTAL | | 33 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|--|-------------------|
| 1 | Walking the dog | 8/19/2020 1:28 PM |
| 2 | It is a combination or "to get places" "recreation/exercise" and also to enjoy a few alcoholic beverages and not need to drive home. | 8/19/2020 8:23 AM |
| 3 | I don't...this is a poorly designed survey | 8/13/2020 6:08 PM |

Q3 How do you feel about motor vehicle behavior on Main Street?

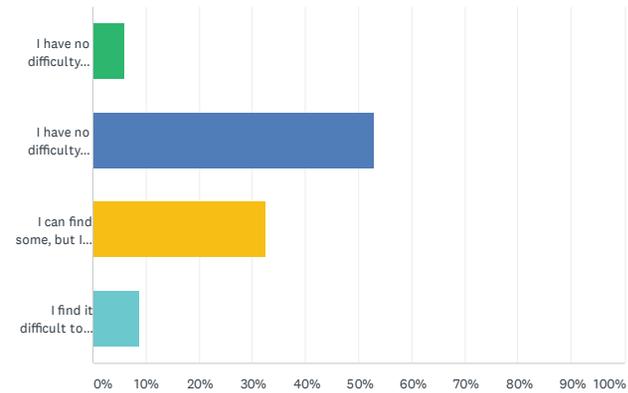
Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| Cars obey the rules of the road. | 26.47% 9 |
| Cars generally obey the rules, but some motorists speed or drive erratically. | 67.65% 23 |
| Cars do not obey the traffic control rules. | 5.88% 2 |
| TOTAL | 34 |

Q4 How do you feel about the pedestrian access around Mascoutah?

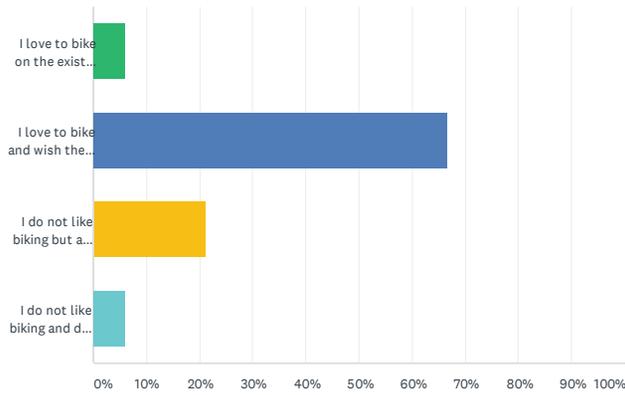
Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| I have no difficulty finding safe places to walk and like the existing spaces (sidewalks, trails, etc.). | 5.88% 2 |
| I have no difficulty finding safe places to walk, but the existing spaces (sidewalks, trails, etc.) need to be repaired. | 52.94% 18 |
| I can find some, but I wish there were more pedestrian spaces (sidewalks, trails, etc.). | 32.35% 11 |
| I find it difficult to find safe places to walk. | 8.82% 3 |
| TOTAL | 34 |

Q5 How do you feel about the bicycle access around Mascoutah?

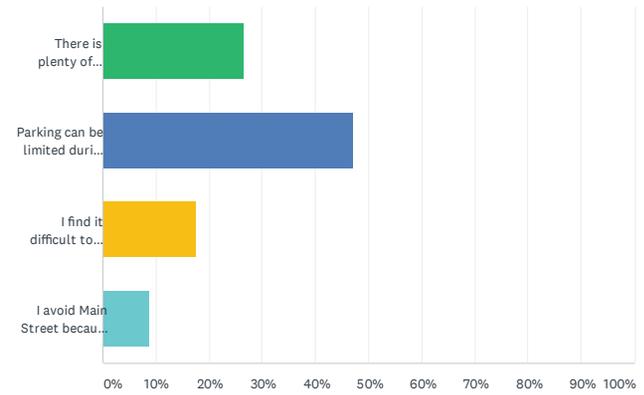
Answered: 33 Skipped: 1



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| I love to bike on the existing bike paths and do not need more. | 6.06% 2 |
| I love to bike and wish there were more bike paths throughout the town. | 66.67% 22 |
| I do not like biking but am open to seeing more bike paths in town. | 21.21% 7 |
| I do not like biking and do not like sharing the roadway with bicyclists. | 6.06% 2 |
| TOTAL | 33 |

Q6 How do you feel about parking on Main Street?

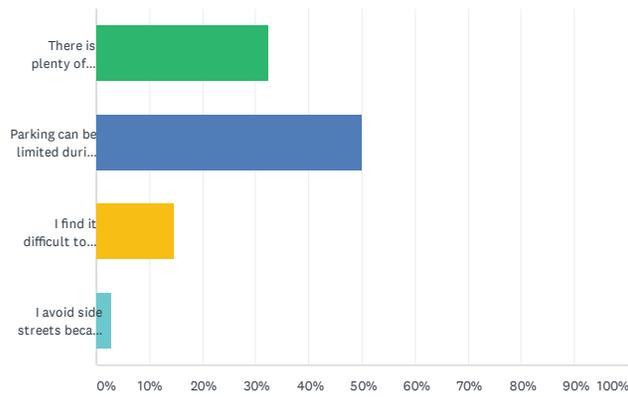
Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| There is plenty of parking. | 26.47% 9 |
| Parking can be limited during peak events. | 47.06% 16 |
| I find it difficult to park. | 17.65% 6 |
| I avoid Main Street because of lack of parking. | 8.82% 3 |
| TOTAL | 34 |

Q7 How do you feel about parking on side streets (Church & State)?

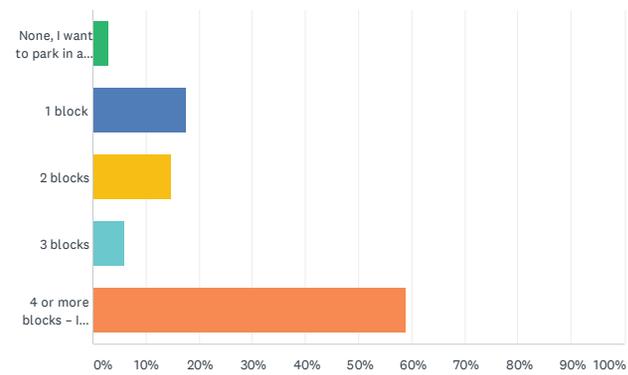
Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| There is plenty of parking. | 32.35% | 11 |
| Parking can be limited during peak events. | 50.00% | 17 |
| I find it difficult to park. | 14.71% | 5 |
| I avoid side streets because of lack of parking. | 2.94% | 1 |
| TOTAL | | 34 |

Q8 How many blocks do you feel safe/comfortable walking to get to your final destination on Main Street?

Answered: 34 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| None, I want to park in a lot or at the door. | 2.94% | 1 |
| 1 block | 17.65% | 6 |
| 2 blocks | 14.71% | 5 |
| 3 blocks | 5.88% | 2 |
| 4 or more blocks - I don't mind walking at all! | 58.82% | 20 |
| TOTAL | | 34 |

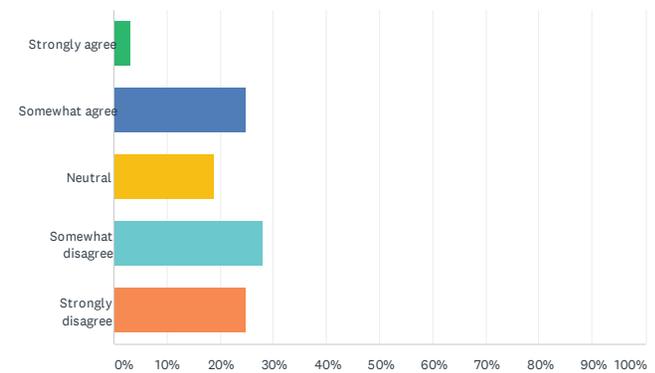
Q9 Are there any other transportation issues that we have not addressed that you would like to bring to our attention?

Answered: 16 Skipped: 18

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | I would like to be able to drive my golf cart on Main Street. | 8/23/2020 9:41 PM |
| 2 | Engine breaking of semi-trucks; it ruins the atmosphere. Need signs prohibiting engine breaking within town. | 8/19/2020 9:51 PM |
| 3 | More bike lanes would be great. I also think the speed limit is too low on the outskirts of town. On Main St I understand the 35 to 25 MPH, but on some of the larger streets that feed into the town, the speed limit hits 35 and there is no reason for it to be that slow, that soon. 45 MPH is what I'd normally expect. | 8/19/2020 8:23 AM |
| 4 | Need more handicapped spaces on main st. Police need to enforce the yellow no parking lines. | 8/18/2020 6:44 PM |
| 5 | Parking on State or Church would be fine. However, many spaces need to be fixed to clearly mark where we are able to park. Some people park in grass and on people's lawns. This would also need to be addressed before putting more parking in these areas. | 8/18/2020 6:41 PM |
| 6 | Pulling out onto main st from the side streets closer to rt 4 is dangerous. The buildings block the view of the road and to cant see if cars are coming | 8/18/2020 6:31 PM |
| 7 | Water flooding every time it rains | 8/18/2020 6:25 PM |
| 8 | In certain ares there's not much you can do to improve parking or sidewalk width due to the buildings being so close to the road. Market street to Jefferson for example... | 8/18/2020 4:59 PM |
| 9 | too many potholes | 8/18/2020 2:47 PM |
| 10 | On bicycle access - we don't need trails. We need roads that are gravel- and pothole-free. We need better driver responsibility and respect. This whole town should be bike safe and bike friendly. | 8/17/2020 6:47 PM |
| 11 | Biking is a joy north of Main Street. I avoid Main street on my bike usually because the sidewalk is absolutely horrible down much of it. | 8/16/2020 11:54 PM |
| 12 | Shouldn't be any parking on Main St. Too narrow for it. | 8/16/2020 9:09 PM |
| 13 | Citizens that live in the subdivisions north of Hog River cannot safely walk or bike to Main Street. Route 4 has no paths or sidewalk to do this in a safe manner. | 8/16/2020 7:25 PM |
| 14 | No | 8/13/2020 11:44 PM |
| 15 | Townsend square does not have sidewalk access to connect the neighborhood to the schools without crossing a 55 mph road. Prairie view needs a bridge from their community lot to the bike path. | 8/13/2020 5:54 PM |
| 16 | A fair amount of streets in town have no sidewalks. | 8/13/2020 5:18 PM |

Q1 How much do you agree or disagree with the following statement?: Additional residential development makes sense in the Main Street study area.

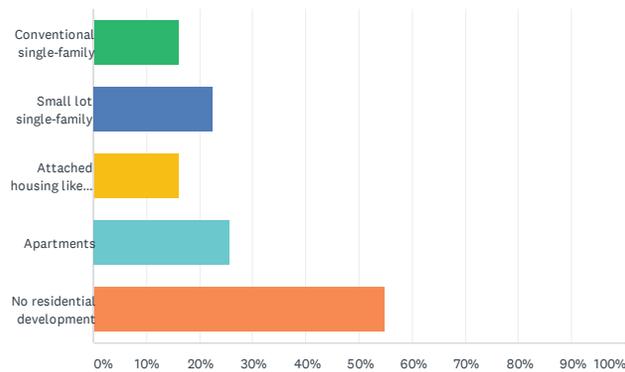
Answered: 32 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|-------------------|-----------|----|
| Strongly agree | 3.13% | 1 |
| Somewhat agree | 25.00% | 8 |
| Neutral | 18.75% | 6 |
| Somewhat disagree | 28.13% | 9 |
| Strongly disagree | 25.00% | 8 |
| TOTAL | | 32 |

Q2 What types of residential development would be appropriate in this area? (Check all that apply)

Answered: 31 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|---|-----------|----|
| Conventional single-family | 16.13% | 5 |
| Small lot single-family | 22.58% | 7 |
| Attached housing like duplexes or townhomes | 16.13% | 5 |
| Apartments | 25.81% | 8 |
| No residential development | 54.84% | 17 |
| Total Respondents: 31 | | |

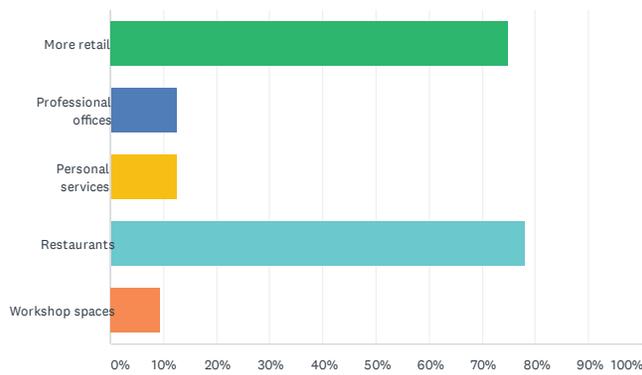
Q3 What sites in the study area do you believe offer the best possibilities for new development?

Answered: 16 Skipped: 16

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | The Mill Former Dodge Dealership | 8/23/2020 8:44 PM |
| 2 | I don't know | 8/23/2020 8:22 AM |
| 3 | Nkt sure | 8/20/2020 7:30 AM |
| 4 | Near el sombrero. Near churches | 8/19/2020 9:57 PM |
| 5 | Between jefferson and Lebanon | 8/19/2020 9:36 PM |
| 6 | East Main Street | 8/19/2020 12:35 PM |
| 7 | Market District, it should be expanded. There is already too much housing on Main St. More entertainment (breweries, music, restaurants, shops) are needed. Add more businesses towards 6th street. Current retail is too centered in one small area of Main St. | 8/19/2020 8:12 AM |
| 8 | Renovate empty buildings to turn into apartments | 8/18/2020 7:00 PM |
| 9 | I'm not sure I understand what you are getting at when you mentioned residential development in questions 1 & 2. Evidently you have something in mind. I would guess you are considering multi-family housing units near downtown. I am not in favor of residential development downtown as I do not see a correlation between that and main street progress. | 8/18/2020 2:26 PM |
| 10 | Old Postal Mill site; old hardware and laundromat site; old gas station/car dealer. | 8/17/2020 6:36 PM |
| 11 | Central; the area across from City Hall and the surrounding area near the Historic Museum. | 8/16/2020 11:42 PM |
| 12 | By city hall the old mill. I would like to see something done with the building with the mural. | 8/14/2020 12:21 PM |
| 13 | We have enough residential | 8/13/2020 10:12 PM |
| 14 | The area by vacant car dealership. | 8/13/2020 6:20 PM |
| 15 | Uncertain | 8/13/2020 5:37 PM |
| 16 | Empty lots and run down houses. | 8/13/2020 5:12 PM |

Q4 Of the following, which type of new development do you feel is most needed in this study area? (Choose two)

Answered: 32 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|-----------------------|-----------|----|
| More retail | 75.00% | 24 |
| Professional offices | 12.50% | 4 |
| Personal services | 12.50% | 4 |
| Restaurants | 78.13% | 25 |
| Workshop spaces | 9.38% | 3 |
| Total Respondents: 32 | | |

Q5 What is your favorite place in the Main Street study area?

Answered: 25 Skipped: 7

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | The Mill | 8/23/2020 8:44 PM |
| 2 | coffe world | 8/20/2020 7:30 AM |
| 3 | El sombrero | 8/19/2020 9:57 PM |
| 4 | Coffee World | 8/19/2020 9:36 PM |
| 5 | Coffee shop | 8/19/2020 5:22 PM |
| 6 | Ace | 8/19/2020 1:23 PM |
| 7 | Visitor's Center | 8/19/2020 12:35 PM |
| 8 | Ace Hardware | 8/19/2020 9:29 AM |
| 9 | Juicy Peanut Bar | 8/19/2020 8:12 AM |
| 10 | el sombrero | 8/19/2020 1:16 AM |
| 11 | /Railway and main street | 8/18/2020 9:40 PM |
| 12 | Daubers pharmacy | 8/18/2020 7:00 PM |
| 13 | Jefferson and Main St. area and anywhere along Jefferson offer the best chance for any commercial and restaurant development due to the traffic volume | 8/18/2020 4:47 PM |
| 14 | Ace Hardware. Great renovation and look the curbside look. We are historically a farm community, and are losing our history and identity. I also love the Indian Mural | 8/18/2020 2:26 PM |
| 15 | Hardware store. | 8/17/2020 6:36 PM |
| 16 | Across from City Hall | 8/16/2020 11:42 PM |
| 17 | Ace | 8/16/2020 8:58 PM |
| 18 | The town square with City Hall and the mural | 8/14/2020 12:21 PM |
| 19 | The coffee shop | 8/14/2020 10:15 AM |
| 20 | Bee Hollow | 8/13/2020 10:12 PM |
| 21 | Mill area | 8/13/2020 7:59 PM |
| 22 | City hall area. | 8/13/2020 6:20 PM |
| 23 | St Louis Coffee World, but I infrequently go there | 8/13/2020 6:04 PM |
| 24 | Coffee shop | 8/13/2020 5:37 PM |
| 25 | City hall. | 8/13/2020 5:12 PM |

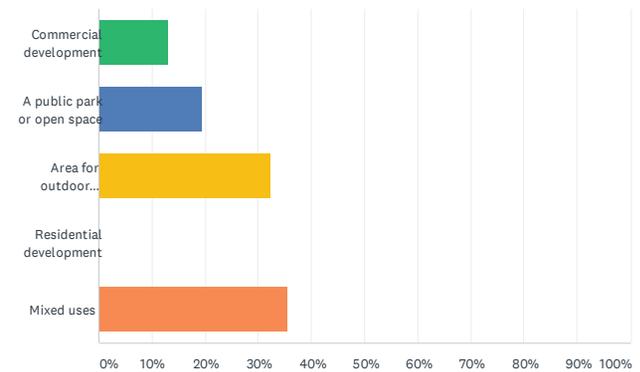
Q6 If you were to choose one new business that you would like to see open up on Main Street, what would it be?

Answered: 26 Skipped: 6

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Craft Brewery | 8/23/2020 8:44 PM |
| 2 | eatery/soda shop | 8/23/2020 8:22 AM |
| 3 | crafty place | 8/20/2020 7:30 AM |
| 4 | Craft brewery with dining | 8/19/2020 9:57 PM |
| 5 | An Italian restaurant with patio seating | 8/19/2020 9:36 PM |
| 6 | Nice diner with outdoor seating, live music or outdoor activities (similar to 4204 in Belleville) | 8/19/2020 5:22 PM |
| 7 | Restaurant | 8/19/2020 12:35 PM |
| 8 | Restaurant | 8/19/2020 9:29 AM |
| 9 | I would LOVE to see a nice restaurant, with an outdoor patio, some craft beers, good food (with vegetarian options), and maybe some live music. Maybe they even do a weekend Brunch! The few restaurants we have, do not have variety. They are mostly pub fare, bbq, pizza, wings.... Look at St Louis and the cute areas they have developed... The Hill, Souldard, Morgan Ford, The Grove, Del Mar Loop. | 8/19/2020 8:12 AM |
| 10 | taco bell | 8/19/2020 1:16 AM |
| 11 | farmers market | 8/18/2020 9:40 PM |
| 12 | Arcade/pool/billiards geared towards teens | 8/18/2020 7:00 PM |
| 13 | Certainly need a restaurant near the city center, something like Reifschneider's in Freeburg | 8/18/2020 4:47 PM |
| 14 | a nice sit-down, non-fast food, restaurant | 8/18/2020 2:26 PM |
| 15 | Quality sit down restaurant | 8/17/2020 6:36 PM |
| 16 | Taco Bell | 8/16/2020 11:42 PM |
| 17 | Walton's Ice Cream | 8/16/2020 8:58 PM |
| 18 | Family restroom Family Restaurant | 8/14/2020 12:21 PM |
| 19 | Diner/restaurant | 8/14/2020 10:15 AM |
| 20 | More dining options | 8/13/2020 10:12 PM |
| 21 | Bakery | 8/13/2020 7:59 PM |
| 22 | Large antique mall | 8/13/2020 6:29 PM |
| 23 | A restaurant | 8/13/2020 6:20 PM |
| 24 | Any business that ISNT a bank or hair salon | 8/13/2020 6:04 PM |
| 25 | Italian Restaurant ! | 8/13/2020 5:37 PM |
| 26 | Restaurant | 8/13/2020 5:12 PM |

Q7 How do you think the mill site should be reused once the mill is disassembled?

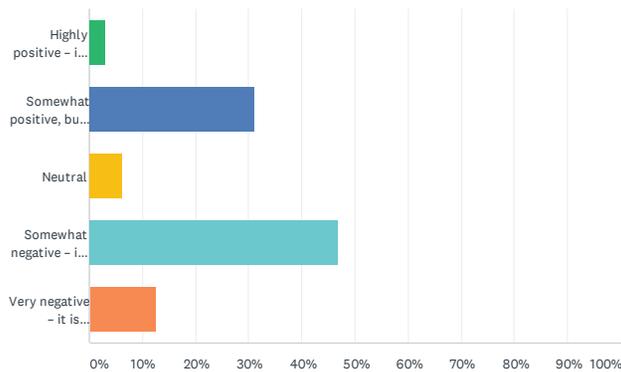
Answered: 31 Skipped: 1



| ANSWER CHOICES | RESPONSES |
|------------------------------|-----------|
| Commercial development | 12.90% 4 |
| A public park or open space | 19.35% 6 |
| Area for outdoor performance | 32.26% 10 |
| Residential development | 0.00% 0 |
| Mixed uses | 35.48% 11 |
| TOTAL | 31 |

Q8 What is your feeling about Main Street as an environment?

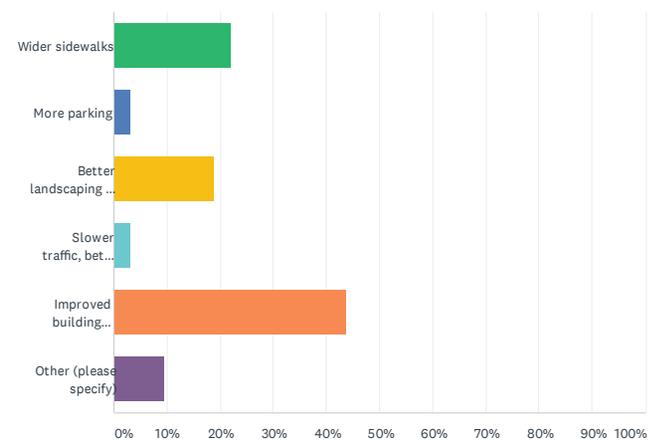
Answered: 32 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| Highly positive – it is a very pleasant street. | 3.13% 1 |
| Somewhat positive, but it needs some work. | 31.25% 10 |
| Neutral | 6.25% 2 |
| Somewhat negative – it needs considerable improvement. | 46.88% 15 |
| Very negative – it is unpleasant for pedestrians and motorists alike. | 12.50% 4 |
| TOTAL | 32 |

Q9 Of the following, what action would most effectively improve the Main Street environment?

Answered: 32 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| Wider sidewalks | 21.88% 7 |
| More parking | 3.13% 1 |
| Better landscaping and lighting | 18.75% 6 |
| Slower traffic, better traffic management | 3.13% 1 |
| Improved building facades | 43.75% 14 |
| Other (please specify) | 9.38% 3 |
| TOTAL | 32 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|--|--------------------|
| 1 | Total replacement of all roads/sidewalks | 8/20/2020 4:08 PM |
| 2 | Better curbs and sidewalks | 8/17/2020 6:36 PM |
| 3 | better streets and side walks. eliminate parking on main and widen the sidewalks. increase parking on the side streets | 8/14/2020 12:51 PM |

Q10 Do you have any additional comments on urban design or land use in Mascoutah?

Answered: 11 Skipped: 21

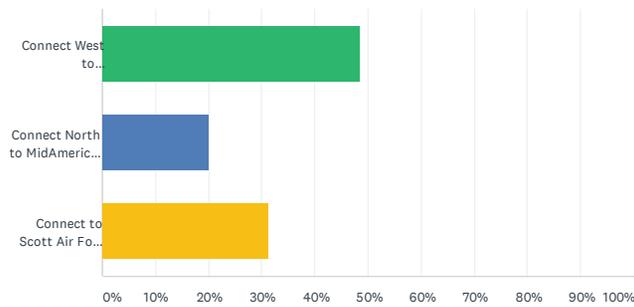
| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Need more park space, need to improve sidewalks along more roads. Especially main streets and route 4 | 8/19/2020 9:57 PM |
| 2 | West segment needs more restaurants/breweries. For the Mill, it would be great to have an outdoor area for music venues, but can also have some mixed use (farmers markets, food truck nights in the summer, and maybe a few standing shops/restaurants) | 8/19/2020 8:12 AM |
| 3 | Many businesses come and go, not sure why... maybe high rent/upkeep? Maybe can't compete with larger communities. I think we would benefit most from family friendly/teen activities. Maybe a mini golf course at the old mill... | 8/18/2020 7:00 PM |
| 4 | Isn't the old mill property privately owned? So how can you try and dictate what happens with it? | 8/18/2020 4:47 PM |
| 5 | We are a historic community with a rich past. I live on Railway and love my street. I would like to see Main street reflect more of our past and small-town charm. the character and charm of a small town is something to be proud of and to promote. Many want progress, but at what expense. I say improve upon what we have, and promote who we are | 8/18/2020 2:26 PM |
| 6 | A popular restaurant and an office building would be an interesting idea. | 8/16/2020 11:42 PM |
| 7 | Make it feel like downtown Belleville. | 8/16/2020 8:58 PM |
| 8 | I really would like to see the town square at City Hall be a more historical setting | 8/14/2020 12:21 PM |
| 9 | Planters and trees throughout | 8/13/2020 7:59 PM |
| 10 | Many buildings/businesses downtown have outdated, shabby, and/or dirty facades | 8/13/2020 6:04 PM |
| 11 | You must lower utilities for businesses to make this work. | 8/13/2020 5:12 PM |

APPENDIX E-2

Survey Results - Round 2

Q1 Multiple shared-use path options have been presented. If only one can be implemented, which would you choose?

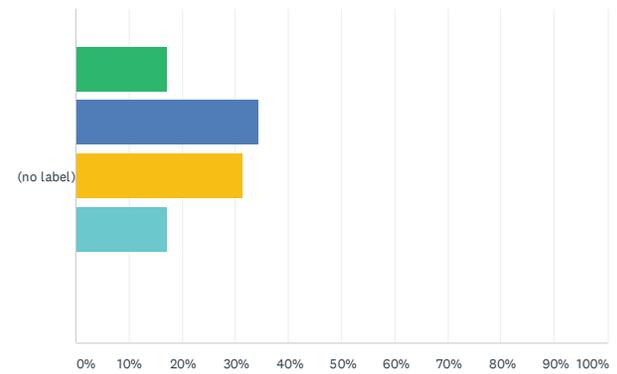
Answered: 35 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|-------------------------------------|-----------|-----------|
| Connect West to MetroBikeWalk path | 48.57% | 17 |
| Connect North to MidAmerica Airport | 20.00% | 7 |
| Connect to Scott Air Force Base | 31.43% | 11 |
| TOTAL | | 35 |

Q2 How likely are you to utilize the regional shared-use path system if it is primarily on-street (street shoulder)?

Answered: 35 Skipped: 0

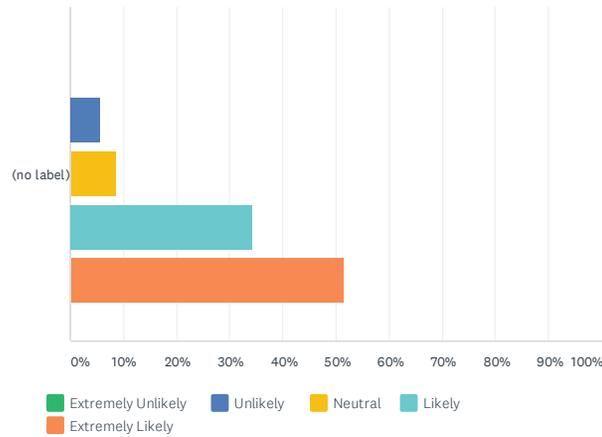


Extremely Unlikely Unlikely Neutral Likely Extremely Likely

| | EXTREMELY UNLIKELY | UNLIKELY | NEUTRAL | LIKELY | EXTREMELY LIKELY | TOTAL | WEIGHTED AVERAGE |
|------------|--------------------|--------------|--------------|-------------|------------------|-------|------------------|
| (no label) | 17.14% 6 | 34.29% 12 | 31.43% 11 | 17.14% 6 | 0.00% 0 | 35 | 2.49 |

Q3 How likely are you to utilize the regional shared-use path system if it is primarily off-street (greenway)?

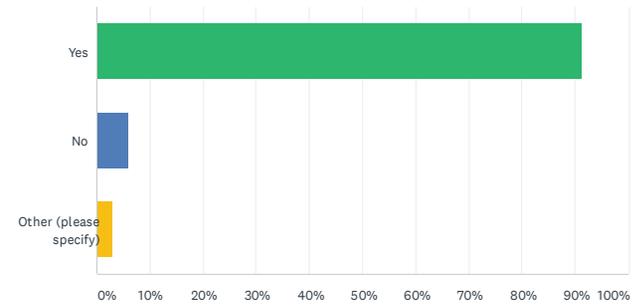
Answered: 35 Skipped: 0



| | EXTREMELY UNLIKELY | UNLIKELY | NEUTRAL | LIKELY | EXTREMELY LIKELY | TOTAL | WEIGHTED AVERAGE |
|------------|--------------------|----------|---------|--------|------------------|-------|------------------|
| (no label) | 0.00% | 5.71% | 8.57% | 34.29% | 51.43% | 35 | 4.31 |
| | 0 | 2 | 3 | 12 | 18 | | |

Q4 Would you use a bike path to connect to the regional trail network?

Answered: 34 Skipped: 1

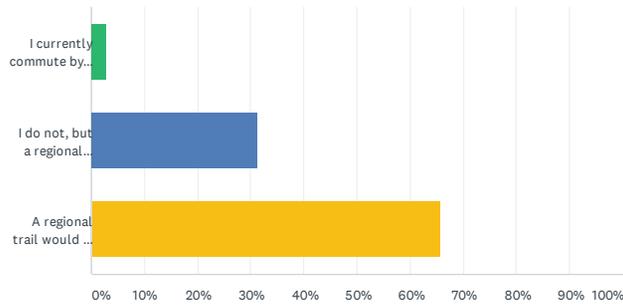


| ANSWER CHOICES | RESPONSES | |
|------------------------|-----------|-----------|
| Yes | 91.18% | 31 |
| No | 5.88% | 2 |
| Other (please specify) | 2.94% | 1 |
| TOTAL | | 34 |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|------------------------|-------------------|
| 1 | Rarely, if ever | 8/26/2020 3:33 PM |

Q5 Do you commute by bike or walking to work? If not, would a regional trail system allow you to? Choose the response that best describes you.

Answered: 35 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| I currently commute by walking or biking. | 2.86% | 1 |
| I do not, but a regional trail would allow be to do so. | 31.43% | 11 |
| A regional trail would not allow me to bike or walk to work. | 65.71% | 23 |
| TOTAL | | 35 |

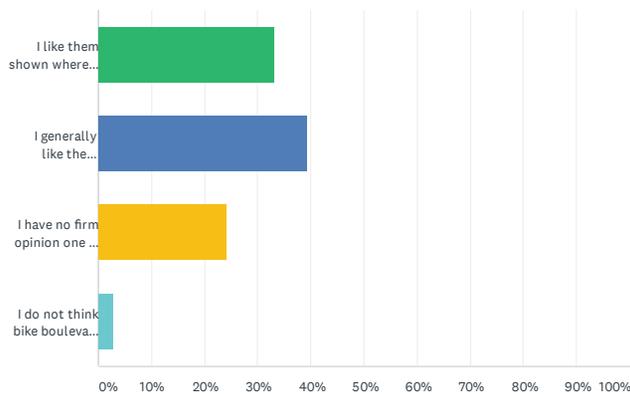
Q6 Besides financial challenges, what potential pitfalls do you foresee in developing a regional multi-use trail system in Mascoutah?

Answered: 20 Skipped: 15

| # | RESPONSES | DATE |
|----|--|--------------------|
| 1 | Distance of the trail and cost. I would rather Mascoutah focus on fixing flooding problems before moving trying to make Mascoutah a tourist destination. | 9/2/2020 12:40 AM |
| 2 | Security | 9/1/2020 1:36 PM |
| 3 | I do not believe that city residents like these paths in near their homes. | 8/31/2020 9:51 PM |
| 4 | Some residents will be against a regional connection leading to loss of the small town charm. | 8/31/2020 9:28 AM |
| 5 | It seems that it would be difficult to find the best route to connect, ie along Rt. 161, there is the creek bottoms which floods often. I have seen floating paths used in other locales and wonder if that Would be an option. Going out of town along Rt. 177 has similar problems. But I definitely feel that if it could be done, it definitely would get much use and would open up so many opportunities to access areas around Belleville and St. Louis by connecting with the Metro Bike Trail. I think it would also appeal to potential homebuyers in Mascoutah. I would love to see it happen!! | 8/30/2020 8:29 PM |
| 6 | drainage | 8/28/2020 10:10 AM |
| 7 | challenges with state road transportation and challenges with crossing the Silver Creek wetlands | 8/27/2020 2:48 PM |
| 8 | Who maintains? | 8/26/2020 10:13 PM |
| 9 | Land Acquisition | 8/26/2020 9:36 PM |
| 10 | None | 8/26/2020 9:35 PM |
| 11 | Silver creek and connecting with the st clair county trails | 8/26/2020 8:49 PM |
| 12 | I don't think we need to connect regionally | 8/26/2020 7:52 PM |
| 13 | Location | 8/26/2020 5:38 PM |
| 14 | Mascoutah is very resistant to changing their "small town" feel so people need to be sold on any plans to improve their way of life. | 8/26/2020 5:01 PM |
| 15 | Safety and upkeep | 8/26/2020 4:00 PM |
| 16 | None | 8/26/2020 3:59 PM |
| 17 | Due to the position of Mascoutah, bikers would have to make a commitment to walk or bike a decent distance before reaching any area for any kind of purpose. | 8/26/2020 3:53 PM |
| 18 | Making it easier for less-desirable citizens to reach Mascoutah would not be ideal. | 8/26/2020 3:33 PM |
| 19 | Continue maintenance and upkeep | 8/26/2020 3:14 PM |
| 20 | Maintenance. Already seem to have issues with it in the parks at times. Also security would be a concern but overall that is great in town | 8/26/2020 2:47 PM |

Q7 Do you agree with the proposed locations of the Bike Boulevards (indicated in blue on the map below).

Answered: 33 Skipped: 2



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| I like them shown where they are. | 33.33% 11 |
| I generally like the locations, but with some adjustments. | 39.39% 13 |
| I have no firm opinion one way or the other. | 24.24% 8 |
| I do not think bike boulevards should be used in Mascoutah. | 3.03% 1 |
| TOTAL | 33 |

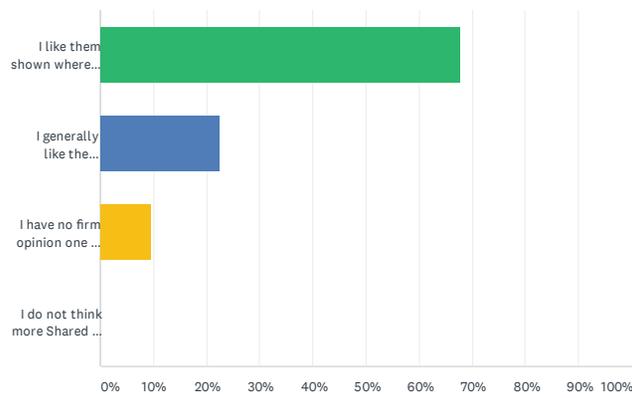
Q8 What changes would you suggest?

Answered: 10 Skipped: 25

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | Leu Civic Center needs to be on one of the routes. | 8/31/2020 9:52 PM |
| 2 | Poplar Street is narrow so it may not be comfortable riding a bike there. | 8/31/2020 9:30 AM |
| 3 | I don't think Harnett and Sixth Streets should be bicycle boulevards. I think Poplar St. Would be a better East/west and I'm not sure which side streets would work better for North/South instead of Sixth Street. There are already many accidents at Harnett and Sixth Street Intersection especially when school is in session. I do like the idea of Church Street being used. | 8/30/2020 8:35 PM |
| 4 | A connection from north to south on both the east and west sides of town, forming a loop around the city | 8/28/2020 12:22 PM |
| 5 | jefferson is too busy to incorporate a bike trail | 8/26/2020 7:54 PM |
| 6 | All of it connected | 8/26/2020 5:39 PM |
| 7 | Speed limit would need to be enforced. | 8/26/2020 4:02 PM |
| 8 | dedicated bike lanes with solid white lines if in use with vehicle traffic so bikers don't take up the whole road | 8/26/2020 3:55 PM |
| 9 | Connecting a path to the SW bike paths | 8/26/2020 2:54 PM |
| 10 | Not sure why have corridor near nursing home in southwest portion. Also be cool to have one along LN ROW | 8/26/2020 2:49 PM |

Q9 Do you agree with the proposed locations of additional Shared Use Paths (indicated in yellow and dashed yellow on the map below).

Answered: 31 Skipped: 4



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| I like them shown where they are. | 67.74% 21 |
| I generally like the locations but with some adjustments. | 22.58% 7 |
| I have no firm opinion one way or the other. | 9.68% 3 |
| I do not think more Shared Use Paths should be used in Mascoutah. | 0.00% 0 |
| TOTAL | 31 |

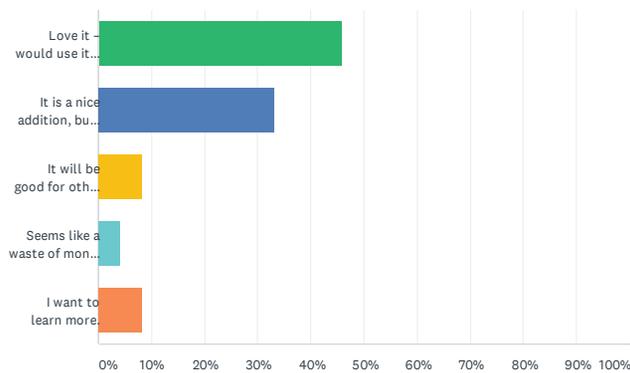
Q10 What changes would you suggest?

Answered: 7 Skipped: 28

| # | RESPONSES | DATE |
|---|--|--------------------|
| 1 | Get rid of the yellow dash trails. No one travels or visits the southern part of town. No reason for those trails. | 9/2/2020 12:42 AM |
| 2 | would not locate those on state routes | 8/27/2020 2:59 PM |
| 3 | Shared use of Brickyard road to County park about 1 mile south of town. | 8/27/2020 10:54 AM |
| 4 | Not using Jefferson St | 8/26/2020 7:57 PM |
| 5 | Need to connect all the subdivisions north of the "big ditch" so people can get into town and also to Dollar General. | 8/26/2020 5:02 PM |
| 6 | None at the moment | 8/26/2020 4:21 PM |
| 7 | Shift the beginnings of the paths on Route 4 to Fountain View Drive. A lot of drivers drive above the speed limit on Route 4 in this area. | 8/26/2020 3:56 PM |

Q1 What do you think about adding shared use paths along side-streets in Mascoutah (like N County Rd, Harper Rd, and/or Brickyard Rd)?

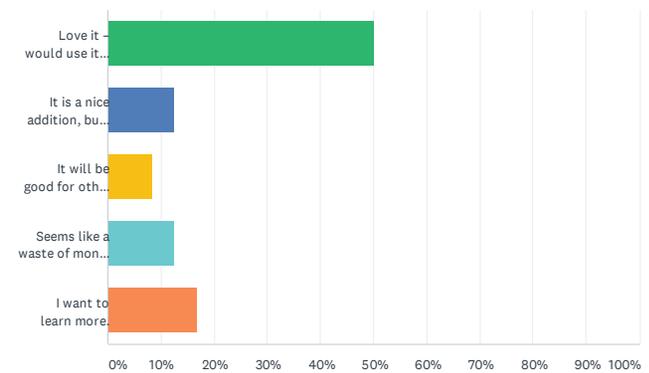
Answered: 24 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| Love it - would use it all the time. | 45.83% 11 |
| It is a nice addition, but I will only use periodically. | 33.33% 8 |
| It will be good for others to use. | 8.33% 2 |
| Seems like a waste of money, don't do it. | 4.17% 1 |
| I want to learn more. | 8.33% 2 |
| TOTAL | 24 |

Q2 What do you think about the bike boulevard treatments along low volume streets in Mascoutah (like Church St, Poplar St, and/or Harnett St)?

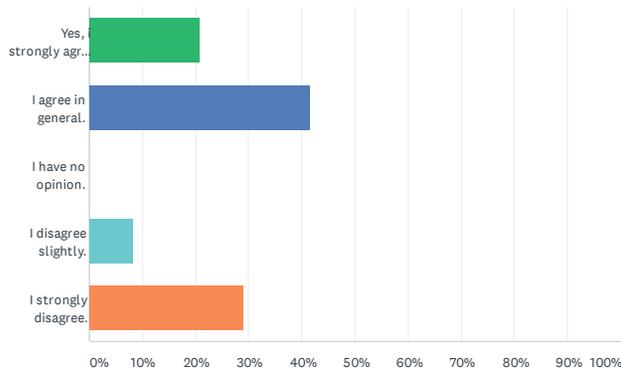
Answered: 24 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| Love it - would use it all the time. | 50.00% 12 |
| It is a nice addition, but I will only use periodically. | 12.50% 3 |
| It will be good for others to use. | 8.33% 2 |
| Seems like a waste of money, don't do it. | 12.50% 3 |
| I want to learn more. | 16.67% 4 |
| TOTAL | 24 |

Q3 Regarding the Economic Development tools that were discussed, do you support the idea of providing financial support (such as TIF district, historic tax credits, facade improvement grants, etc.) for Main Street properties in the study area?

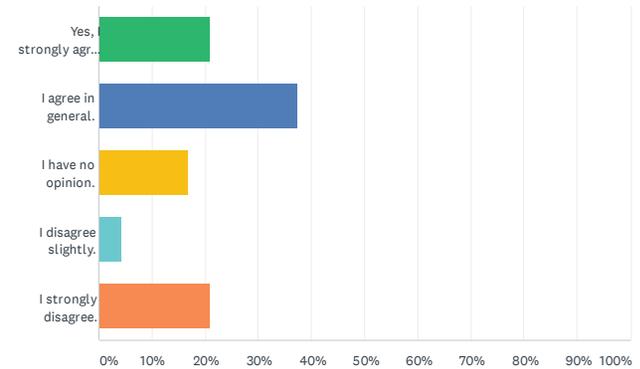
Answered: 24 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---------------------------------------|-----------|----|
| Yes, i strongly agree with this idea. | 20.83% | 5 |
| I agree in general. | 41.67% | 10 |
| I have no opinion. | 0.00% | 0 |
| I disagree slightly. | 8.33% | 2 |
| I strongly disagree. | 29.17% | 7 |
| TOTAL | | 24 |

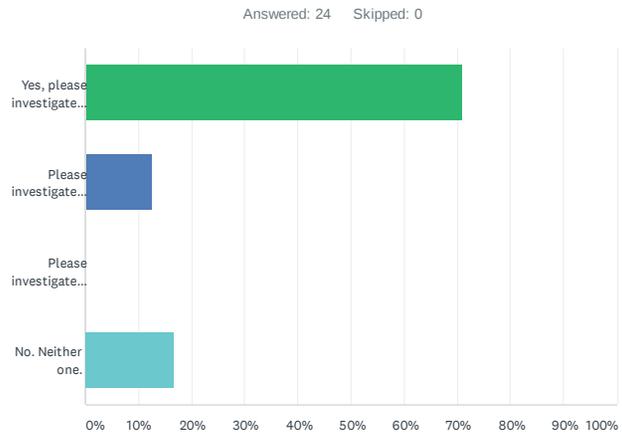
Q4 Do you likewise support the technical support (such as tenant screening assistance, contracting or legal assistance, marketing assistance, etc.) for the Main Street properties in the study area?

Answered: 24 Skipped: 0



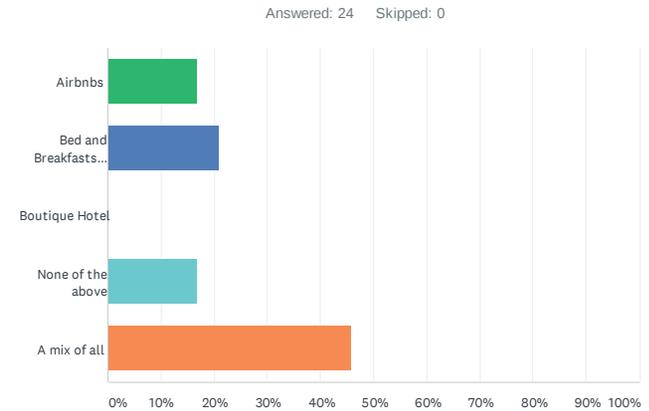
| ANSWER CHOICES | RESPONSES | |
|---------------------------------------|-----------|----|
| Yes, I strongly agree with this idea. | 20.83% | 5 |
| I agree in general. | 37.50% | 9 |
| I have no opinion. | 16.67% | 4 |
| I disagree slightly. | 4.17% | 1 |
| I strongly disagree. | 20.83% | 5 |
| TOTAL | | 24 |

Q5 Do you support the use of technical support or financial support (such as rental licensing, review of zoning codes, connectivity and provision of amenities, etc.) for the older neighborhoods south of Main Street to maintain quality and level of amenity consistent with other Mascoutah neighborhoods?



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| Yes, please investigate both technical and financial support options. | 70.83% 17 |
| Please investigate technical options only. | 12.50% 3 |
| Please investigate financial support options only. | 0.00% 0 |
| No. Neither one. | 16.67% 4 |
| TOTAL | 24 |

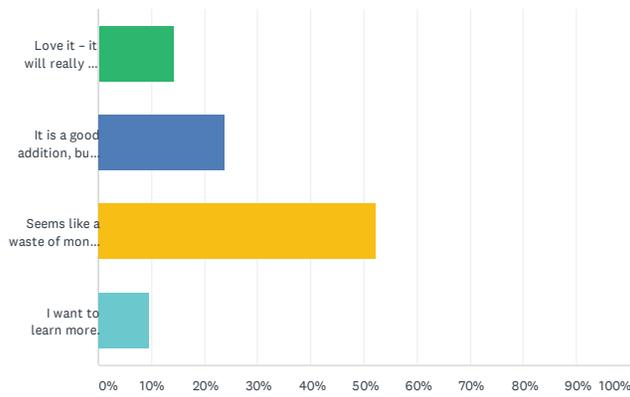
Q6 Regarding the potential for short-term rentals, b&bs, etc. in the study corridor, which of the following hospitality products would you be most interested in seeing on Main Street?



| ANSWER CHOICES | RESPONSES |
|-------------------------------------|-----------|
| Airbnbs | 16.67% 4 |
| Bed and Breakfasts (historic homes) | 20.83% 5 |
| Boutique Hotel | 0.00% 0 |
| None of the above | 16.67% 4 |
| A mix of all | 45.83% 11 |
| TOTAL | 24 |

Q1 What do you think about using entry signage (monument strategies) as a transition to Main Street from the rural area east of Mine Road?

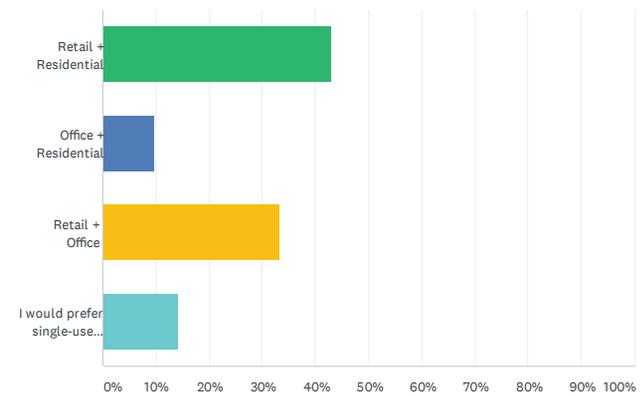
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | COUNT |
|--|-----------|-----------|
| Love it – it will really add to Mascoutah. | 14.29% | 3 |
| It is a good addition, but I am indifferent. | 23.81% | 5 |
| Seems like a waste of money, don't do it. | 52.38% | 11 |
| I want to learn more. | 9.52% | 2 |
| TOTAL | | 21 |

Q2 Which mix of land-uses would you prefer to see on Main Street?

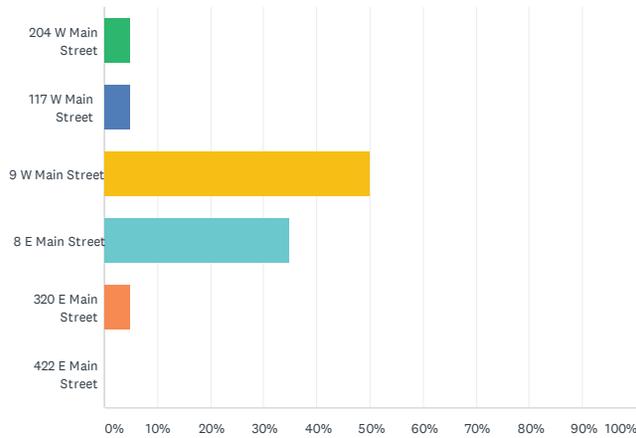
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | COUNT |
|---|-----------|-----------|
| Retail + Residential | 42.86% | 9 |
| Office + Residential | 9.52% | 2 |
| Retail + Office | 33.33% | 7 |
| I would prefer single-use buildings over mixed-use. | 14.29% | 3 |
| TOTAL | | 21 |

Q3 Which of the building redevelopments would you like to see prioritized?

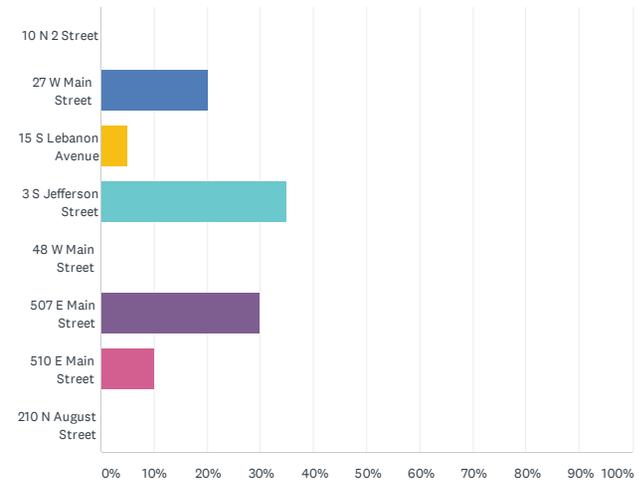
Answered: 20 Skipped: 1



| ANSWER CHOICES | RESPONSES | Count |
|-------------------|-----------|-----------|
| 204 W Main Street | 5.00% | 1 |
| 117 W Main Street | 5.00% | 1 |
| 9 W Main Street | 50.00% | 10 |
| 8 E Main Street | 35.00% | 7 |
| 320 E Main Street | 5.00% | 1 |
| 422 E Main Street | 0.00% | 0 |
| TOTAL | | 20 |

Q4 Which of the following vacant sites would you like to see prioritized for infill development?

Answered: 20 Skipped: 1



| ANSWER CHOICES | RESPONSES | Count |
|----------------------|-----------|-----------|
| 10 N 2 Street | 0.00% | 0 |
| 27 W Main Street | 20.00% | 4 |
| 15 S Lebanon Avenue | 5.00% | 1 |
| 3 S Jefferson Street | 35.00% | 7 |
| 48 W Main Street | 0.00% | 0 |
| 507 E Main Street | 30.00% | 6 |
| 510 E Main Street | 10.00% | 2 |
| 210 N August Street | 0.00% | 0 |
| TOTAL | | 20 |

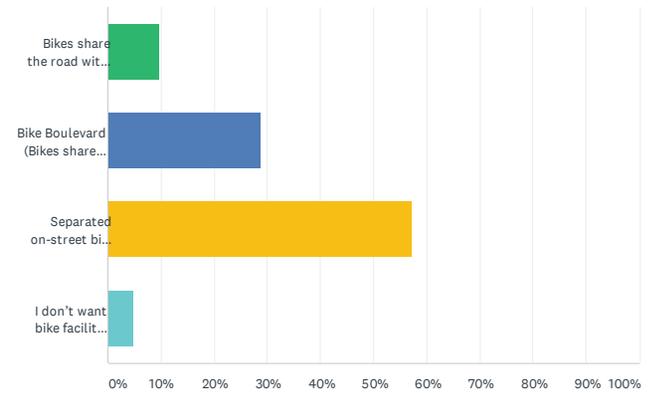
Q5 Mascoutah is considered a "Historic" town. Can you share your favorite historical element of Mascoutah?

Answered: 12 Skipped: 9

| # | RESPONSES | DATE |
|----|---|--------------------|
| 1 | The depot | 9/2/2020 1:26 PM |
| 2 | I don't consider Mascoutah a historic town. | 9/2/2020 12:50 AM |
| 3 | The area surrounding Holy Childhood Church | 8/31/2020 9:59 PM |
| 4 | The small town feel and atmosphere. It's very difficult to answer the previous questions in the current format. Basically we believe restaurants and small specialty type stores are preferred. | 8/31/2020 9:17 PM |
| 5 | The history behind the mill and the adjoining mural building. It would be nice to capitalize on the historic nature of it and that area be utilized for public gatherings/ restaurants/ farmers market, etc | 8/30/2020 8:49 PM |
| 6 | Center of population | 8/28/2020 12:36 PM |
| 7 | Homes on South Railway. Maybe the Ace Hardware building. | 8/27/2020 11:06 AM |
| 8 | Former center of population. | 8/26/2020 5:46 PM |
| 9 | It was the mill | 8/26/2020 5:00 PM |
| 10 | The bowling alley | 8/26/2020 4:07 PM |
| 11 | The historic old buildings on main street with the brick facades | 8/26/2020 3:25 PM |
| 12 | Historical building | 8/26/2020 2:44 PM |

Q6 Which of the following options would you prefer for new bike facilities along Church Street?

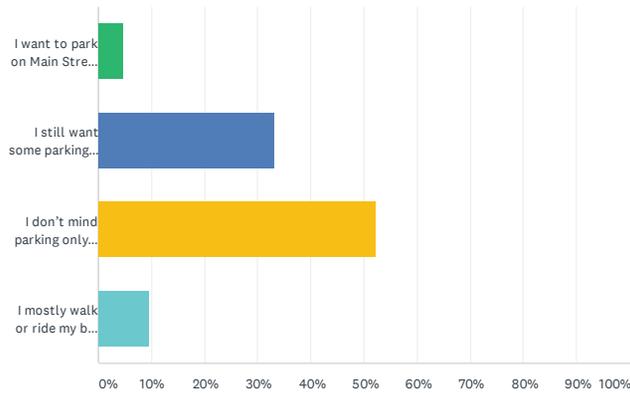
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| Bikes share the road with cars as existing. | 9.52% 2 |
| Bike Boulevard (Bikes share road with cars with more traffic calming elements.) | 28.57% 6 |
| Separated on-street bike lanes. | 57.14% 12 |
| I don't want bike facilities on Church Street. | 4.76% 1 |
| TOTAL | 21 |

Q7 How would you prefer to park in downtown Mascoutah?

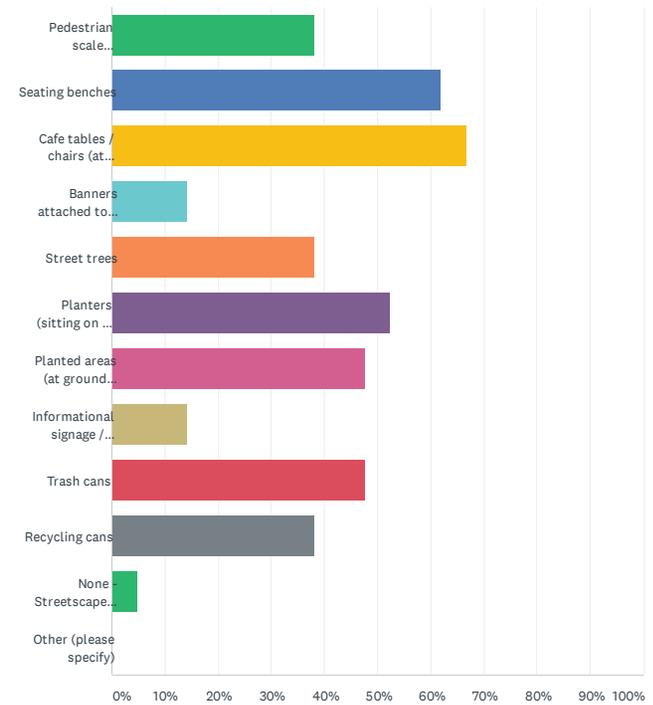
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| I want to park on Main Street right outside my destination. | 4.76% 1 |
| I still want some parking on Main Street, but I don't mind walking a few blocks to my destination. | 33.33% 7 |
| I don't mind parking only on side streets or back lots off Main Street. | 52.38% 11 |
| I mostly walk or ride my bike to Main Street. | 9.52% 2 |
| TOTAL | 21 |

Q8 We have heard that Main Street lacks sufficient "streetscape" amenities. Which of the sidewalk elements listed below would you like to see incorporated on Main Street? (select all that apply)

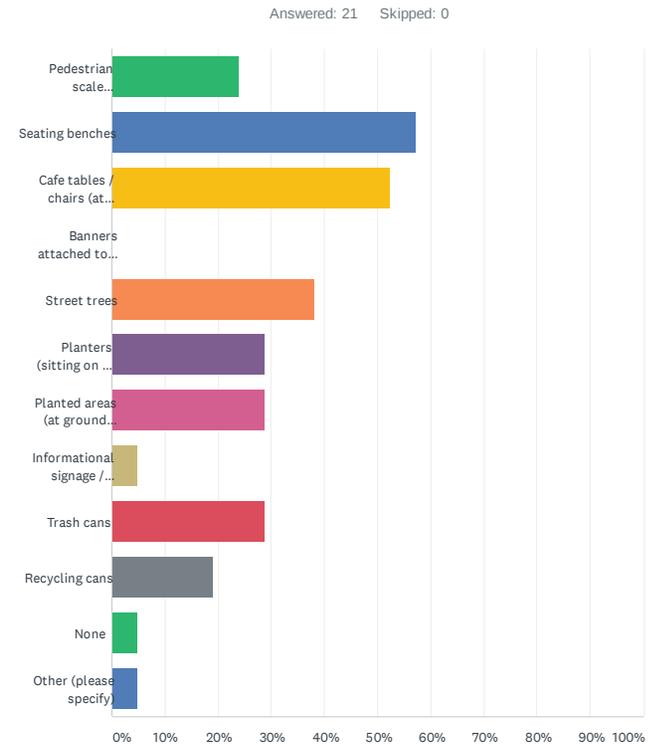
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| Pedestrian scale ornamental lighting | 38.10% 8 |
| Seating benches | 61.90% 13 |
| Cafe tables / chairs (at restaurants) | 66.67% 14 |
| Banners attached to light poles | 14.29% 3 |
| Street trees | 38.10% 8 |
| Planters (sitting on the ground) | 52.38% 11 |
| Planted areas (at ground level) | 47.62% 10 |
| Informational signage / kiosks | 14.29% 3 |
| Trash cans | 47.62% 10 |
| Recycling cans | 38.10% 8 |
| None - Streetscape amenities are not important for Main Street | 4.76% 1 |
| Other (please specify) | 0.00% 0 |
| Total Respondents: 21 | |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|-------------------------|------|
| | There are no responses. | |

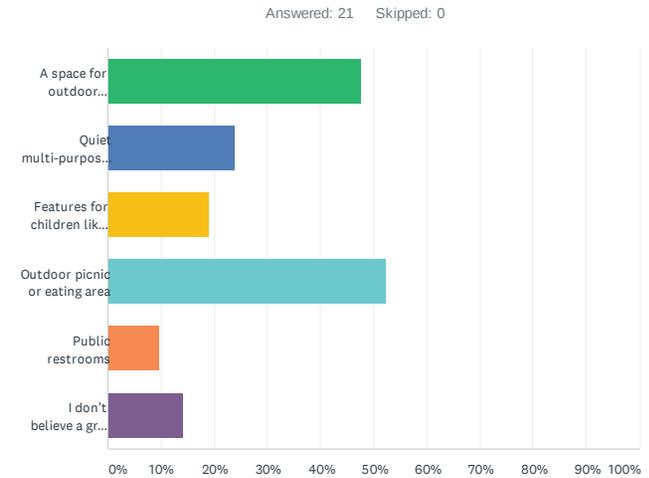
Q9 Please select your top three priorities for streetscape amenities:



| ANSWER CHOICES | RESPONSES | |
|---------------------------------------|-----------|----|
| Pedestrian scale ornamental lighting | 23.81% | 5 |
| Seating benches | 57.14% | 12 |
| Cafe tables / chairs (at restaurants) | 52.38% | 11 |
| Banners attached to light poles | 0.00% | 0 |
| Street trees | 38.10% | 8 |
| Planters (sitting on the ground) | 28.57% | 6 |
| Planted areas (at ground level) | 28.57% | 6 |
| Informational signage / kiosks | 4.76% | 1 |
| Trash cans | 28.57% | 6 |
| Recycling cans | 19.05% | 4 |
| None | 4.76% | 1 |
| Other (please specify) | 4.76% | 1 |
| Total Respondents: 21 | | |

| # | OTHER (PLEASE SPECIFY) | DATE |
|---|------------------------|-------------------|
| 1 | Saving tax payer money | 9/2/2020 12:50 AM |

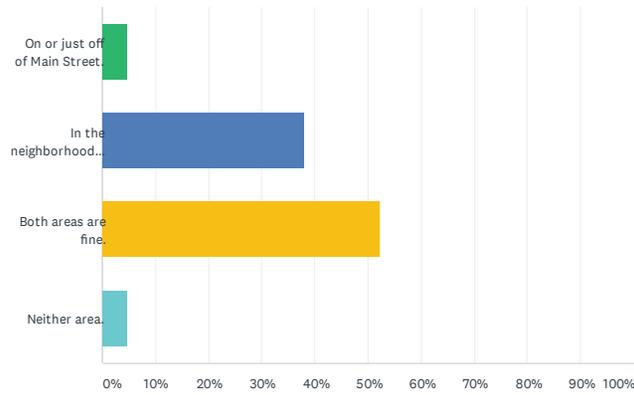
Q10 If a significant green space were included in the Main Street Plan, of the following, what items would you like to see included (choose up to two)?



| ANSWER CHOICES | RESPONSES | |
|---|-----------|----|
| A space for outdoor performances | 47.62% | 10 |
| Quiet multi-purpose green space | 23.81% | 5 |
| Features for children like a splash pad or playground | 19.05% | 4 |
| Outdoor picnic or eating area | 52.38% | 11 |
| Public restrooms | 9.52% | 2 |
| I don't believe a green space is needed | 14.29% | 3 |
| Total Respondents: 21 | | |

Q11 Where, in general, would you approve of new small lot single family homes in the following areas:

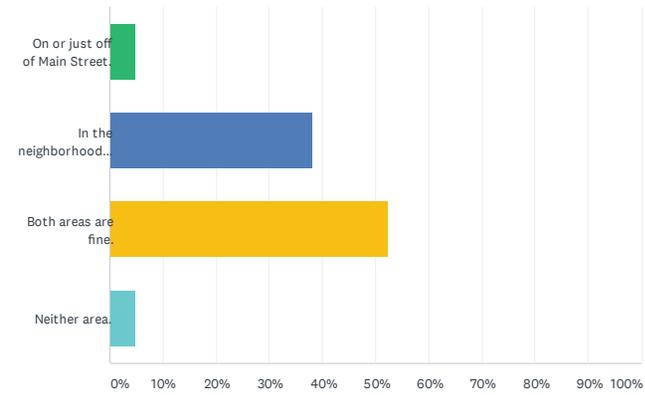
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|----|
| On or just off of Main Street. | 4.76% | 1 |
| In the neighborhoods adjacent to Main Street. | 38.10% | 8 |
| Both areas are fine. | 52.38% | 11 |
| Neither area. | 4.76% | 1 |
| TOTAL | | 21 |

Q12 Where, in general, would you approve of new single family cottages in the following areas:

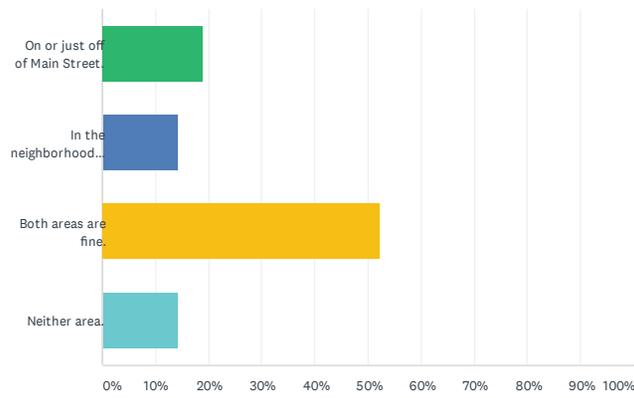
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|----|
| On or just off of Main Street. | 4.76% | 1 |
| In the neighborhoods adjacent to Main Street. | 38.10% | 8 |
| Both areas are fine. | 52.38% | 11 |
| Neither area. | 4.76% | 1 |
| TOTAL | | 21 |

Q13 Where, in general, would you approve of new modern rowhouses in the following areas:

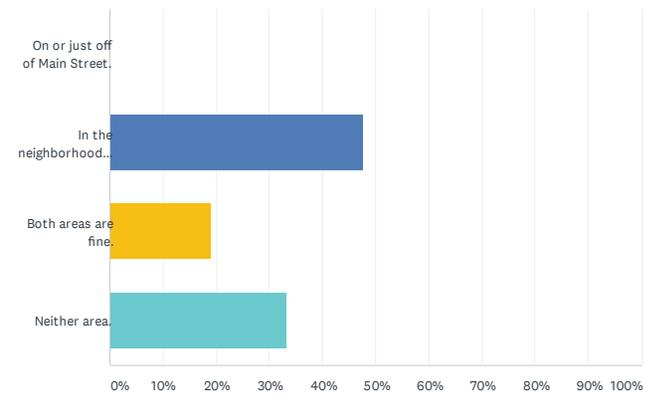
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|----|
| On or just off of Main Street. | 19.05% | 4 |
| In the neighborhoods adjacent to Main Street. | 14.29% | 3 |
| Both areas are fine. | 52.38% | 11 |
| Neither area. | 14.29% | 3 |
| TOTAL | | 21 |

Q14 Where, in general, would you approve of new semi-attached townhomes in the following areas:

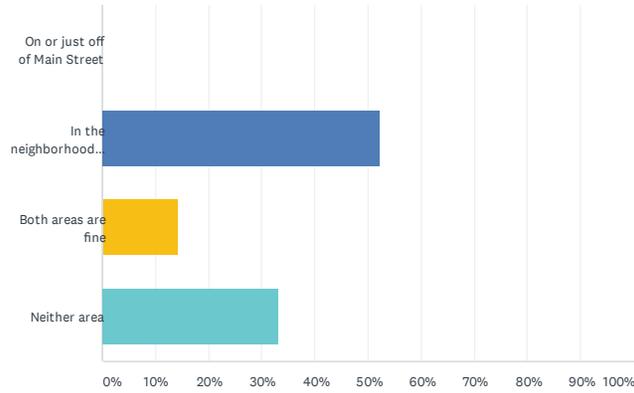
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|----|
| On or just off of Main Street. | 0.00% | 0 |
| In the neighborhoods adjacent to Main Street. | 47.62% | 10 |
| Both areas are fine. | 19.05% | 4 |
| Neither area. | 33.33% | 7 |
| TOTAL | | 21 |

Q15 Where, in general, would you approve of new residentially scaled apartments in the following areas:

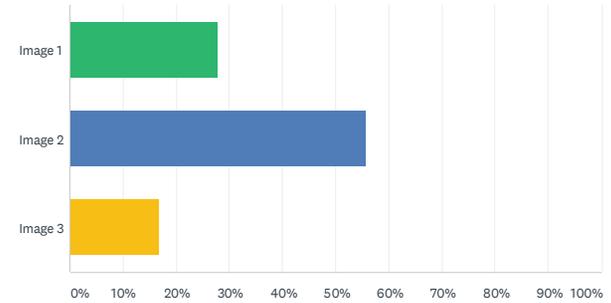
Answered: 21 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| On or just off of Main Street | 0.00% | 0 |
| In the neighborhoods adjacent to Main Street | 52.38% | 11 |
| Both areas are fine | 14.29% | 3 |
| Neither area | 33.33% | 7 |
| TOTAL | | 21 |

Q1 Which option do you prefer for the west segment (Main Street from 6th to 2nd Street)?

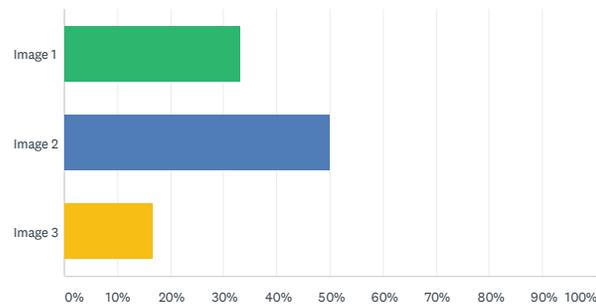
Answered: 18 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----------|
| Image 1 | 27.78% | 5 |
| Image 2 | 55.56% | 10 |
| Image 3 | 16.67% | 3 |
| TOTAL | | 18 |

Q2 Which option do you prefer for the central segment (Main Street from Independence Street to Mine Street)?

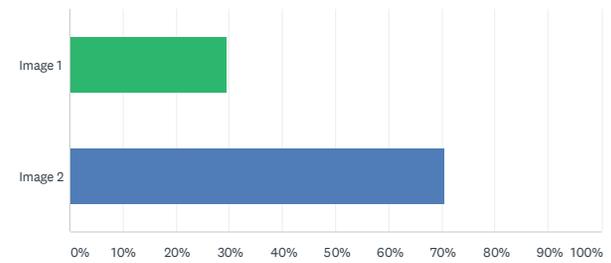
Answered: 18 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|----|
| Image 1 | 33.33% | 6 |
| Image 2 | 50.00% | 9 |
| Image 3 | 16.67% | 3 |
| TOTAL | | 18 |

Q3 Which option do you prefer for the east segment (Main Street from Independence Street to Mine Street)?

Answered: 17 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|----|
| Image 1 | 29.41% | 5 |
| Image 2 | 70.59% | 12 |
| TOTAL | | 17 |

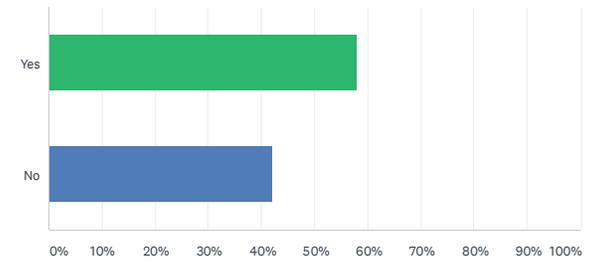
Q4 Do you have any specific comments on the proposed configurations within any or all of the particular segments (i.e. thoughts on removing on-street parking or recommendations for particular uses of the pedestrian space)?

Answered: 6 Skipped: 12

| # | RESPONSES | DATE |
|---|--|-------------------|
| 1 | The set up of main street is fine. The side walks need to be repaired but that's about it. Major design changes would be a waste of money. | 9/2/2020 12:56 AM |
| 2 | We'd like to see a focus on aesthetics and bringing in more restaurants and small business. | 8/31/2020 9:25 PM |
| 3 | Do not get rid of on street parking, add to it. | 8/26/2020 5:49 PM |
| 4 | Would like to keep street parking or add parking lots behind the buildings | 8/26/2020 4:12 PM |
| 5 | Get rid of on Main Street parking | 8/26/2020 4:11 PM |
| 6 | Be really nice to have extra wide sidewalks downtown to allow for outdoor dining like Belleville. Create a more fun downtown | 8/26/2020 2:41 PM |

Q1 The curb extensions shown in the video are intended for stormwater management purposes. Should they be extended to all four corners of each Main Street intersection, even if not needed for stormwater management?

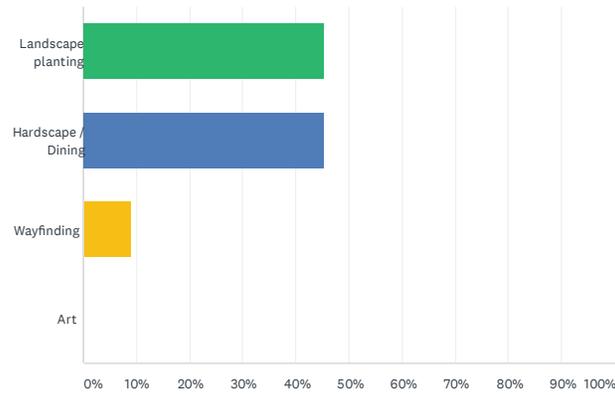
Answered: 19 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|----|
| Yes | 57.89% | 11 |
| No | 42.11% | 8 |
| TOTAL | | 19 |

Q2 What should the space within those curb extensions become?

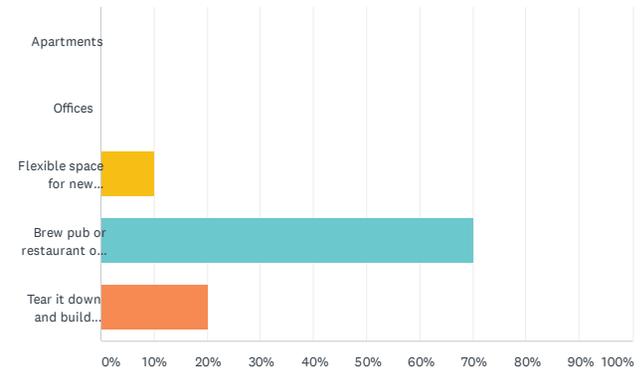
Answered: 11 Skipped: 8



| ANSWER CHOICES | RESPONSES | |
|--------------------|-----------|-----------|
| Landscape planting | 45.45% | 5 |
| Hardscape / Dining | 45.45% | 5 |
| Wayfinding | 9.09% | 1 |
| Art | 0.00% | 0 |
| TOTAL | | 11 |

Q1 What would be your preferred use for the "Mural Building" on Railway Street?

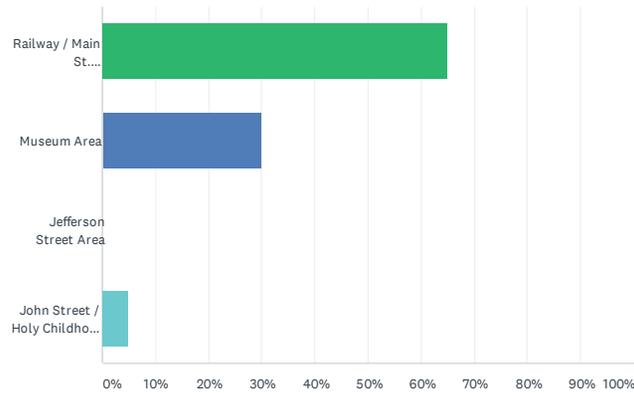
Answered: 20 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Apartments | 0.00% | 0 |
| Offices | 0.00% | 0 |
| Flexible space for new business starts | 10.00% | 2 |
| Brew pub or restaurant on the lower level with some other use above | 70.00% | 14 |
| Tear it down and build something new on the site | 20.00% | 4 |
| TOTAL | | 20 |

Q2 Which key site proposal do you like the best?

Answered: 20 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Railway / Main St. Intersection | 65.00% | 13 |
| Museum Area | 30.00% | 6 |
| Jefferson Street Area | 0.00% | 0 |
| John Street / Holy Childhood Promenade | 5.00% | 1 |
| TOTAL | | 20 |

Q3 Do you have any specific comments about any of the proposed key sites?

Answered: 9 Skipped: 11

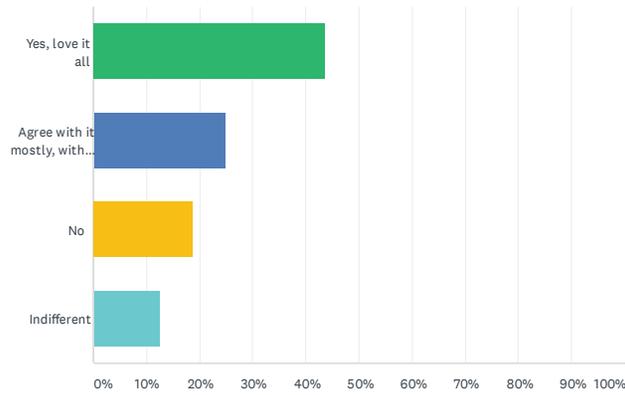
| # | RESPONSES | DATE |
|---|---|--------------------|
| 1 | All the options seem like a waste of money. | 9/2/2020 12:53 AM |
| 2 | How is all of this being funded? Hopefully taxes won't be raised. Please consider utilizing Sheve Park for some green space ideas, and the green space across from Tom's Market, if possible. An example for Sheve Park would be a monthly Farmers Market. | 8/31/2020 9:38 PM |
| 3 | All good ideas. | 8/31/2020 10:46 AM |
| 4 | It was a toss up between the railway intersection and museum. The railway intersection is the "hub" of town and feel that it is underutilized now and could be transformed into a busy city gathering area. However, I also really like the idea of the history path along Hog River—the path and green space combined with sitting areas and historical information would be a relaxing place to walk and spend some time. | 8/30/2020 9:03 PM |
| 5 | Fix the eyesore moto mart plaza! | 8/26/2020 5:51 PM |
| 6 | Create a nice atmosphere and get rid of parking on Main street so there is more room for sidewalks and businesses to use. | 8/26/2020 5:10 PM |
| 7 | Would be nice to have an icecream shop in town, similar to Walton's in Smithton. | 8/26/2020 4:14 PM |
| 8 | No on street parking | 8/26/2020 4:13 PM |
| 9 | Be awesome to have brewery with outdoor entertainment space pub | 8/26/2020 2:39 PM |

APPENDIX E-3

Survey Results - Round 3

Q1 Do you agree with the recommended regional connection plan?

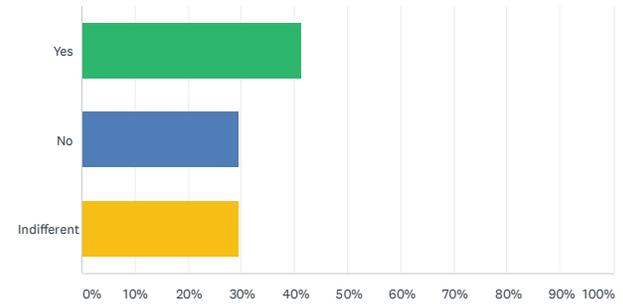
Answered: 16 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, love it all | 43.75% | 7 |
| Agree with it mostly, with some changes | 25.00% | 4 |
| No | 18.75% | 3 |
| Indifferent | 12.50% | 2 |
| TOTAL | | 16 |

Q2 Do you agree with prioritizing the connection to St. Clair County BikeLink trail over the connection to Scott Airforce Base and the Multi-Modal Hub at the MidAmerica Airport?

Answered: 17 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----------|
| Yes | 41.18% | 7 |
| No | 29.41% | 5 |
| Indifferent | 29.41% | 5 |
| TOTAL | | 17 |

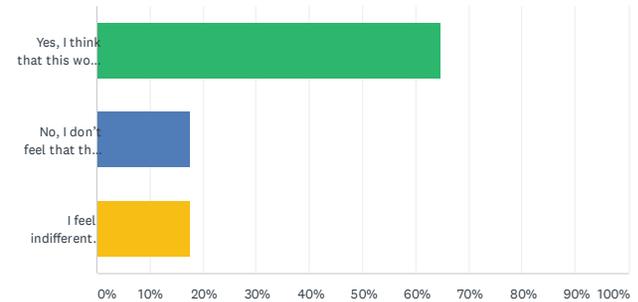
Q3 What changes would you make to the regional connection plan?

Answered: 9 Skipped: 8

| # | RESPONSES | DATE |
|---|--|--------------------|
| 1 | None | 9/21/2020 11:37 PM |
| 2 | Would there be a way long term if the focus was initially on connecting with the st Clair county bike trail, but to eventually create a shortcut to also connect with SAFB? I was also wondering if somehow we could obtain the rights to the L &N railway track into Belleville? I also would like consideration of a bike path around the perimeter of Mascoutah. I can't remember if that was in the plan | 9/21/2020 12:54 PM |
| 3 | More accessible entry | 9/21/2020 10:09 AM |
| 4 | None | 9/16/2020 9:43 AM |
| 5 | eliminate it | 9/16/2020 7:19 AM |
| 6 | Connect to safb | 9/15/2020 7:12 PM |
| 7 | Always more trees, more gardens, more green space. | 9/15/2020 6:56 PM |
| 8 | None | 9/15/2020 6:27 PM |
| 9 | Not do it | 9/15/2020 5:06 PM |

Q4 Do you agree with the proposed regional highway wayfinding directional signage, located on Hwy 64 and Rte. 4 and at Rte. 177 and 158?

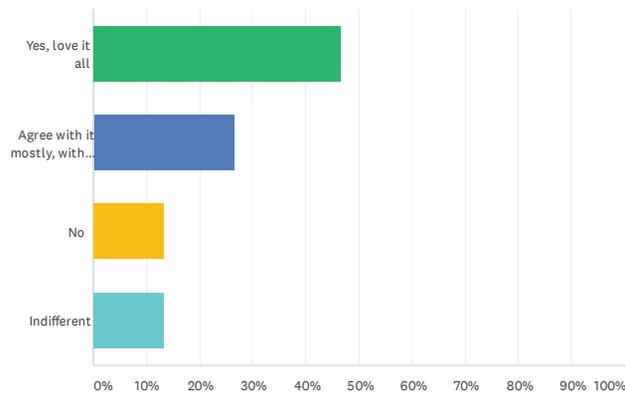
Answered: 17 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|---|-----------|
| Yes, I think that this would be beneficial. | 64.71% 11 |
| No, I don't feel that they are needed. | 17.65% 3 |
| I feel indifferent. | 17.65% 3 |
| TOTAL | 17 |

Q1 Do you agree with the recommended pedestrian and bike network plan?

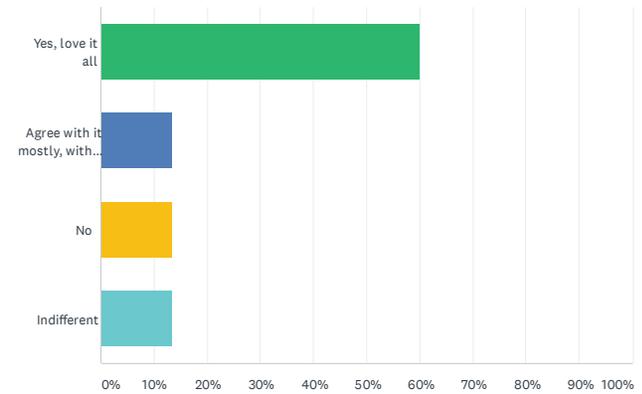
Answered: 15 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, love it all | 46.67% | 7 |
| Agree with it mostly, with some changes | 26.67% | 4 |
| No | 13.33% | 2 |
| Indifferent | 13.33% | 2 |
| TOTAL | | 15 |

Q2 Do you agree with the recommended first phase implementation of bike boulevard loop on 10th Street, Railway Street, Independence Street, Poplar Street, South Street, and temporarily on Church Street?

Answered: 15 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, love it all | 60.00% | 9 |
| Agree with it mostly, with some changes | 13.33% | 2 |
| No | 13.33% | 2 |
| Indifferent | 13.33% | 2 |
| TOTAL | | 15 |

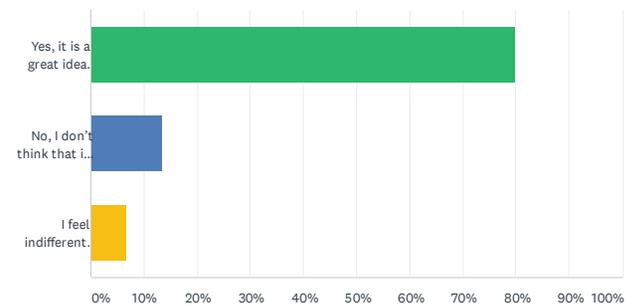
Q3 Would you make any changes to the pedestrian and bike network plan?

Answered: 7 Skipped: 8

| # | RESPONSES | DATE |
|---|---|--------------------|
| 1 | No | 9/21/2020 11:38 PM |
| 2 | no changes | 9/21/2020 2:59 PM |
| 3 | I would hesitate using Harnett and Sixth Streets as main routes for shared bike routes. I would suggest Poplar St. for the East/west route (with some re-routing on the eastern edge of town) and 10th Street as the north/south route. Harnett and Sixth st are too heavily traveled, and already too many accidents occurring at Harnett and Sixth St. Intersection | 9/21/2020 1:00 PM |
| 4 | none | 9/16/2020 9:44 AM |
| 5 | eliminate it | 9/16/2020 7:20 AM |
| 6 | No | 9/15/2020 6:59 PM |
| 7 | Unsure | 9/15/2020 6:58 PM |

Q4 Do you agree with the following recommendation for the bike trail wayfinding network?: Add informational and directional signage as the proposed trails are implemented.

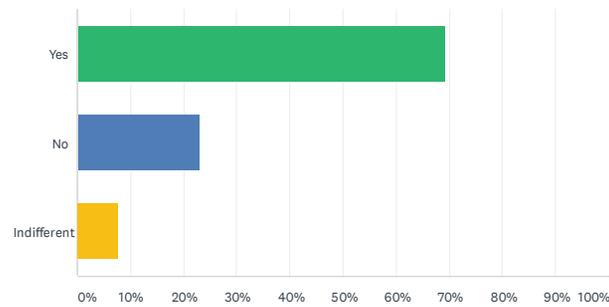
Answered: 15 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, it is a great idea. | 80.00% | 12 |
| No, I don't think that it is necessary. | 13.33% | 2 |
| I feel indifferent. | 6.67% | 1 |
| TOTAL | | 15 |

Q1 Do you agree with the recommendation for a Shared Use Path on the north side of Church Street instead of a Bike Boulevard?

Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----------|
| Yes | 69.23% | 9 |
| No | 23.08% | 3 |
| Indifferent | 7.69% | 1 |
| TOTAL | | 13 |

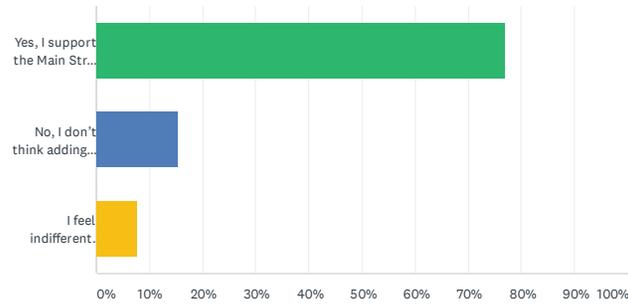
Q2 For your answer to the previous question, why or why not?

Answered: 6 Skipped: 7

| # | RESPONSES | DATE |
|---|---|--------------------|
| 1 | The street is wide enough to support a shared use path | 9/21/2020 11:42 PM |
| 2 | I don't think church street is heavily traveled enough to warrant a bike boulevard. It's already fairly bike friendly | 9/21/2020 1:12 PM |
| 3 | The street is busy enough without a bike lane! | 9/21/2020 10:10 AM |
| 4 | Bikes and pedestrians sharing the same path is not a good idea IMO... Fix the sidewalks so they are useable and add a separate bike lane on Church Street | 9/21/2020 8:53 AM |
| 5 | I still have concerns for traffic. I've seen bikes in sidewalks in town, so bicyclists might not feel comfortable sharing the road with cars. | 9/16/2020 9:48 AM |
| 6 | no need for this in residential areas | 9/16/2020 7:23 AM |

Q3 Do you agree with the recommendations of using pedestrian and vehicular wayfinding signage along the Main Street corridor to direct, inform, and guide visitors?

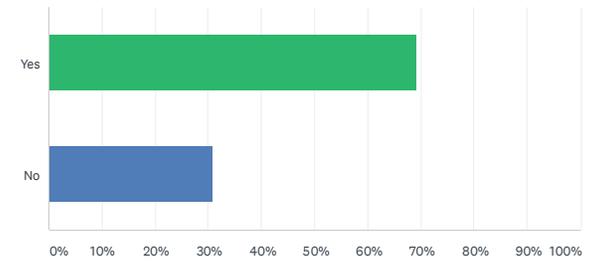
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Yes, I support the Main Street wayfinding signage recommendations. | 76.92% | 10 |
| No, I don't think adding wayfinding signage on Main Street is necessary. | 15.38% | 2 |
| I feel indifferent. | 7.69% | 1 |
| TOTAL | | 13 |

Q4 Mascoutah has beautiful sculptures, but mostly on the east end. Do you think organizing the sculpture (possibly relocating existing statues) into the newly created curb extensions as an art walk could be favorably received?

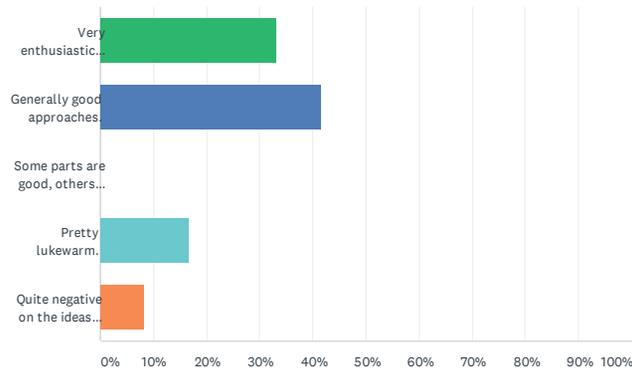
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----------|
| Yes | 69.23% | 9 |
| No | 30.77% | 4 |
| TOTAL | | 13 |

Q5 What is your reaction to the overall ideas discussed for the Main Street corridor?

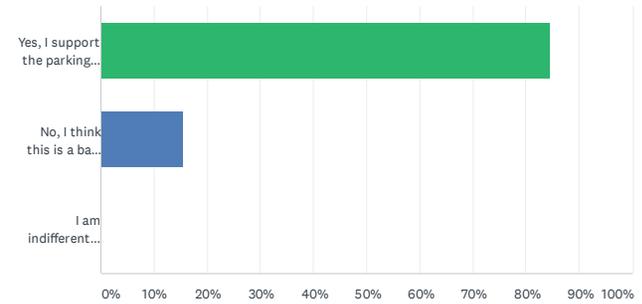
Answered: 12 Skipped: 1



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 33.33% | 4 |
| Generally good approaches. | 41.67% | 5 |
| Some parts are good, others are doubtful. | 0.00% | 0 |
| Pretty lukewarm. | 16.67% | 2 |
| Quite negative on the ideas – off the mark. | 8.33% | 1 |
| TOTAL | | 12 |

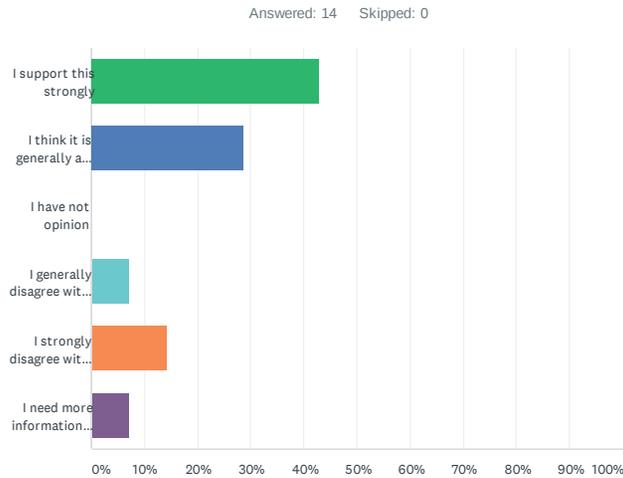
Q6 Do you agree with the recommendations to better organize and coordinate parking behind the buildings along Main Street with primary vehicular access off of Church Street?

Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, I support the parking recommendations. | 84.62% | 11 |
| No, I think this is a bad idea. | 15.38% | 2 |
| I am indifferent about this. | 0.00% | 0 |
| TOTAL | | 13 |

Q1 Do you support establishing a National Historic District on Main Street in the center of downtown Mascoutah, allowing for the use of historic tax credits on major building renovations?



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| I support this strongly | 42.86% | 6 |
| I think it is generally a good idea | 28.57% | 4 |
| I have not opinion | 0.00% | 0 |
| I generally disagree with this idea | 7.14% | 1 |
| I strongly disagree with this idea | 14.29% | 2 |
| I need more information before I decide | 7.14% | 1 |
| TOTAL | | 14 |

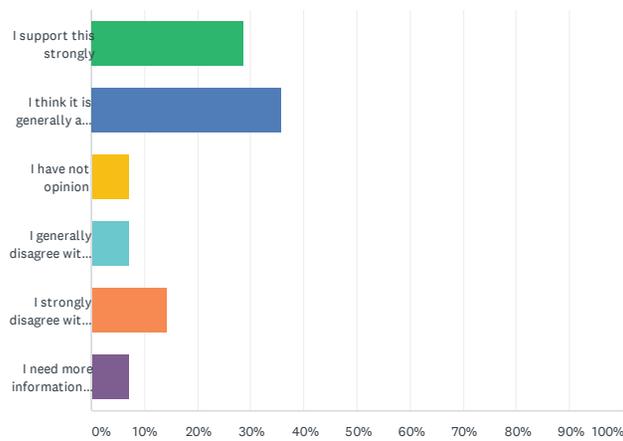
Q2 Do you have specific suggestions that could improve this idea?

Answered: 7 Skipped: 7

| # | RESPONSES | DATE |
|---|---|--------------------|
| 1 | Funds could be used to coordinate building fronts | 9/21/2020 11:41 PM |
| 2 | I love your ideas to turn the postel mill into a pub with spaces in the upper levels of the building for artists, boutique stores, start-ups, and the performance area where the grain elevator was. I also love the idea of using the Fred's hardware/laundromat space for outdoor cafes and indoor dining. I'd love for a coffee/ tea cafe (similar to the Abbey in Belleville). This could be another place to gather/hang out and have live entertainment. I think Mascoutah needs some quaint, creative establishments over just plain functional or franchise establishments. Would like to create a welcoming cozy atmosphere. | 9/21/2020 1:08 PM |
| 3 | No | 9/16/2020 9:46 AM |
| 4 | owners should be responsible for building | 9/16/2020 7:22 AM |
| 5 | Really need downtown food and nightlife. Brew pub that plays to our historical brewery would be awesome | 9/15/2020 7:14 PM |
| 6 | Encourage local school students k-12 to submit reports or projects to help promote our historical buildings & histories in conjunction with the Mascoutah Historical Society. Have them research specific streets, buildings, landmarks & incorporate their findings onto building placards ,and then permanently at museum | 9/15/2020 7:08 PM |
| 7 | No | 9/15/2020 7:01 PM |

Q3 Do you support establishing one or more local historic districts south of Main Street?

Answered: 14 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| I support this strongly | 28.57% | 4 |
| I think it is generally a good idea | 35.71% | 5 |
| I have not opinion | 7.14% | 1 |
| I generally disagree with this idea | 7.14% | 1 |
| I strongly disagree with this idea | 14.29% | 2 |
| I need more information before I decide | 7.14% | 1 |
| TOTAL | | 14 |

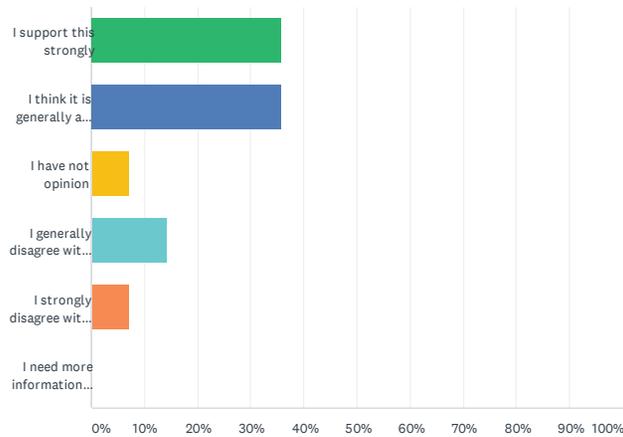
Q4 Do you have specific suggestions that could improve this idea?

Answered: 4 Skipped: 10

| # | RESPONSES | DATE |
|---|--|-------------------|
| 1 | Get rid of mobile homes in town. Some of the homes in this area are not kept up very well. | 9/16/2020 9:46 AM |
| 2 | eliminate it | 9/16/2020 7:22 AM |
| 3 | Designated entry signage, street signage, historical home placards. | 9/15/2020 7:08 PM |
| 4 | No | 9/15/2020 7:01 PM |

Q5 Do you support establishing some sort of business organization for Main Street to collectively address common things such as marketing, parking, district improvements, and some maintenance?

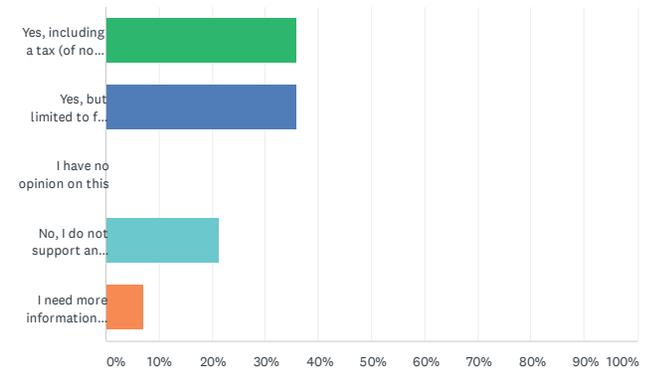
Answered: 14 Skipped: 0



| ANSWER CHOICES | RESPONSES | COUNT |
|---|-----------|-----------|
| I support this strongly | 35.71% | 5 |
| I think it is generally a good idea | 35.71% | 5 |
| I have not opinion | 7.14% | 1 |
| I generally disagree with this idea | 14.29% | 2 |
| I strongly disagree with this idea | 7.14% | 1 |
| I need more information before I decide | 0.00% | 0 |
| TOTAL | | 14 |

Q6 If you do support such an organization, do you support a district that would generate its own revenues to invest back into and manage the district?

Answered: 14 Skipped: 0



| ANSWER CHOICES | RESPONSES | COUNT |
|--|-----------|-----------|
| Yes, including a tax (of no more than 1%) | 35.71% | 5 |
| Yes, but limited to fund raising and contributions | 35.71% | 5 |
| I have no opinion on this | 0.00% | 0 |
| No, I do not support an organized district | 21.43% | 3 |
| I need more information before I decide | 7.14% | 1 |
| TOTAL | | 14 |

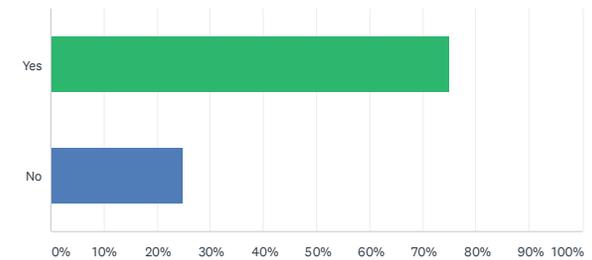
Q7 Do you have specific suggestions that might improve the idea of a business district on Main Street?

Answered: 6 Skipped: 8

| # | RESPONSES | DATE |
|---|---|-------------------|
| 1 | I thought there was already an organization called Main Street Mascoutah. There used to be a paid person leading this group, however I don't recall how it was funded. Maybe we need to institute that again. | 9/21/2020 1:08 PM |
| 2 | This isn't rocket science. It has to start with sidewalks that you can actually use, curbs that aren't crumbling, etc... A repaved main street would be a great help as well. | 9/21/2020 8:49 AM |
| 3 | I do not want property taxes to go up. They go up every year. | 9/16/2020 9:46 AM |
| 4 | let business start/fail on their own | 9/16/2020 7:22 AM |
| 5 | Work with the local chamber of commerce to collaborate with the local businesses. Outdoor eating establishments, a bakery, a butcher, a book store, an ice cream shop, a thrift shop. | 9/15/2020 7:08 PM |
| 6 | No | 9/15/2020 7:01 PM |

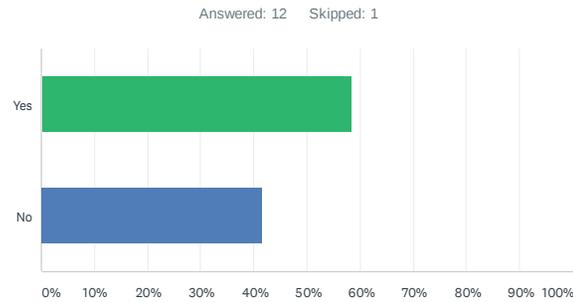
Q1 Do you place a greater value on the additional 4 feet of new pedestrian space, on both sides of Main Street (in the center segment of our Main Street study area), as a better use than the current one-sided parallel parking? The additional 4 feet of sidewalk increases the total from 8 to 12 feet for pedestrians and includes lighting, benches, signs, and trash receptacles.

Answered: 12 Skipped: 1



| ANSWER CHOICES | RESPONSES | COUNT |
|----------------|-----------|-------|
| Yes | 75.00% | 9 |
| No | 25.00% | 3 |
| TOTAL | | 12 |

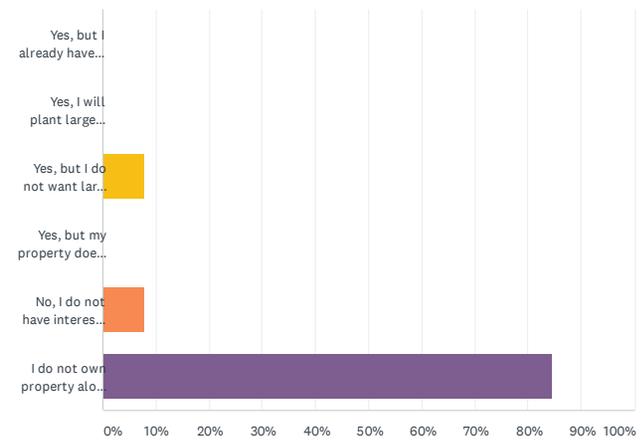
Q2 The Illinois Department of Transportation will not allow street trees species which will grow larger than a 4" trunk diameter at maturity along Main Street. The current plan does not include street trees between 2nd and Lebanon because of low branching height (potential vehicle conflict and blocking business signage) and inability to shade pedestrians in hot weather. Would you prefer to still include small trees for seasonal color and as a way to green the corridor?



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----------|
| Yes | 58.33% | 7 |
| No | 41.67% | 5 |
| TOTAL | | 12 |

Q3 If you own a home or business along Main Street (between 2nd and 6th Streets to the west or between Independence and Mine Streets to the east), do you have interest in planting large canopy trees within your private parcel, since they cannot occur in the Pedestrian ROW (right of way)?

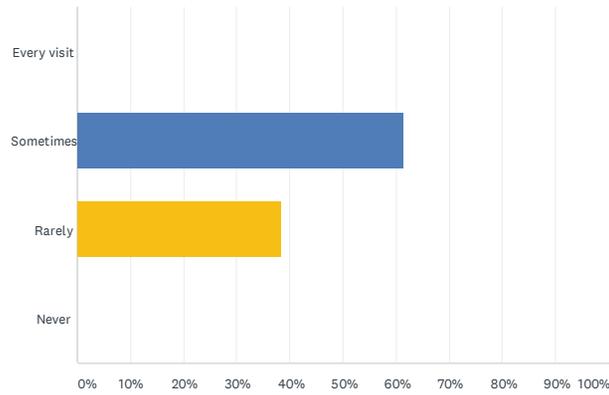
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----------|
| Yes, but I already have large canopy trees. | 0.00% | 0 |
| Yes, I will plant large trees, but need help purchasing and planting them. | 0.00% | 0 |
| Yes, but I do not want large canopy trees on my property. | 7.69% | 1 |
| Yes, but my property does not have a front yard. | 0.00% | 0 |
| No, I do not have interest in planting large canopy trees on my property along Main Street. | 7.69% | 1 |
| I do not own property along Main Street. | 84.62% | 11 |
| TOTAL | | 13 |

Q4 How often do you parallel park your vehicle downtown on Main Street?

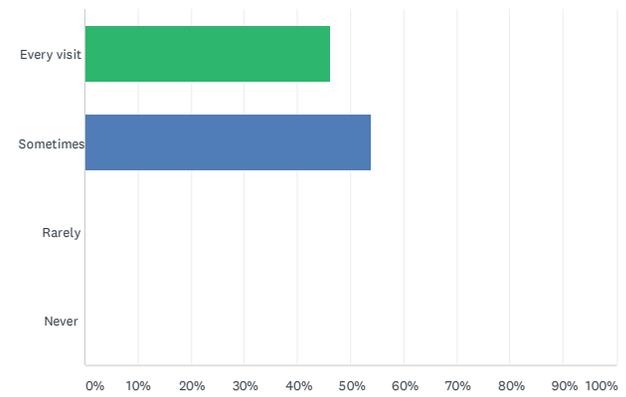
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | COUNT |
|----------------|-----------|-----------|
| Every visit | 0.00% | 0 |
| Sometimes | 61.54% | 8 |
| Rarely | 38.46% | 5 |
| Never | 0.00% | 0 |
| TOTAL | | 13 |

Q5 How often do you find places to park on traditional lots or side streets when visiting downtown?

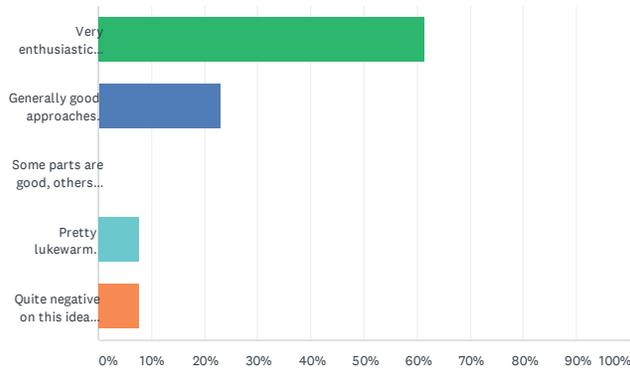
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | COUNT |
|----------------|-----------|-----------|
| Every visit | 46.15% | 6 |
| Sometimes | 53.85% | 7 |
| Rarely | 0.00% | 0 |
| Never | 0.00% | 0 |
| TOTAL | | 13 |

Q6 What is your reaction to the overall idea discussed of adding curb extensions to the western segment (6th Street - 2nd Street) of Main street?

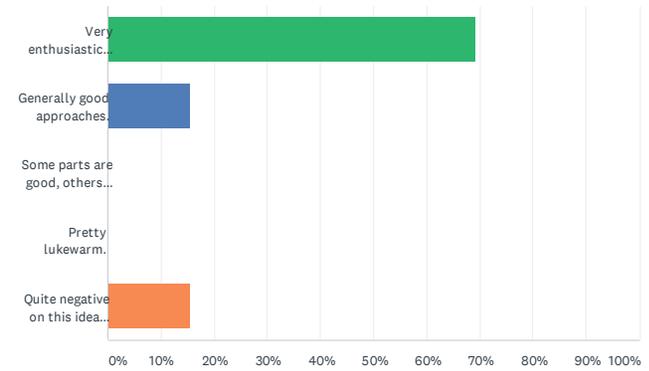
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 61.54% 8 |
| Generally good approaches. | 23.08% 3 |
| Some parts are good, others are doubtful. | 0.00% 0 |
| Pretty lukewarm. | 7.69% 1 |
| Quite negative on this idea – off the mark. | 7.69% 1 |
| TOTAL | 13 |

Q7 What is your reaction to the overall idea discussed for removing parallel parking and increasing the sidewalk width in the central segment of the corridor? (1st Street - Independence)

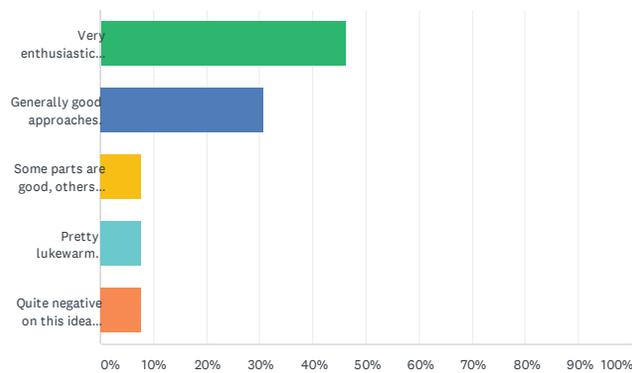
Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 69.23% 9 |
| Generally good approaches. | 15.38% 2 |
| Some parts are good, others are doubtful. | 0.00% 0 |
| Pretty lukewarm. | 0.00% 0 |
| Quite negative on this idea – off the mark. | 15.38% 2 |
| TOTAL | 13 |

Q8 What is your reaction to the overall idea discussed for adding curb extensions and increasing sidewalk width to the eastern segment (6th Street - 2nd Street) of Main Street?

Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 46.15% | 6 |
| Generally good approaches. | 30.77% | 4 |
| Some parts are good, others are doubtful. | 7.69% | 1 |
| Pretty lukewarm. | 7.69% | 1 |
| Quite negative on this idea – off the mark. | 7.69% | 1 |
| TOTAL | | 13 |

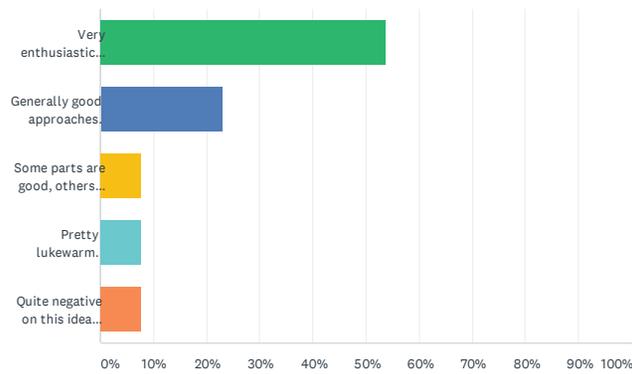
Q9 Do you have any additional comments about the Main Street corridor?

Answered: 5 Skipped: 8

| # | RESPONSES | DATE |
|---|--|-------------------|
| 1 | Taking parking off of Main Street is a bad idea IMO. Repair the existing sidewalks and curbs | 9/21/2020 8:57 AM |
| 2 | No | 9/16/2020 9:50 AM |
| 3 | leave it alone | 9/16/2020 7:25 AM |
| 4 | Add more handicapped spaces | 9/16/2020 7:03 AM |
| 5 | If trees/branches are a concern due to traffic/ visual safety, why not try trellises, arbors, shrubs, bulbs as well as colorful historically correct awnings or overhangs. | 9/15/2020 7:13 PM |

Q1 What is your reaction to the overall ideas discussed for the Pig Creek Historic Path?

Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 53.85% | 7 |
| Generally good approaches. | 23.08% | 3 |
| Some parts are good, others are doubtful. | 7.69% | 1 |
| Pretty lukewarm. | 7.69% | 1 |
| Quite negative on this idea – off the mark. | 7.69% | 1 |
| TOTAL | | 13 |

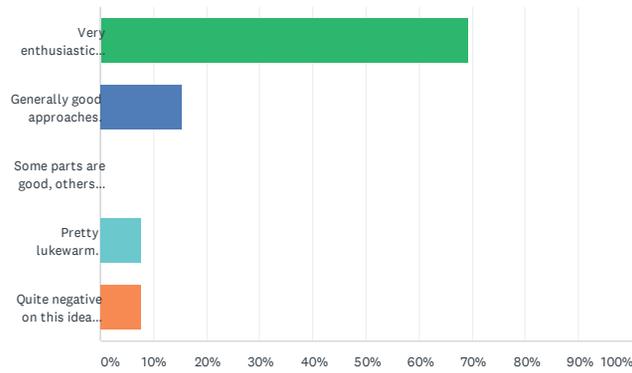
Q2 Do you have any specific comments about that Pig Creek Historic Path proposal?

Answered: 6 Skipped: 7

| # | RESPONSES | DATE |
|---|--|--------------------|
| 1 | Would it be possible to widen it so that bikes could use it too? If not, I still love the idea of the walking path | 9/21/2020 1:24 PM |
| 2 | Safety | 9/21/2020 10:11 AM |
| 3 | We can't/haven't maintained the sidewalks, curbs, streets, etc... that we have now. And you want to add something else the city needs to maintain? | 9/21/2020 8:59 AM |
| 4 | no | 9/16/2020 9:52 AM |
| 5 | leave it alone | 9/16/2020 7:26 AM |
| 6 | No | 9/15/2020 7:02 PM |

Q3 What is your reaction to the overall ideas discussed for the Railway Nucleus?

Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 69.23% | 9 |
| Generally good approaches. | 15.38% | 2 |
| Some parts are good, others are doubtful. | 0.00% | 0 |
| Pretty lukewarm. | 7.69% | 1 |
| Quite negative on this idea – off the mark. | 7.69% | 1 |
| TOTAL | | 13 |

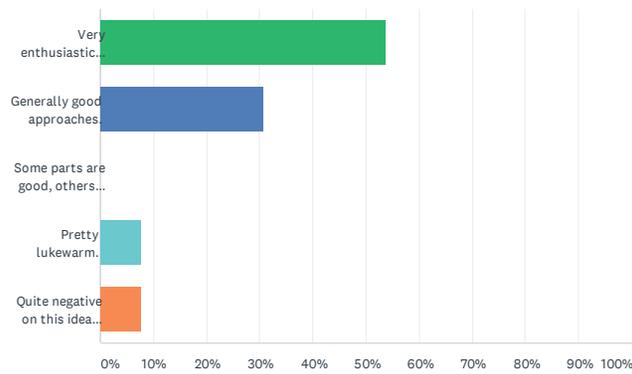
Q4 Do you have any specific comments about the Railway Nucleus proposal?

Answered: 6 Skipped: 7

| # | RESPONSES | DATE |
|---|--|--------------------|
| 1 | I love your vision for that area. I wouldn't mind if instead of the pub it would be a winery, but either is ok. It would also be nice to have a bike cafe/repair and bike accessory shop there as a destination for bike riders once we are connected to the st Clair path. That would attract bike riders to make Mascoutah a destination | 9/21/2020 1:24 PM |
| 2 | None | 9/21/2020 10:11 AM |
| 3 | This is my favorite part of the plan. The open green space and use of mill building will really redefine Mascoutah and make it attractive | 9/16/2020 9:52 AM |
| 4 | eliminate it | 9/16/2020 7:26 AM |
| 5 | Be awesome spot for events and festivals. Mascoutah needs more smaller festivals for create community. Beer festival etc | 9/15/2020 7:16 PM |
| 6 | No | 9/15/2020 7:02 PM |

Q5 What is your reaction to the overall ideas discussed for the Jefferson Gateway?

Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 53.85% | 7 |
| Generally good approaches. | 30.77% | 4 |
| Some parts are good, others are doubtful. | 0.00% | 0 |
| Pretty lukewarm. | 7.69% | 1 |
| Quite negative on this idea – off the mark. | 7.69% | 1 |
| TOTAL | | 13 |

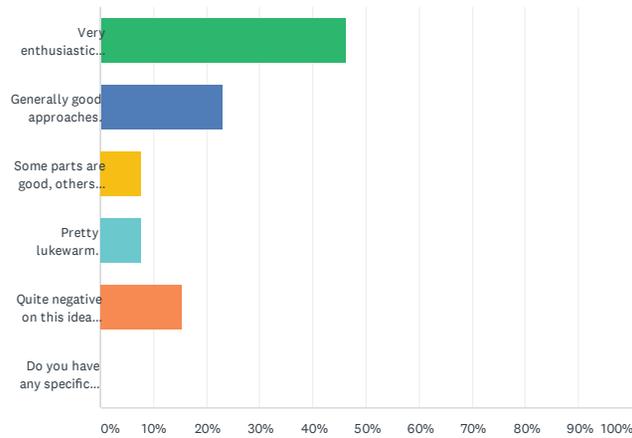
Q6 Do you have any specific comments about the Jefferson Gateway proposal?

Answered: 5 Skipped: 8

| # | RESPONSES | DATE |
|---|--|--------------------|
| 1 | I'm not sure that Mascoutah could support all those retail businesses that you suggest, but it would be great if so! | 9/21/2020 1:24 PM |
| 2 | Congestion | 9/21/2020 10:11 AM |
| 3 | No | 9/16/2020 9:52 AM |
| 4 | eliminate it | 9/16/2020 7:26 AM |
| 5 | No | 9/15/2020 7:02 PM |

Q7 What is your reaction to the overall ideas discussed for the Holy Childhood Focus?

Answered: 13 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|--|-----------|
| Very enthusiastic. Could be great for Mascoutah. | 46.15% 6 |
| Generally good approaches. | 23.08% 3 |
| Some parts are good, others are doubtful. | 7.69% 1 |
| Pretty lukewarm. | 7.69% 1 |
| Quite negative on this idea – off the mark. | 15.38% 2 |
| Do you have any specific comments about the Holy Childhood Focus proposal? | 0.00% 0 |
| TOTAL | 13 |

Q1 Of all the recommendations and ideas we have presented, please list the three you like the most.

Answered: 9 Skipped: 0

| ANSWER CHOICES | RESPONSES |
|----------------|-----------|
| 1. | 100.00% 9 |
| 2. | 100.00% 9 |
| 3. | 100.00% 9 |

| # | 1. | DATE |
|---|--|-------------------|
| 1 | A network of bicycle paths that connect with the St Clair County trails/bike paths | 9/21/2020 6:05 PM |
| 2 | reorganization of parking off Main St | 9/21/2020 3:10 PM |
| 3 | Sidewalk expansion on Main St | 9/21/2020 2:18 PM |
| 4 | Developing bike paths and connecting to st Clair co system | 9/21/2020 1:36 PM |
| 5 | Reduce main st parking | 9/16/2020 5:00 PM |
| 6 | Railway Nucleus | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Creative parking ideas | 9/15/2020 7:32 PM |
| 9 | Railway park with stage and brew pub | 9/15/2020 7:18 PM |

| # | 2. | DATE |
|---|--|-------------------|
| 1 | Developing the Main Street- Railway Avenue nucleus | 9/21/2020 6:05 PM |
| 2 | additional sidewalks on Main & Church | 9/21/2020 3:10 PM |
| 3 | Historical Society creek project | 9/21/2020 2:18 PM |
| 4 | Focusing on the Railway nucleus to make that a gathering center with cafes, performance areas, etc. There is so much potential for that to become an attraction/ destination to our city | 9/21/2020 1:36 PM |
| 5 | Route 4/Jefferson Parking lot changes | 9/16/2020 5:00 PM |
| 6 | Old gas station converted to ice cream shop | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Embracing the historical aspects of the buildings & areas | 9/15/2020 7:32 PM |
| 9 | New and Wider sidewalks downtown with opportunity to eat | 9/15/2020 7:18 PM |

| # | 3. | DATE |
|---|--|-------------------|
| 1 | Developing a historic walking path along the Hog River. | 9/21/2020 6:05 PM |
| 2 | shared use paths & bike blvds | 9/21/2020 3:10 PM |
| 3 | Railway and Main redesign | 9/21/2020 2:18 PM |
| 4 | The pig creek historic path | 9/21/2020 1:36 PM |
| 5 | Proposed Ice Cream shop | 9/16/2020 5:00 PM |
| 6 | Bike path connection on west end of town to St Clair County network. (Would be great to be able to bike to Eckert's) | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | The incorporation of trees, flowers & green space. | 9/15/2020 7:32 PM |
| 9 | Historical district to help with more redevelopment | 9/15/2020 7:18 PM |

Q2 Which three address the most practical needs of Mascoutah?

Answered: 9 Skipped: 0

| ANSWER CHOICES | RESPONSES |
|----------------|-----------|
| 1. | 100.00% 9 |
| 2. | 100.00% 9 |
| 3. | 100.00% 9 |

| # | 1. | DATE |
|---|--|-------------------|
| 1 | Bike Path network starting with connecting Railway Avenue with South Tenth Street along the old railroad track. | 9/21/2020 6:05 PM |
| 2 | additional sidewalks | 9/21/2020 3:10 PM |
| 3 | Sidewalks along Main St | 9/21/2020 2:18 PM |
| 4 | Bike paths and connecting to St Clair county system to offer an alternate and sustainable mode of transportation | 9/21/2020 1:36 PM |
| 5 | Main st sidewalk improvements | 9/16/2020 5:00 PM |
| 6 | Railway Nucleus | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Parking issues | 9/15/2020 7:32 PM |
| 9 | Safer sidewalks downtown | 9/15/2020 7:18 PM |

| # | 2. | DATE |
|---|---|-------------------|
| 1 | Main Street -Railway Avenue core development | 9/21/2020 6:05 PM |
| 2 | more parking | 9/21/2020 3:10 PM |
| 3 | Refresh parking along Church St (more green space) | 9/21/2020 2:18 PM |
| 4 | Development of the Railway nucleus to revitalize the city's economy | 9/21/2020 1:36 PM |
| 5 | Making main st more ped friendly | 9/16/2020 5:00 PM |
| 6 | Convert old gas station into ice cream shop. | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Using Postal Mill as a viable space | 9/15/2020 7:32 PM |
| 9 | More dining/entertainment downtown | 9/15/2020 7:18 PM |

| # | 3. | DATE |
|---|--|-------------------|
| 1 | Hog River walking path | 9/21/2020 6:05 PM |
| 2 | bike / ped paths | 9/21/2020 3:10 PM |
| 3 | Revitalize areas south of Main St | 9/21/2020 2:18 PM |
| 4 | Widening the sidewalks and eliminating parallel parking on Main Street | 9/21/2020 1:36 PM |
| 5 | more bike paths | 9/16/2020 5:00 PM |
| 6 | Connect bike path on west end of town to St. Clair County network. | 9/16/2020 9:55 AM |
| 7 | none | 9/16/2020 7:27 AM |
| 8 | Addressing the awful sidewalks (or lack of) along main street. | 9/15/2020 7:32 PM |
| 9 | More bike trails | 9/15/2020 7:18 PM |

Q3 What is the most important thing to do first?

Answered: 9 Skipped: 0

| # | RESPONSES | DATE |
|---|---|-------------------|
| 1 | Bike path network should be first and is the least expensive. It will be a catalyst to attract people to move around the City while embracing an active lifestyle. | 9/21/2020 6:05 PM |
| 2 | bike/ped paths | 9/21/2020 3:10 PM |
| 3 | Presentation to get support for new sidewalks on Main | 9/21/2020 2:18 PM |
| 4 | Bike path recommendations—I think it would attract future homeowners if we were more connected to the overall network in the area. Right now, I feel that we are isolated in that regard. | 9/21/2020 1:36 PM |
| 5 | Main street improvement from Route 4 to 2nd street | 9/16/2020 5:00 PM |
| 6 | Railway Nucleus | 9/16/2020 9:55 AM |
| 7 | nothing - leave it all alone | 9/16/2020 7:27 AM |
| 8 | Main street sidewalks. Then parking. | 9/15/2020 7:32 PM |
| 9 | Railway park with dining/nightlight | 9/15/2020 7:18 PM |

Q4 How would you describe success in this project?

Answered: 8 Skipped: 1

| # | RESPONSES | DATE |
|---|--|-------------------|
| 1 | Implementing the plan by connecting with existing paths and completing the connection between South Railway and South 10th Street will bring outdoor life to the city by pedestrians and bicycle riders. When people get out and move they will be ready to engage I. Use if the downtown are and the historic Hog River walkway too | 9/21/2020 6:05 PM |
| 2 | Completion of one project every 3 years to complete the 3 most practical projects | 9/21/2020 2:18 PM |
| 3 | I've already seen an increase in walkers and bike riders since the berm path was built. If we had a bigger network, I would foresee many more people out and about on these paths, increasing the overall health and well being of our residents, as well as also increasing commuter options by bike. It would result in a healthier community. | 9/21/2020 1:36 PM |
| 4 | If it actually happens | 9/16/2020 5:00 PM |
| 5 | Downtown becomes a vibrant district that attracts customers from the entire region and property taxes are not raised as a result of this. | 9/16/2020 9:55 AM |
| 6 | leave it all alone | 9/16/2020 7:27 AM |
| 7 | When we see more hometown citizens using the sidewalks, riding their bikes & patronizing our local businesses. | 9/15/2020 7:32 PM |
| 8 | More vibrant downtown | 9/15/2020 7:18 PM |

APPENDIX F

Mascoutah Great Streets – IDOT compiled comments

Presentation 2

There are numerous proposed crossings of IL 4 of the SUP. Will these be elevated crossings or at-grade?

Slide 4- Any requested signage on I-64 must meet Department policies. There is not a “Special Signs Program” where IDOT would be funding these signs. The current “Exit 20” signs indicating Mascoutah and Lebanon are standard policy signs. These would not be altered.

There is an existing green board mileage sign on IL 177, east of Il 158 that indicates Mascoutah is 4 miles ahead. Additional directional signage would not be permitted.

Slide 2 – The bike plan shows a SUP along IL Route 158 until it reaches IL Route 161 and the connection to ‘bike friendly roads’. But the SUP accommodates not just bikes but pedestrians as well. Roadway shoulders aren’t acceptable pedestrian accommodations. How will the plan accommodate pedestrians or is the path really just a bike path and not shared use?

Bike plan shows potential SUP along IL Route 158 between IL Route 177 and IL Route 161. The IL route 158 bridge over Loop Creek overtops frequently. The district is studying extending IL Route 158 from the ‘T’ intersection at IL Route 161 to the south to intersect with IL Route 177. This will have shoulders wide enough to accommodate bikes. You may want to consider showing the SUP to continue west along IL Route 177 to the IL Route 158 extension to use the shoulders on the IL Route 158 extension for bikes.

Presentation 3

Slide 1 The SUP for 6th St – where does it connect to to the north?

Slide 2 – The two SUP coming from the south that terminate in the open space nodes, how will those be connected to other paths or bike facilities to provide connectivity rather than dead ends?

Is there enough room to provide a SUP on 6th Street without building impacts? 6th Street is an IDOT maintained roadway. All suggested paths or improvements would need to follow current IDOT policies.

Slide 6 – The bicycle wayfinding signs appear very elaborate. Have you investigated the signage used in Edwardsville & SIU-E for the MCT Bike Trails. There are far less wayfinding signs in Edwardsville in a much larger area than you have proposed for Mascoutah and they have thousands of users each day.

Presentation 5

Slide 13 – Banners must adhere to the current Outdoor Advertising guidelines for advertising (no more than 1/3 of banner space devoted to a business brand). All vehicular signage must follow the MUTCD guidelines. Typically, decorative signage and patterns do not meet that criteria.

Wayfinding signs are allowed, via a permit, but there are limitations as to size and messages. Other entry type signs would not be allowed on State right-of-way.

Slide 15 – The Department cannot allow a kiosk sign to be placed in the center of the intersection of IL 177 at Railway. This is a safety concerns and violates multiple Department policies.

Slide 16 – The proposal recommends the continental crosswalks be installed. For IL 177, the city needs to keep in mind, this is a maintenance function that the city is required to perform which it appears the existing crossing markings are very worn & faded. Crosswalks should be limited to one across Main Street at any specific intersection. One crosswalk on either side of an intersection is not permitted.

Slide 17 – The proposed Rapid Flashing Beacon is typically for a mid-block crossing for a state route, not an unsignalized intersection. This would also be the city’s maintenance for any installation and require a permit from Operations. Each proposed installation would have to be reviewed to ensure compliance with Department policies. Multiple mid-block crossing should be discourage and limited to preferred intersections.

Slide 18 – Pedestrian bump-outs are usually only required when there is on-street parking. If on-street parking is being eliminated, then bump-outs would not be allowed. Even if they were, the 30’ minimum roadway width would need to be maintained at all times.

Slide 20- Large canopy trees will not be permitted to overhang the driving lanes of IL 177/Main Street.

Slide 21- A WB-65 turning vehicle must be accommodated with encroachment on all public side roads, in addition to the 30’ minimum width. This will impact the design on any proposed bump-outs or stormwater public gardens and their location/orientation. Seating within bump-outs or stormwater gardens should be limited to the City’s roads, and not along IL 177.

Presentation 6

Keep in mind all sidewalk along a state route, must be on public ROW and maintained by the city. There are some locations in Mascoutah that the existing sidewalk may not be on public ROW.

Slide 3 - the current construction project has the brick stamped pavement – this is an aesthetic design feature that if the City choses they want this carried through on the IDOT project, the City will be required to pay the extra cost for that work and likely enter into a maintenance agreement with IDOT to maintain that surface. The Department would not be able to patch or maintain stamped concrete.

The city’s two block project also was granted a design exception for narrow lane widths and narrow face-to-face of curb. When IDOT studies the remaining roadway, we will be required to follow policy and the minimum face-to-face of curb will be 30’ without parking.

The Department would not be agreeable to incorporating special design features such as stamped concrete or decorative light poles into our proposed roadway reconstruction. Decorative lighting and/or wayfinding signs are allowed via a permit from Operations and will become the City’s maintenance. Any special features must adhere to current Statues and Department policies.

Slide 4 – Food trucks cannot be permitted to operate within the parking lanes.

Slide 5 - the curb extensions would have to be evaluated to make sure the design vehicle turning movement could be accommodated

Slide 8 - If the curb is moved, will the city reconstruct the inlets and other aspects of the affected drainage system? More inlets may be needed as well since there won't be as much room for encroachment of storm water on the pavement. Will the city reconstruct the sidewalk and ensure it meets ADA? They will need to maintain the 5' minimum sidewalk as well. If wider sidewalk widths are desired, the City should use these widths to address ADA access (removal of steps and construction of ramps) into all non-conforming businesses first. Once policy ramps are installed, many will require railings, and then the required 5' pedestrian access route around these ramps, that may incorporate all of the proposed 12' sidewalk width.

Other - IDOT encourages implementation of parking lots and bike access off of Main Street. However, removal of parking spaces for parkettes and outdoor dining is not a permitted use of State right-of-way.

Presentation 7

Should the City wish to pursue traffic signals or a round-about at the IL 4/Onyx intersection, this would be the City's responsibility to provide a traffic study showing that signals are warranted for either Warrants 1 or 7. This is not a high-accident location and IDOT would not participate in development-driven signals or improvements. If warrants 1 or 7 are not met then signals cannot be installed. If they are then any signals would be the cost responsibility of the City, including any pavement widening for turn lanes.